

Americans with Disabilities Act
Compliance on
Locally Administered Projects
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What We'll Cover Today

- Brief overview of ADA
- Curb Ramp Compliance
- Midblock crossings/RRFBs
- Misc. Sidewalk and Parking Stuff
- Technical Infeasibility
- Work Zone Compliance
- Website and Map Viewer

Americans with Disabilities Act of 1990

- Prior to ADA - Sec. 504 Rehabilitation Act (1973)
 - Accessibility on Federally funded facilities
- The intent of ADA is to allow people with disabilities to **fully participating in society.**
 - **Accessibility in all facilities, work places, commercial facilities. (When built or altered)**
- ADA compliance is not only about accessibility...
It is about Safety!

Why is accessible infrastructure important?

- Individuals rely on our pedestrian infrastructure to get to work, shop, access health care and connect with friends and family.
- Individuals with disabilities rely on safe and accessible infrastructure to provide consistent access to jobs and people.
- If there is no access or limited access, people with disabilities are prevented from **fully participating in society.**



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Curb Ramps

New Maine DOT Standard Details

- MaineDOT has more comprehensive Standard Details for Pedestrian Ramps.
- They provide more scenarios for curb ramps options
- These can be found at:
- <https://www.maine.gov/mdot/contractors/publications/standarddetail/>

Where must curb ramps be provided?

Curb ramps:

- Are needed when a **sidewalk or other pedestrian walkway** crosses a vehicular or pedestrian path of travel.
- Must be located to ensure a person with a mobility disability can travel from a sidewalk on one side of the street, over or through any curbs or traffic islands, to the sidewalk on the other side of the street.

Cheat sheet for curb-ramp compliance

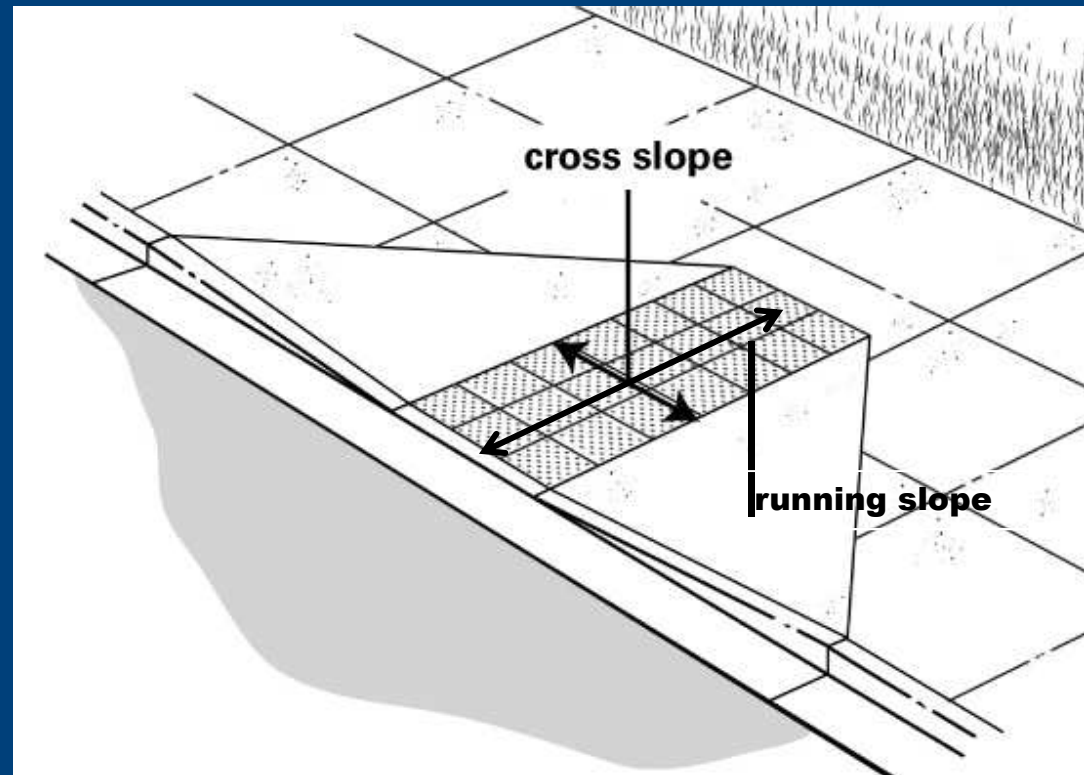
	Minimum Requirements for NEW or RECONSTRUCTED Pedestrian Facilities
Running Slope	Max. 8.33% (1:12)
Cross Slope	Max. 2% (1:50)
Clear Width	Min. 4 feet
Counter Slope	Max. 5% (1:20)
Flared Sides	Max. 10% (1:10)
Landings (Turning Space)	<p>A turning space 4 feet long and as wide as the ramp must be present to be compliant.</p> <p>This space is typically at the top of the ramp for perpendicular ramps and at the bottom for parallel ramps.</p>
Diagonal Ramps	A 4 foot square clear space must be present at the bottom of the ramp outside active travel lanes to be compliant.
Detectable Warnings	Required at traffic controlled intersections. Mid-Blocks. Full length of curb ramp opening

Running Slope & Cross Slope

Running slope should be no more than 8.3% or 1:12

The cross slope of a curb ramp should be no more than 2% or 1:48.

Both slopes should be measured at the center of the ramp.



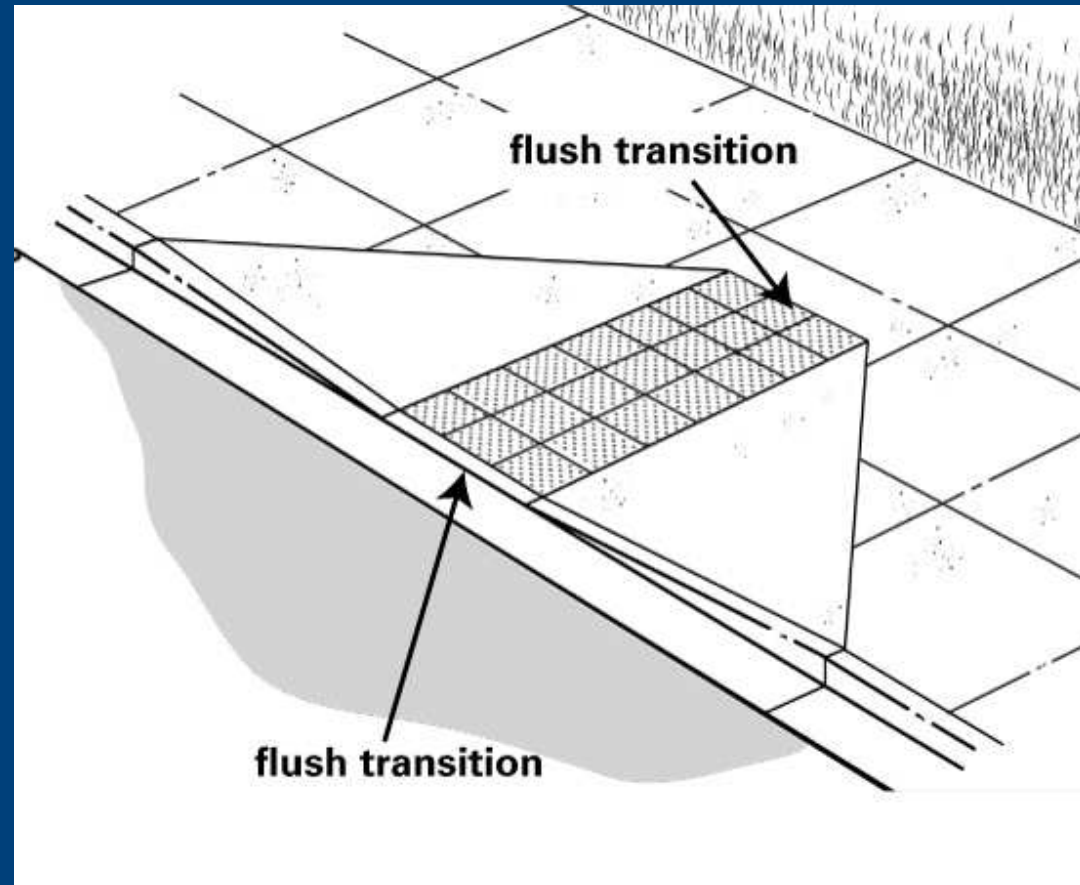
Ramp Slope



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Flush Transition

The curb ramp must be flush with the pavement and any transition to the ramp.



Is This Flush?

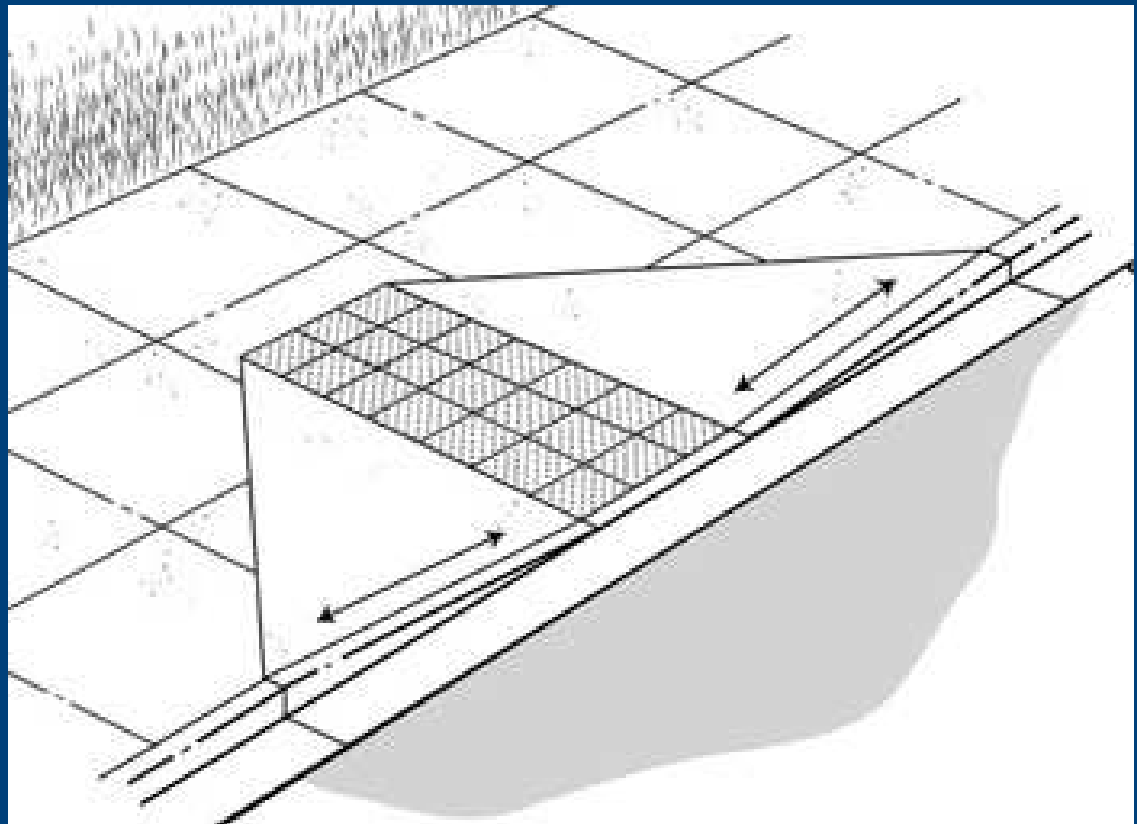


Flared sides

Not all curb ramps have flared sides.

Where there are flared sides, the slope of the flares cannot exceed 10%.

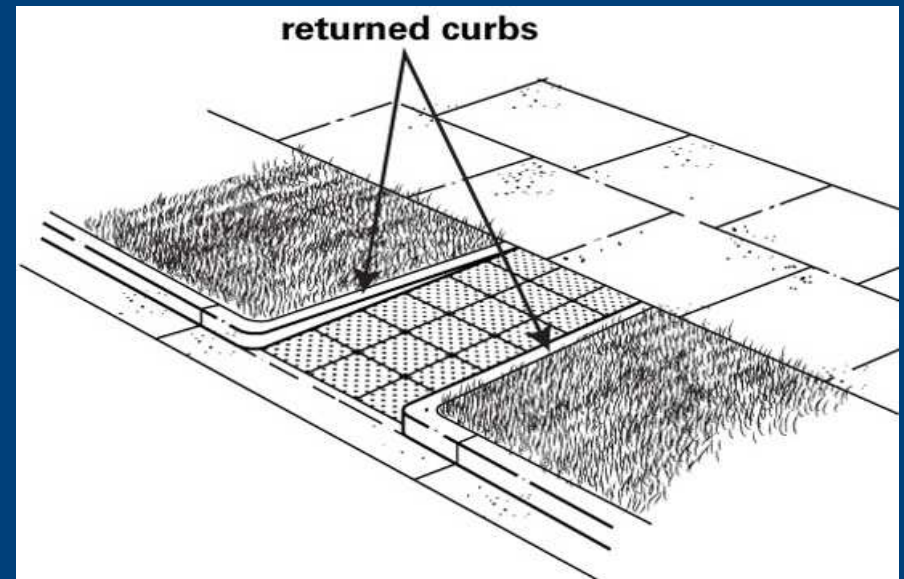
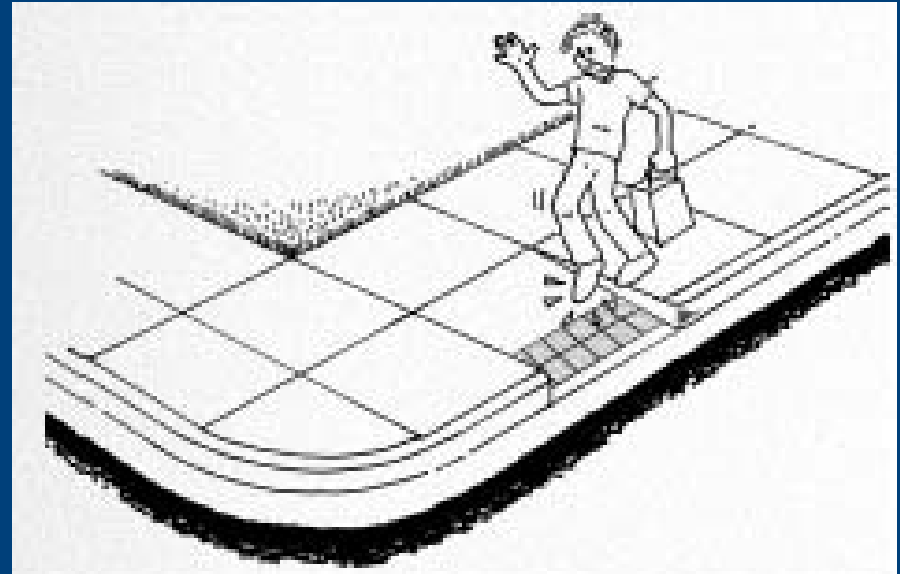
8 Foot curb ends



Returned Curbs

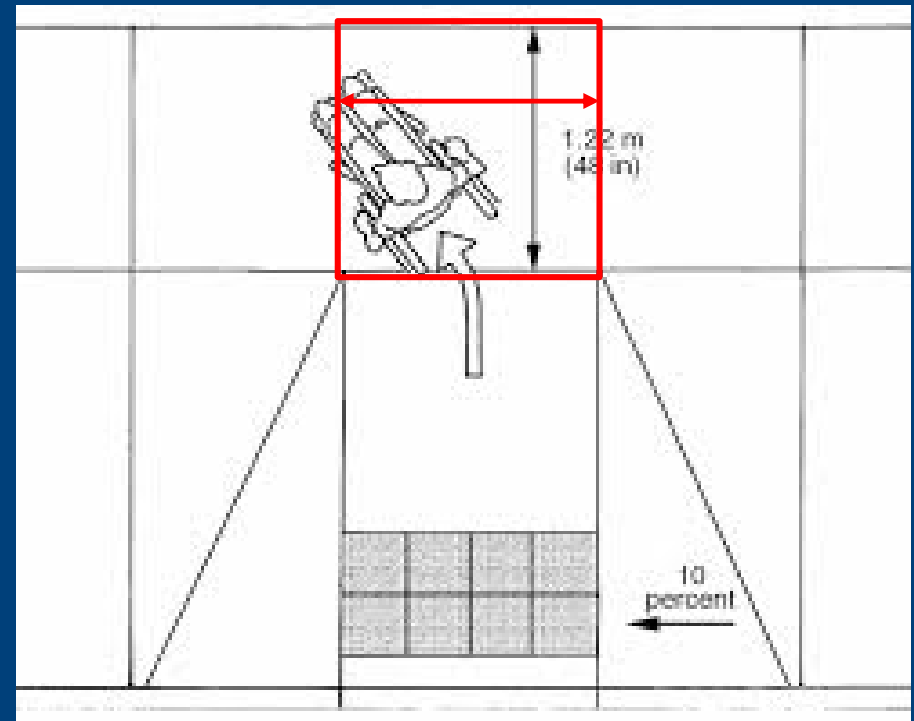
Returned curbs should never be on the path of travel.

They should be used only with an esplanade or if there are obstacles, e.g. a signal post

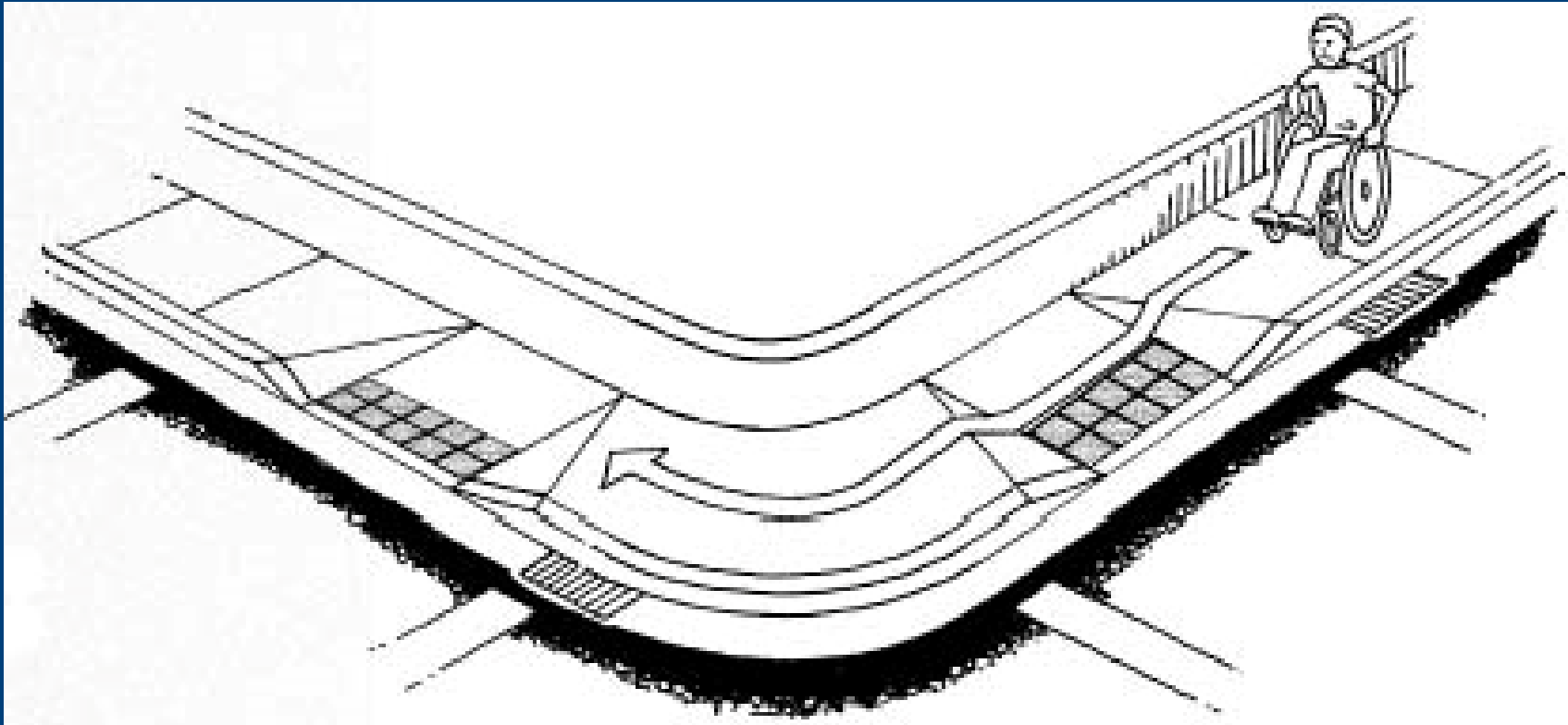


Level Turning Space (Landings)

- A level landing is vital to ensure that a chair or mobility device can make a turn on and off the ramp *or* go past the ramp.
- Width of the ramp.
- Level landings should be no more than 2% in both directions.



This is why we require level turning spaces.



Designing Sidewalks and Trails for Access, FHWA 2002

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Counter Slope concerns

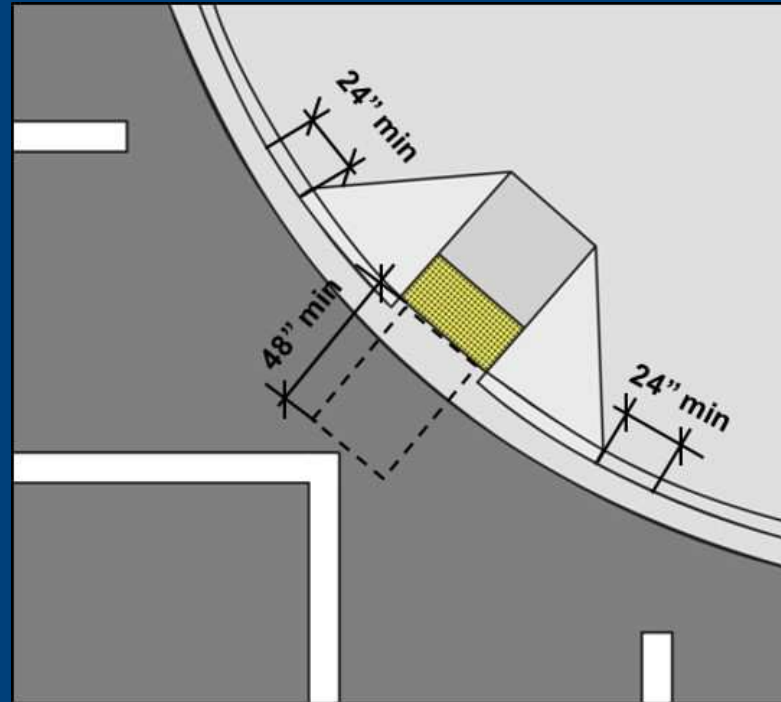
Counter slopes should be no more than 5%



Designing Sidewalks and Trails for Access, FHWA 2002

Diagonal Clear Space

There should be a 5X5 clear space when using a diagonal curb ramp for perpendicular curb ramps



Detectable Warnings (DWs)



Why do we have DWs?



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Detectable Warnings

- Plate or area at bottom of curb ramp with raised truncated domes.
- Domes alert people with visual impairments to **STOP** at roadways.
- They do **NOT** direct a person.
- Should be placed **FULL** width of the curb ramp.
- Should be at least 2 feet deep.



More on Detectable Warnings

How do you determine where DW's are needed?

- At street intersections with signalization:
 - Stop/Yield sign
 - Signal
 - At Mid-block crossings
 - Some RR crossings
- Not needed at driveways or parking lots unless they warrant signalization.

Nice try, but??



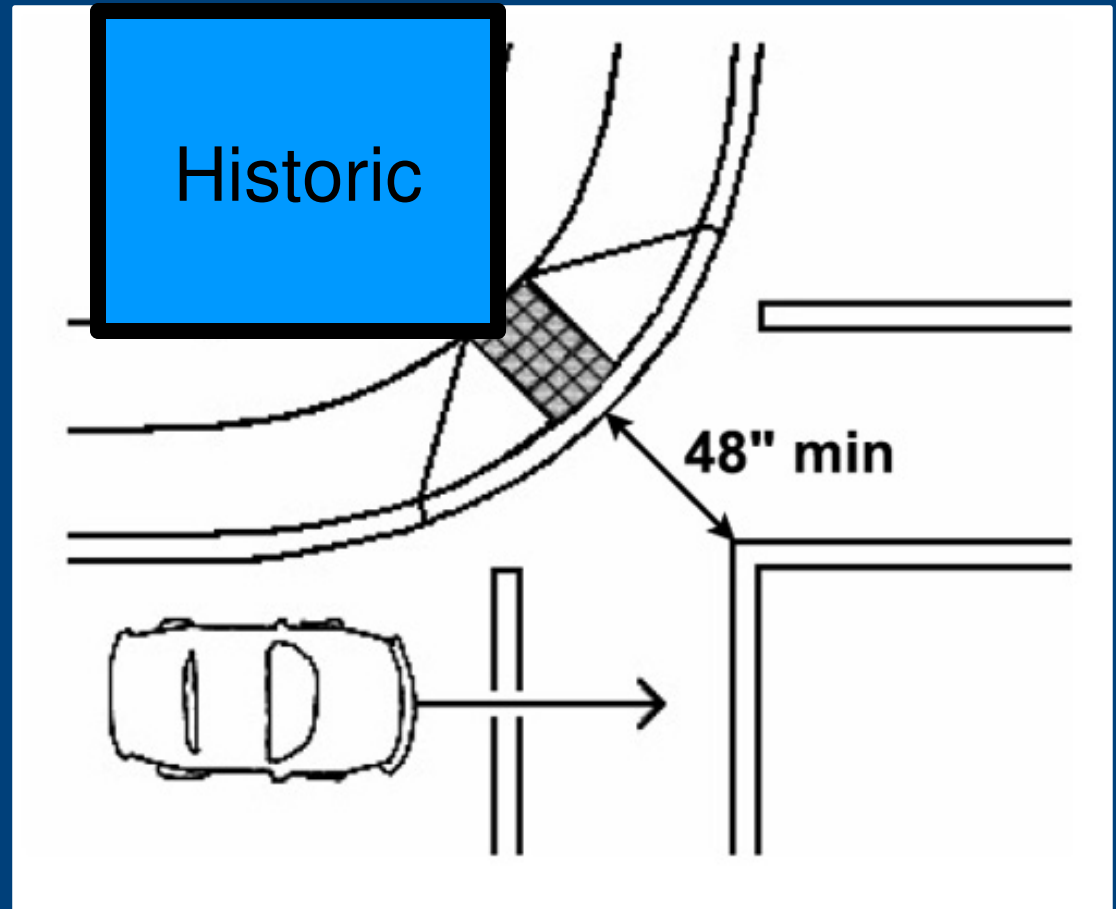
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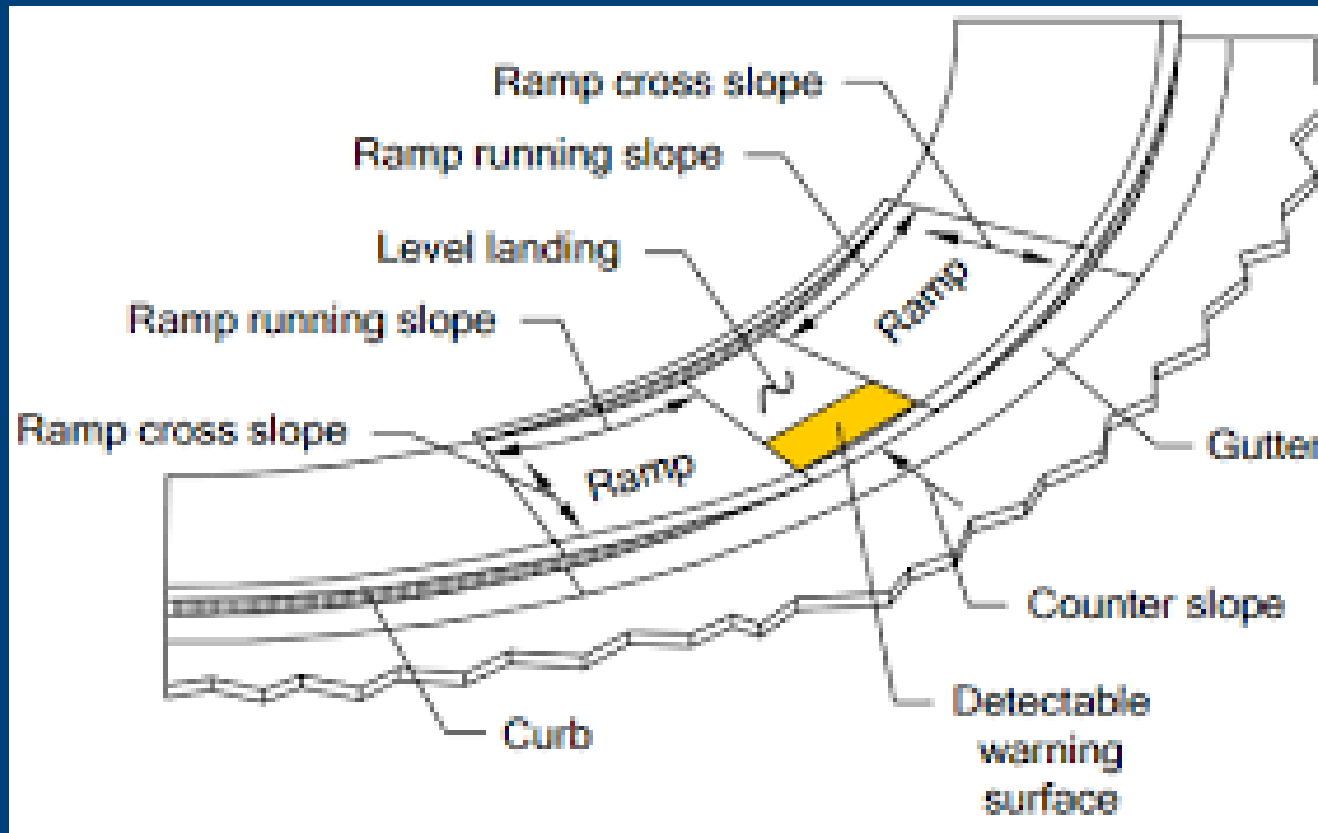
These are not DWs!



Diagonal Curb Ramps



Example of Compliant Diagonal Parallel Ramp



Perpendicular Curb Ramps

- Will become more common as we transition away from diagonal curb ramps



Parallel Curb Ramps



Midblock Crossings

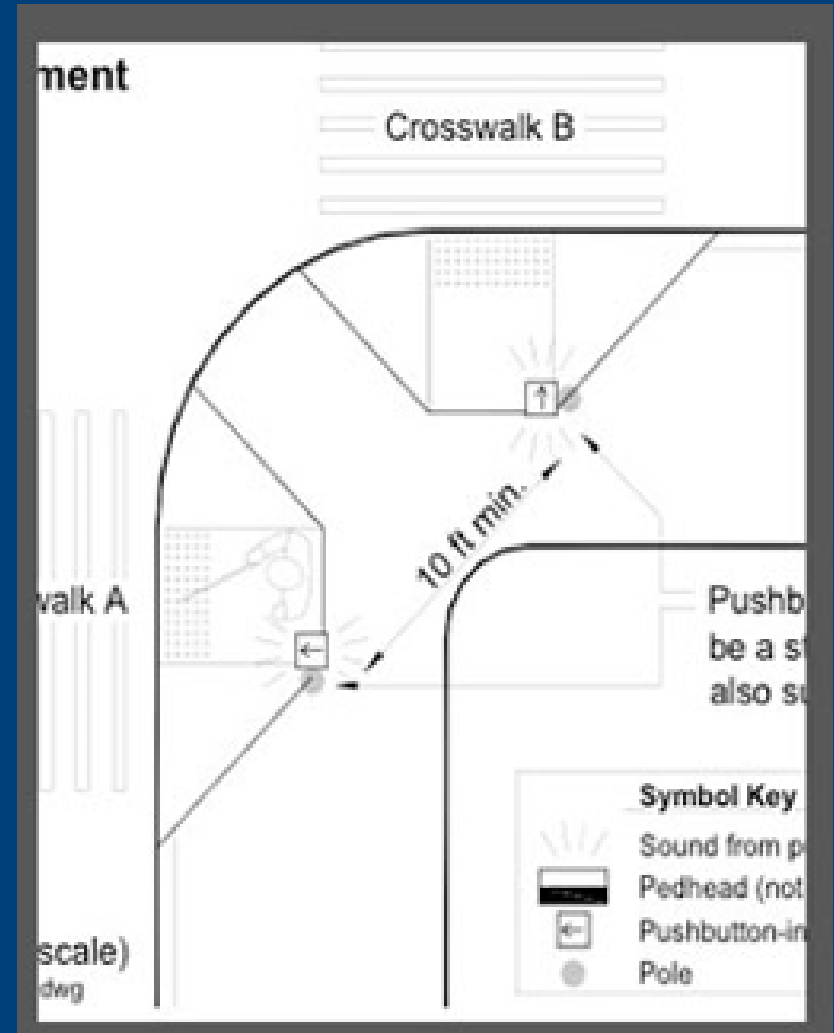


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Accessible Pedestrian Signals (APS)

- Guidance in the MUTCD, Sec. 4E.
- APS should be within 6 feet of curb.
- Signal buttons should be 10 feet apart, unless work is the addition of APS at existing poles, *or* where it is technically infeasible.
- If it is necessary to mount to push buttons on one pole, there should be a speech walk message and a street identification message.



Rectangular Rapid Flashing Beacons (RRFBs)



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On-Street Parking

- No parking shall be allowed within 20 feet of an unsignalized crosswalk (includes mid-block) and 30 feet of a signalized intersection.
- Parking restrictions can be removed when bump-outs or curb extensions are built.
- Need to allow the pedestrian to be seen by the traveling public.
- “No Parking” signs should be installed.

Sidewalks

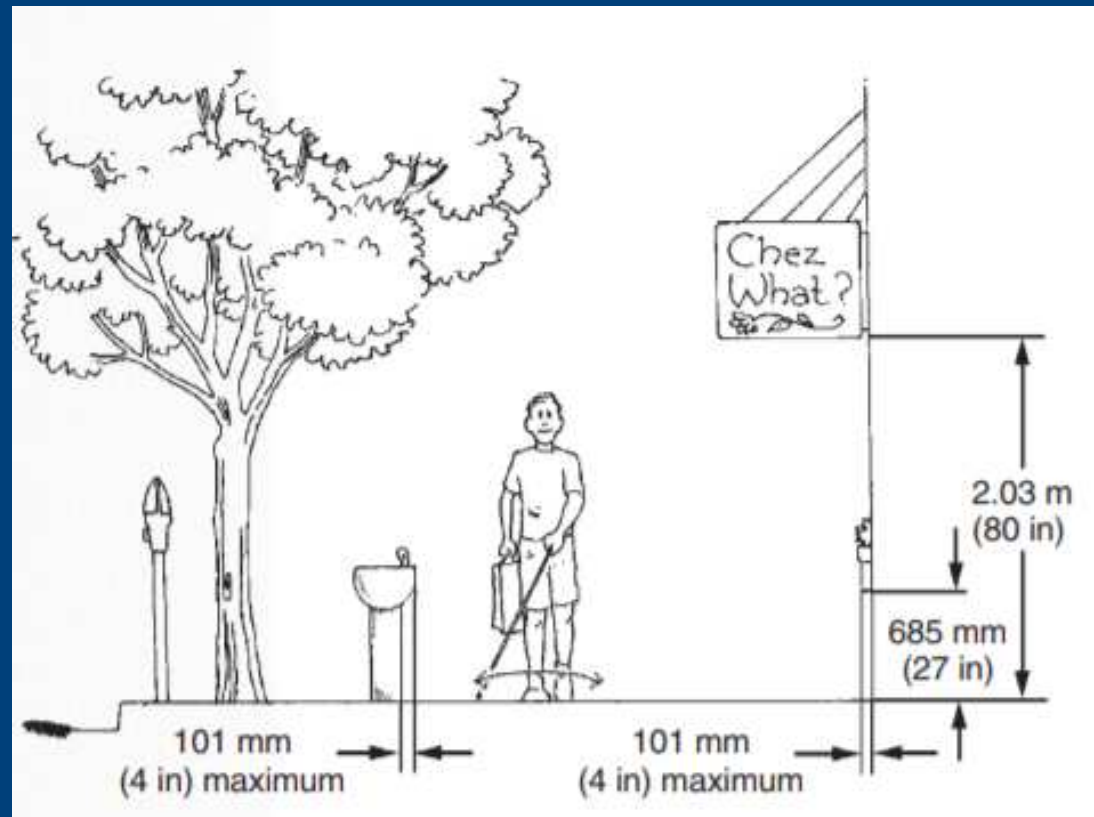
- On state roads, sidewalks should be built 5 feet wide.
- A sidewalk can be 4 feet if necessary, **BUT** such a sidewalk must have a 5x5 turning space every 200 feet.
- Existing sidewalks can be as narrow as 3 feet, but these are not ideal.
- Sidewalks should be firm, stable and slip-resistant.
- Sidewalks must have cross slopes of less than 2%

Streetscaping

- Clear path of travel
- No tripping hazards
- Furniture placement
- Plantings

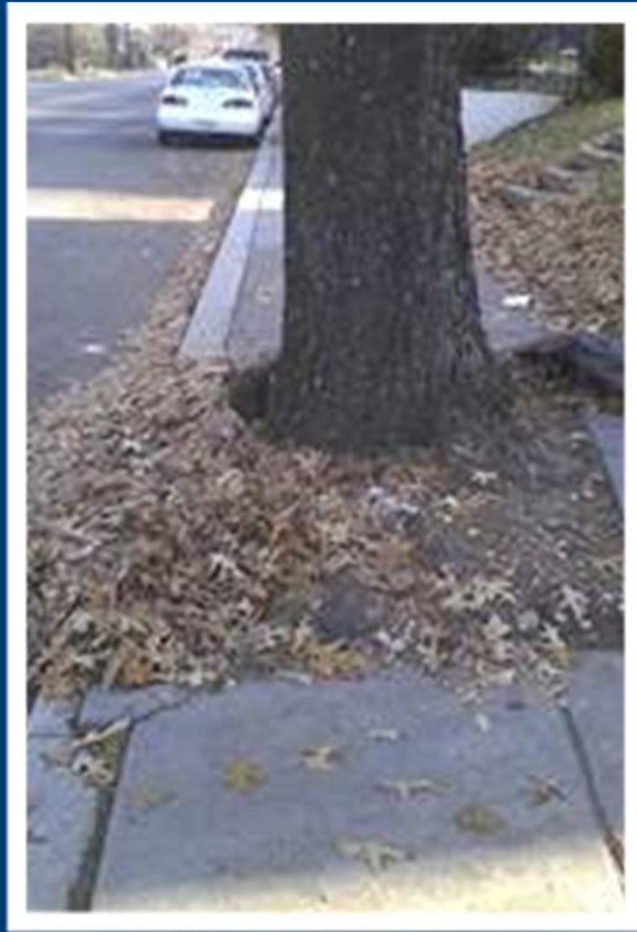


Sidewalk Corridors



Designing Sidewalks and Trails for Access, FHWA 2002

Mighty Oaks, from Little Acorns Grow





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ADA and Utilities:

- You must have at least a 3-foot travel way, *but*
- That doesn't mean it can't be wider.
- Think about the user...

New Facility or Alteration?



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Working with Abutters

- Are you required to provide ADA access to businesses or public buildings if you are replacing a sidewalk?
- No. But, you cannot make access worse:
 - **202.3.1 Prohibited Reduction in Access.** An alteration that decreases or has the effect of decreasing the accessibility of a building or facility below the requirements for new construction at the time of the alteration is prohibited.

- 2010 ADA Standards for Accessible Design

Technical Infeasibility

The 2011 PROWAG recognizes that it is not always possible for altered facilities to fully comply with new construction requirements because of existing physical constraints.

Where existing physical constraints make it infeasible for altered facilities to fully comply with the requirements for new construction, compliance is required to the maximum extent feasible *within the scope of the project*.

MaineDOT ADA Technical Infeasibility Form (TIF)

- When a curb ramp is built, MaineDOT must document that it is built to the maximum extent feasible.
- If a curb ramp cannot be made completely compliant, we must document the reasons why.
- On MaineDOT roads, ADA Technical Infeasibility Form must be filled out and submitted.

ADA TIF

- Form should be filled out ONLY when the curb ramp does not meet ADA requirements.
- Completed form should go to Region Engineer, Program Manager, Title II ADA Coordinator for review.
- Request may be approved at the Program level or forwarded to the Engineering Council for more review.
- Completed and approved form should be sent to the ADA Coordinator.
- <https://www.maine.gov/mdot/engineering/highway/>
- Under “General” and “Forms”

Temporary Traffic Control (TTC) Zone Compliance

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Alternate Pedestrian Access Routes

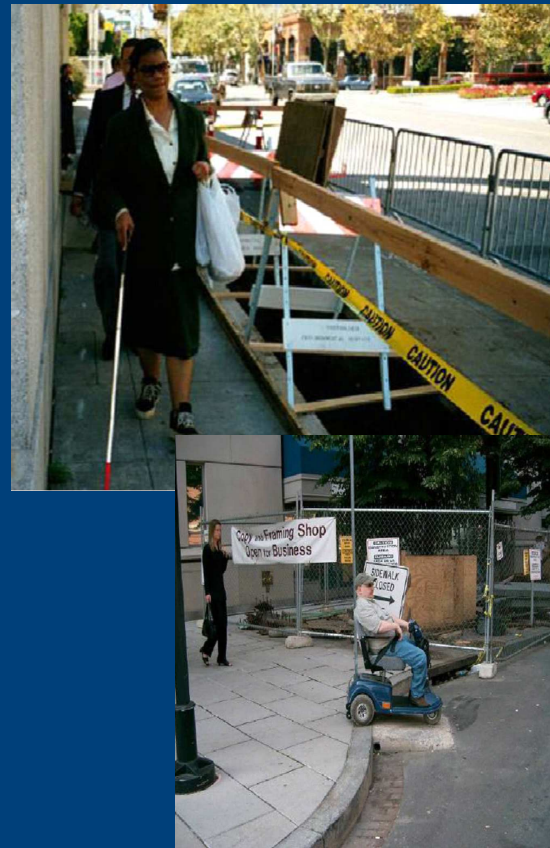
Alternate Pedestrian Access Routes are required “to the maximum extent feasible” when an existing pedestrian access route is blocked by construction in a TTC zone or other temporary condition.

Pedestrians

- Pedestrians will not want to backtrack to other intersections or add distance to their trips
- More importantly ...
 - People who have physical impairments may not be able to do so.

Alternate Routes

- Provide **detectable alternate routes** when pedestrian access route is detoured
- **Same-side** is best, if feasible



Considerations for TTC Zone

- Promote adequate pedestrian safety via **physical separation** from work space and vehicular traffic.
- Provide **adequate and safe detour(s)** whenever sidewalks are closed or blocked.
- **Maintain pedestrian access** to businesses, residences, transit stops, etc.
- Provide **temporary nighttime lighting** for pedestrian walkways throughout the TTC zone.

Ensure compliance with ADA

- Provide an **alternate route** when existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone.
- Ensure a **minimum sidewalk width of 36"**, erect curb ramps, and provide passing space (minimum 5 foot by 5 foot space every 200 feet).
- Maintain a **minimum width** and **smooth surface** to avoid creating tripping danger and to minimize barriers to wheelchair use.
- Make all barriers and channelizing devices **detectable** for pedestrians with visual disabilities.

Public Information

- Community announcements – Residents, businesses, schools
- Project web sites – Particularly useful with major projects, heavily used facilities
- Other languages, as appropriate

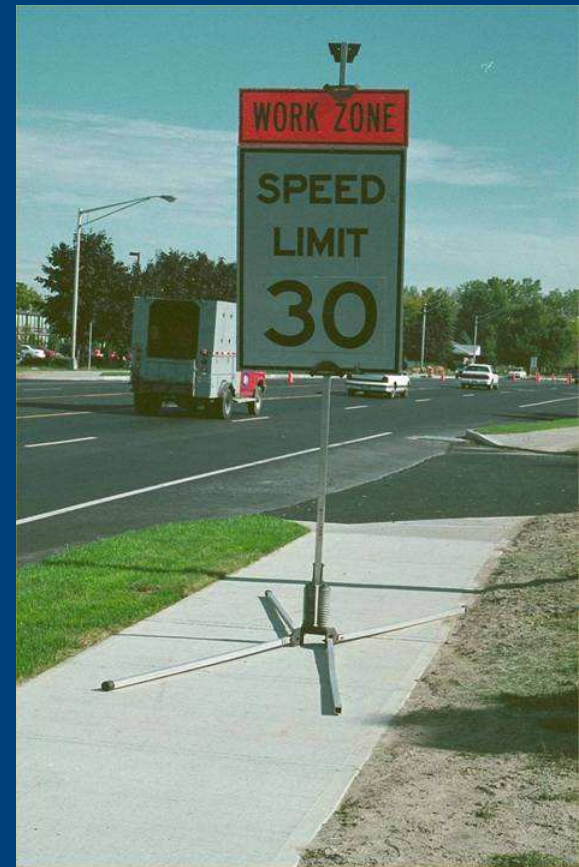
Guidance

- Manual on Uniform Traffic Control Devices 2009:
 - Part 6 Temporary Traffic Control
 - Chapter 6D.01 Pedestrian Considerations
 - Chapter 6D.02 Accessibility Considerations
- ATSSA Pedestrian Check list:
www.fhwa.dot.gov/indiv/docs/atssa_pedestrian_checklist.pdf
- Applying ADA in Work Zone: A Practitioner's Guide:
www.workzonesafety.org/files/documents/training/fhwa_wz_grant/ada_guide.pdf

Effective Barriers



Examples of what not to do...



Common Sense



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MaineDOT ADA Website

MaineDOT ADA Website

<https://www.maine.gov/mdot/civilrights/ada/>

MaineDOT Map Viewer

<https://www.maine.gov/mdot/mapviewer/>

What is wrong here?



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And here...?

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Fixing things at the end is costly!

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Thank you!