

## Supplemental Supporting Information for a Finding of Effect

Project: Portland 24293.00

Scope: Intersection Improvements

Finding of Effect: **No Adverse Effect**

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### Purpose and Need

The purpose of the proposed action is to correct vehicular and pedestrian safety deficiencies at the Congress Square intersection while maintaining a smooth traffic flow on High Street in downtown Portland.

The need for this project is because the project area is a major cross-town arterial connecting the Casco Bay Bridge with I-295 that sees a heavy volume of daily vehicular traffic, as well as heavy pedestrian traffic. Traffic controlling measures would maintain traffic flow while providing pedestrian improvements to this active pedestrian area.

### Project Background

This roadway and intersection project is part of a larger City improvement program to revitalize Congress Square and improve all transportation modes at the intersection. Police reports indicate that the intersection is a high-crash location that experiences several collisions with pedestrians in the crosswalks. The new design would simplify this busy intersection and the proposed improvements would benefit both motorists and pedestrians in terms of safety. New pedestrian signal heads and more prominent crosswalks – along with upgraded traffic signal equipment to replace the existing 40-year-old equipment – are proposed to help mitigate this situation. Overhead lane-use signage and improved pavement markings would also alleviate the number of turning maneuvers from wrong lanes. Finally, eliminating the Free Street slip-lane in front of the PMA would address the potential confusion for drivers headed east-bound on Congress Street and which lane to be in.

A strong focus has also been placed on upgrading Congress Square Park and the plaza in front of the Portland Museum of Art (PMA) to improve pedestrian accessibility. The City has been working with a local citizens group, Friends of Congress Square Park, and the Portland Public Art Committee (PPAC) for several years now to develop a new design vision for this intersection and public space surrounding it. The City has also worked with and presented materials for approval to the Portland Historic Preservation Board and Greater Portland Landmarks. The intersection of Congress at High and Free Streets represents a significant communal and economic focal point for the City and as such local interests have been committing substantial time and efforts to see it improved.

The City retained the New York-based artist Sze Studio and a Design Team led by Philadelphia-based design firm WRT. Several public forums were conducted during the Conceptual Design Phase that was completed in 2017. Subsequently, the City and the WRT Design Team have further developed the proposed design to the Design Development stage, or approximately a 50% complete design state.

As part of this design process, several intersection design alternatives were evaluated, including ones that considered both one-way and two-way operations on High Street. The City's Traffic Department and the MaineDOT's Planning Department provided their feedback on these alternatives and their preferred design is what is presented herein for the current one-way operations on High Street.

Private fundraising is underway for the redesign of Congress Square Park. Changes made to the Park are not part of this MaineDOT project, therefore it is not included in this review. In the meantime, the City, with financial assistance from MaineDOT, is proceeding with the intersection reconfiguration aspect of this overall project, as well as its adjoining streetscape, as Phase 1 of this redevelopment program.

### **Proposed Action**

The proposed action would modify the existing lane configuration on the Congress Street west-bound approach to create a dedicated west-bound, left-turn lane onto Free Street. The action would eliminate the Free Street slip-lane in front of the PMA and expand the PMA's plaza in the process. The sidewalk bump-out in front of Congress Square Park on Congress Street would be extended and the sidewalk pavement at the meeting of Congress Street and Free Street in front of the Hay Block would be reduced. The existing center median island on the Congress Street east-bound approach would be removed. A "table-top" design, which consists of a raised profile of granite pavers, will be established at Free Street to signal to vehicles that they are entering a heavily trafficked pedestrian area.

All four approaches at the Congress Street/High Street intersection would receive a 2" mill and fill treatment to provide a fresh paving surface for the new pavement markings. A total of seven new 12' wide paver crosswalks would be established within the intersection: four at the Congress Street/High Street intersection, one across Congress Street adjacent to the Hay Block, and two across Free Street. The action would provide new traffic signal mast arms, signal heads, and pedestrian equipment to meet ADA and the City's design standards. The action would also install new streetscape elements including landscaping, lighting, and pedestrian amenities within the limits of the intersection's sidewalks.

### **Federal Action**

Federal funding.

### **Definition of Area of Potential Effect (APE)**

The proposed project is located at the Congress Square intersection in Portland. A map is attached below that shows the APE.

# PORTLAND 24293.00

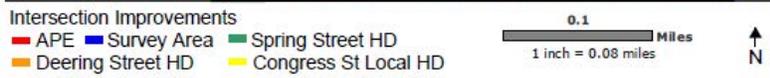
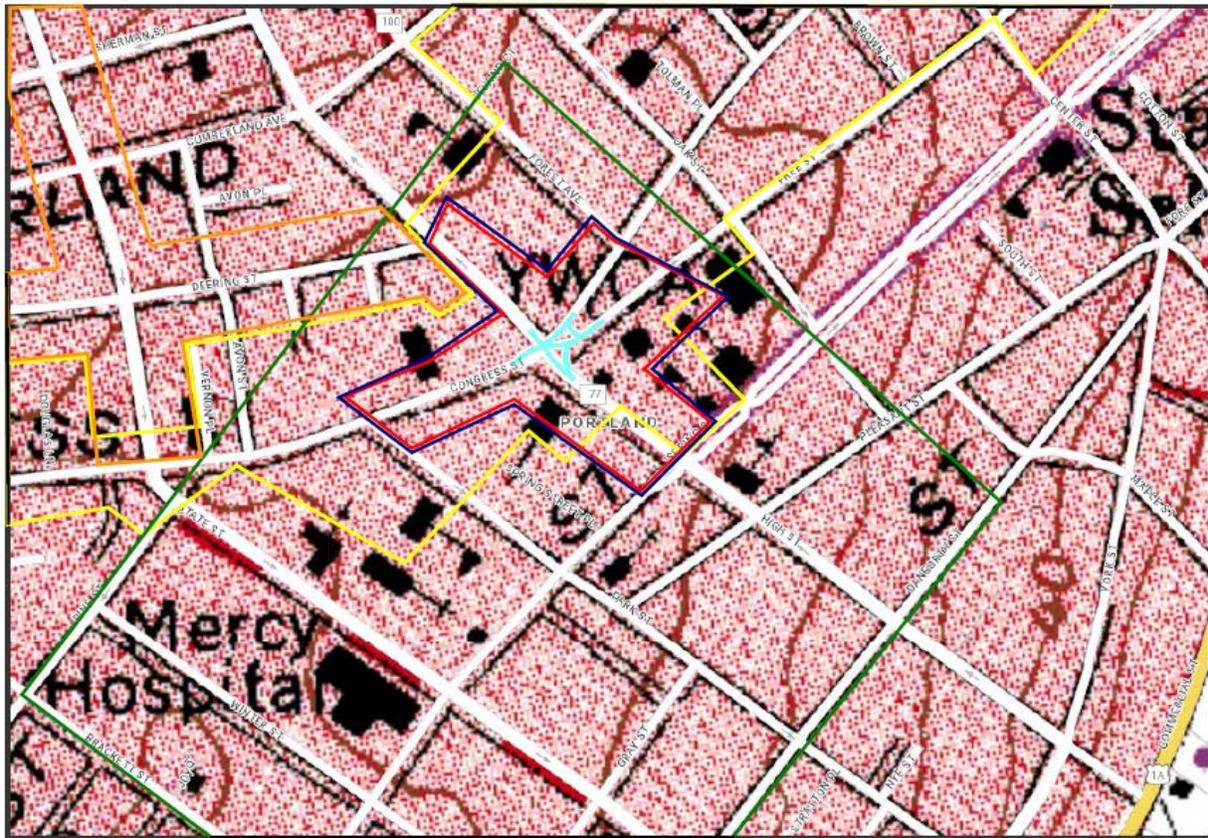
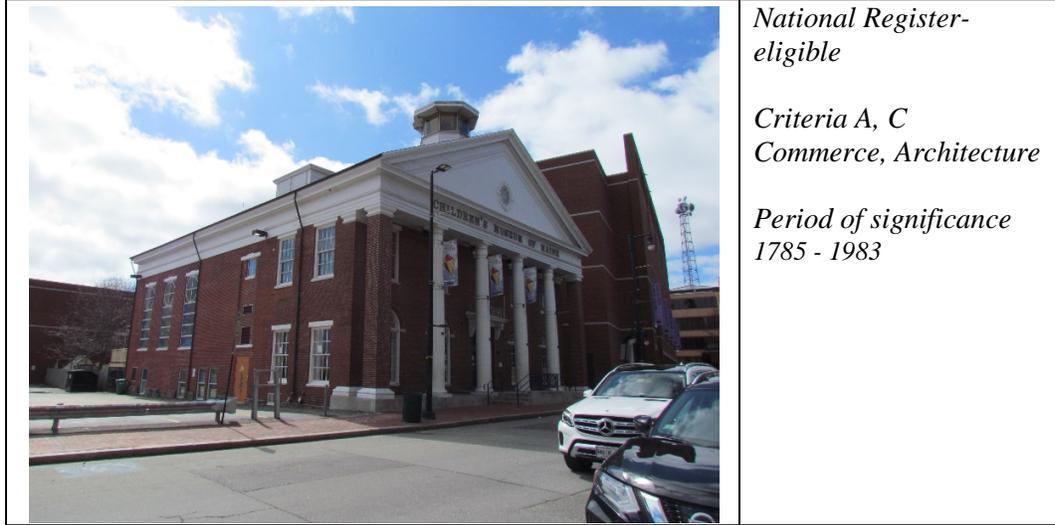


Figure 1. Portland 24293.00 Area of Potential Effect

## Historic Properties

### Congress Street Historic District (Various Owners)



As Portland's 'Main Street', the Congress Street Historic District represents the city's history. The district encompasses all of Congress Street between Franklin Street and Bramhall Square, as well as extending down Forest Avenue, Casco Street, Preble Street, Elm Street, Chestnut Street, and Myrtle Street to Cumberland Avenue, and Free Street between Center Street and Temple Street. It is made up of residential, commercial, municipal, religious, and cultural structures. Styles present include Federal, Greek Revival, Italianate, 19th/20th Century Revival, and Modern. It includes buildings designed by local prominent architects such as Francis H. Fassett, John Calvin Stevens, and George Burnham. The majority of the buildings are constructed of brick and range in height from 2 stories to more than 10 stories. Many of the storefronts have been altered and their fenestration and cladding materials changed while the upper floors remain intact. The streetscape is relatively uniform, with wide brick sidewalks and deciduous trees planted at regular intervals. There are 3 parks/public squares in the district. Despite some modern infill and changes to storefronts, the district is largely intact.

Spring Street Historic District (Various Owners)

	<p><i>National Register-listed</i></p> <p><i>Criteria A, C</i> <i>Social History,</i> <i>Architecture</i></p> <p><i>Period of significance</i> <i>1800 - 1899</i></p>
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The Spring Street Historic District contains many of Portland’s early residences as well as several churches. Forming a rectangle that extends roughly from Danforth Street to Brackett Street to Pine Street and blocks of the north side of Congress Street to Oak Street, it encompasses approximately 75 acres. It contains both narrow residential streets and wide commercial corridors. As Portland grew into an important economic center in the 19th century, many successful residents built their homes in the area. In addition to homes, churches, schools, and headquarters for social and institutional organizations were also built in the district. The area escaped the Great Fire of 1866 and therefore is one of the most intact areas of the peninsula. Styles represented include Federal, Greek Revival, Gothic Revival, and Italianate. There has been some unsympathetic infill added but overall the district retains a cohesive look and feel.

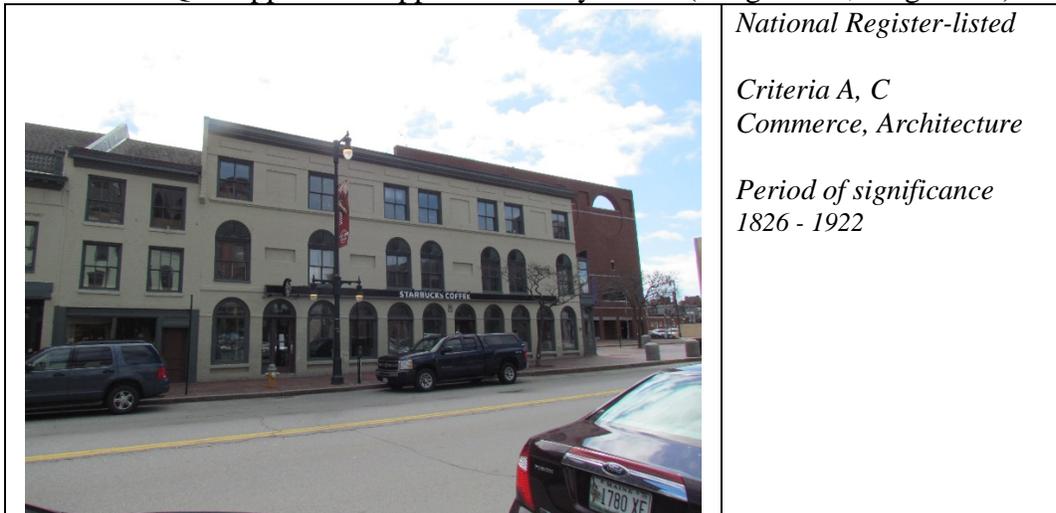
Deering Street Historic District (Various Owners)

	<p><i>National Register-listed</i></p> <p><i>Criteria A, C</i> <i>Exploration/Settlement,</i> <i>Architecture</i></p> <p><i>Period of significance</i> <i>1859 - 1900</i></p>
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The Deering Street district encompasses Deering Avenue between Mellen Street and High Street as well as State Street between Park Avenue and Congress Street. The houses are 2 or 3 stories in height and set back from the street with large trees providing a buffer between the sidewalk and the street. The buildings are placed close to one another, with just a driveway dividing them. The

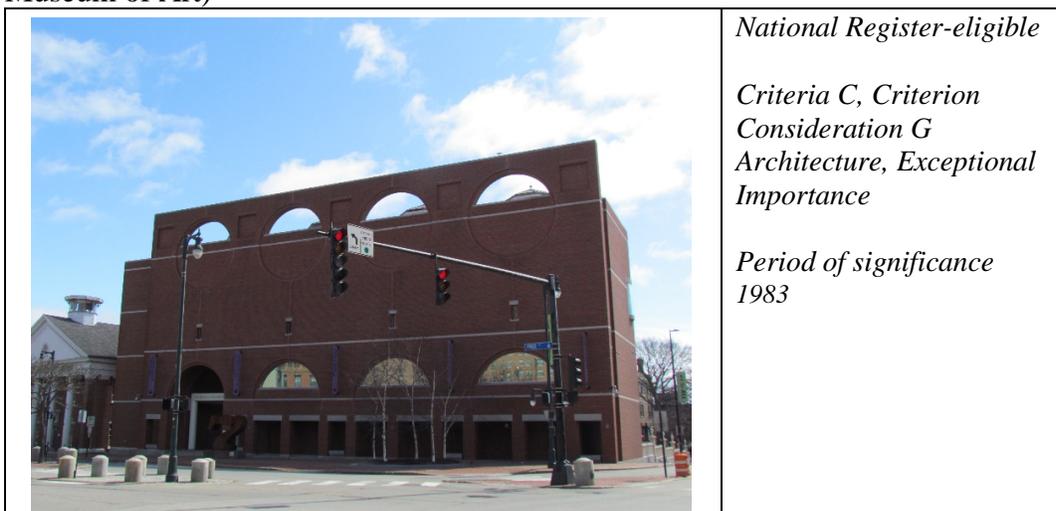
area was initially part of the large land holdings of the Nathaniel Deering family and used as a cow pasture. As Portland grew, residential development spread to the adjacent Longfellow Square and Deering eventually divided his land into house lots. The first house was constructed in 1859. Styles represented include Italianate, Romanesque Revival, Gothic Revival, Queen Anne, Second Empire, and 19th/20th Century Revival. The district includes designs by local architects Francis H. Fassett and John Calvin Stevens.

The Charles Q. Clapp Block/Upper H. H. Hay Block (Fitzgibbons, Virginia S.)



Located at the intersection of Congress, Free, and High Streets, the 3-story Charles Q. Clapp Block/Upper H. H. Hay Block is almost triangular in shape due to the site. The main entrance to the building is located in the narrow elevation facing the intersection. It was built by Charles Q. Clapp, a prominent Portland builder and land speculator, as a two-story building in 1826. The druggist H. H. Hay opened a pharmacy in the building in 1841. The third level was added in 1922 and designed by John Calvin Stevens.

The Portland Museum of Art's Charles Shipman Payson Building (Portland Museum of Art)



The Portland Museum of Art's Charles Shipman Payson Building is clad in red brick with granite belt courses, sills, and lintels. It is four stories at the front, decreasing in height to become one

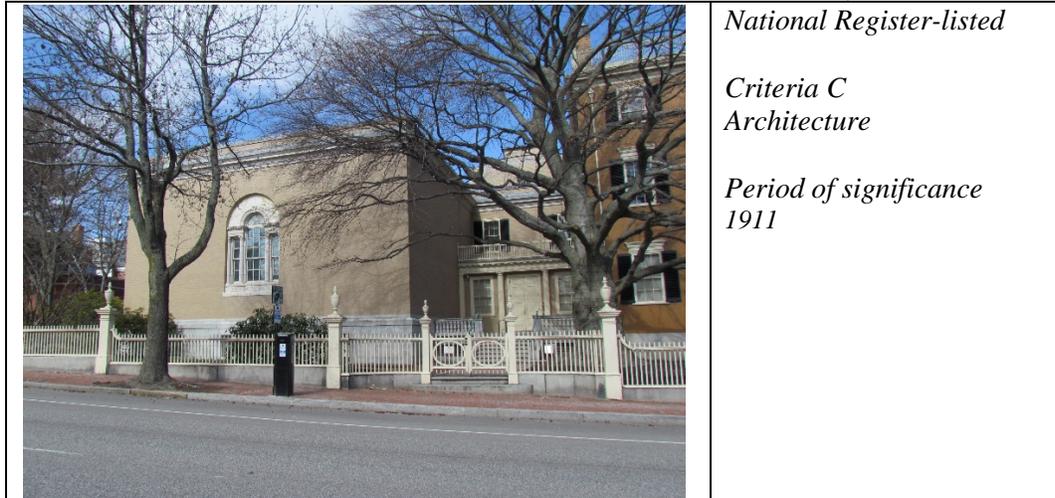
story at the rear. The façade is made up of four bays defined by large semi-circular openings. The first floor contains a recessed arcaded area with wide brick pillars. It was designed by Henry Nichols Cobb, FAIA of I. M. Pei and Partners and completed in 1983. Cobb was a founding partner of the Pei, Cobb, Freed & Partners (currently Pei Cobb Freed & Partners), which has won numerous awards since its founding in 1955. He designed many notable buildings, including the John Hancock Tower in Boston. The Payson Building is an excellent example of a Contemporary style building that is sensitive to its historic setting. The use of historic materials (brick) and massing in a scale similar to the surrounding buildings allows the Payson Building to contribute rather than detract from its historic surroundings. Cobb also referenced the structure that previously stood at that corner by incorporating large semicircular elements into the façade. The Payson Building received a National Honor Award from the American Institute of Architects in 1985.

The McLellan-Sweat Mansion (Portland Museum of Art)

	<p><i>National Register-listed, National Historic Landmark</i></p> <p><i>Criteria C Architecture</i></p> <p><i>Period of significance 1800</i></p>
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The McLellan-Sweat Mansion is a three-story rectangular brick building laid in Flemish bond set on a granite foundation. The façade is symmetrical with a central entry. The entry is flanked by sidelights and topped by a fanlight. The door is sheltered by a semicircular porch supported by Doric columns and topped by a balustrade. The first floor windows are 9/9, the second floor are 6/6, and the third floor are 6/3. The first and second floor windows are topped by molding. The central second floor window is a Palladian window. All windows are double hung wood windows. The cornice is supported by delicate brackets. A balustrade lines the shallow hip roof. The building was built in 1800 for Hugh McLellan, a wealthy Portland merchant from designs by John Kimball. After passing to other owners, it was purchased by Lorenzo de Medici Sweat, a Maine Congressman, in 1880. His widow bequeathed the house to the Portland Society of Art (now the Portland Museum of Art).

## The L. D. M. Sweat Memorial Galleries (Portland Museum of Art)



The L. D. M. Sweat Memorial Galleries is a large blonde brick addition that is located at the rear of the McLellan-Sweat Mansion and connected by a two-story ell. The addition was designed by prominent Maine architect John Calvin Stevens in the Beaux Arts style to compliment the Federal style of the McLellan-Sweat Mansion. The building was named after Lorenzo de Medici Sweat, the husband of Margaret Jane Mussey Sweat, who gifted the McLellan-Sweat Mansion to the Portland Society of Art (now the Portland Museum of Art). It opened to the public in 1911.

### Archeological Resources

No archaeological resources would be affected by the proposed action.

### **Impacts to Properties**

*Note – due to the overlap of Congress Street Historic District and Spring Street Historic District at the project location, impacts to both districts will be discussed together.*

#### Congress Street Historic District (Various Owners)

*National Register-eligible*

*Criteria A, C, Commerce, Architecture*

#### Spring Street Historic District (Various Owners)

*National Register-listed*

*Criteria A, C, Social History, Architecture*

The proposed action would result in **No Adverse Effect** to the National Register-eligible Congress Street Historic District and the National Register-listed Spring Street Historic District. The proposed action would modify the existing lane configuration on the Congress Street west-bound approach to create a dedicated west-bound, left-turn lane onto Free Street. The action would eliminate the Free Street slip-lane in front of the PMA and expand the PMA's plaza in the process. The sidewalk bump-out in front of Congress Square Park on Congress Street would be extended and the sidewalk at the meeting of Congress Street and Free Street in front of the Hay Block would be reduced. The existing center median island on the Congress Street east-bound approach would be removed. A "table-top" design, which consists of a raised profile of

granite pavers, will be established at Free Street to signal to vehicles that they are entering a heavily trafficked pedestrian area. These actions would not significantly diminish any of the Districts' aspects of integrity.

For many years this area was defined by the large intersection where Congress Street, High Street, and Free Street met. It originally accommodated horse-drawn carriages, then streetcars, and finally automobiles. While the configuration of the intersection has not significantly changed, it did integrate islands, sidewalk bump-outs, and extensions overtime to safely manage the changing volume of traffic and pedestrians. Historic photos and aerials indicate that sometime c. 1970 sidewalk bump-outs were added in front of the Hay Block and Congress Square Park. Islands were added within the intersection most likely around the same time. Historic photos also indicate that the wide curvilinear sidewalk in front of the Portland Museum of Art was present since the early-to-mid 1900s. The proposed action would not significantly change the setting or feeling at the intersection because the changes would modify the bump-outs and sidewalk extensions that already exist at the location. While the addition of the “table-top” to the entrance of Free Street would introduce granite pavers and add an additional bump-out, it would not adversely affect either district because bump-outs and granite material are already present at the intersection and throughout the Congress Street corridor. Furthermore, the “table-top” treatment would be a minimal change to the visual character of the intersection.

The proposed action would integrate various materials, including granite and brick pavers at the sidewalks and crosswalks. The existing sidewalks at the High Street and Congress Street corners would be replaced in-kind with brick pavers and the granite pavers would be installed adjacent to the concrete tip-downs. Brick pavers would replace the sidewalks adjacent to the Hay Block and the PMA and granite pavers would be added. The sidewalk adjacent to the Congress Square Park would be replaced with light and dark gray granite pavers. In-kind granite curb would be used throughout. Granite planter curbs will be located along the perimeter of the sidewalks and deciduous trees will be planted within, along with various perennials. These actions would not significantly diminish any of the Congress Street Historic District's or the Spring Street Historic District's aspects of integrity. Granite pavers, planters, and bollards, as well as deciduous trees, are currently present at the Congress Square intersection. Granite pavers would be introduced in the crosswalks and would not compromise the character of the intersection, as historic photos indicate that granite pavers have been used within the roadway before. The introduction of simple granite pavers at the sidewalks would be compatible with the current setting. New curb planters with deciduous trees at the sidewalks would be installed so that they would not obstruct the view of the adjacent historic buildings.

Updated traffic signals equipment would also be installed at the Congress Square intersection and would not significantly diminish any of the Congress Street Historic District's or the Spring Street Historic District's aspects of integrity. Three black mast arms (25', 40', and 45') would be installed at the intersection and would replace the existing mast arms at that location. Several black 8' pedestal poles would also be installed at the intersection to replace the existing pedestal poles and mast arm-mounted pedestrian countdown signal heads at that location. The pedestal poles would be placed adjacent to the sidewalk tip-downs in order to comply with current ADA standards. A black L48" x W36" x H48" ground-mounted controller cabinet would also be installed. Street lighting would not be installed as part of this project. These actions would not

significantly diminish the setting of the districts, as equipment of similar size, color, and appearance already exist at the location.

Deering Street Historic District (Various Owners)

*National Register-listed*

*Criteria A, C, Exploration/Settlement, Architecture*

The proposed action would result in **No Adverse Effect** to the National Register-listed Deering Street Historic District. The proposed action (as detailed previously) would not impact the District as the majority of the work takes place outside of the District's boundaries. The sidewalk adjacent to the Immanuel Baptist Church, a contributing resource to the District, would be reconstructed using in-kind brick sidewalk and granite curb. This would not significantly diminish any aspects of integrity.

The Charles Q. Clapp Block/Upper H. H. Hay Block (Fitzgibbons, Virginia S.)

*National Register-listed*

*Criteria A, C, Commerce, Architecture*

The proposed action would result in **No Adverse Effect** to the National Register-listed Charles Q. Clapp Block/Upper H. H. Hay Block. The proposed action (as previously detailed) would reduce the sidewalk at the meeting of Congress Street and Free Street in front of the Hay Block. The existing center median island on the Congress Street east-bound approach that is adjacent to the Hay Block would be also be removed. The sidewalk adjacent to the building would be replaced in-kind with brick. Granite pavers would be added to the sidewalk, along with granite curb planters on either side of the block adjacent to Free Street and Congress Street. A crosswalk would be established adjacent to the building and an 8' black pedestrian pedestal pole would be installed at the tipdown. Although the sidewalk at the southern-facing point of the building would be reduced, there would still be adequate pedestrian space between the roadway and building. The addition of granite pavers would present a minimal visual change that would still compatible with the setting. The action would remove the existing trees and replace them with one deciduous tree on either side of the building that would avoid obstructing the view of the building. These actions would not significantly diminish any of the Hay Block's aspects of integrity.

The Portland Museum of Art's Charles Shipman Payson Building (Portland Museum of Art)

*National Register-eligible*

*Criteria C, Criterion Consideration G, Architecture, Exceptional Importance*

The proposed action would result in **No Adverse Effect** to the National Register-eligible Portland Museum of Art's Charles Shipman Payson Building. The proposed action (as previously detailed) would extend the plaza in front of the PMA with brick pavers to match the existing and add granite pavers. Although this action would alter the existing footprint of the sidewalk in front of the building and add new material, it does not represent a change so substantial as to diminish the architectural significance of the PMA. Effects would be minimized by retaining the existing brick sidewalk in front of the building, as well as the existing sculpture and circular granite landscape planter. Granite curb planters would be added to the plaza extension adjacent to the roadway, with one deciduous tree located in the planter adjacent to High Street and two deciduous trees located in the planter adjacent to Free Street. An additional planter area would be installed in the brick paver portion of the sidewalk extension. It would contain one deciduous tree, along with a precast concrete bench and concrete blocks. Although these actions would

introduce new elements to the setting of the PMA, these additions are in character with the overall setting. The addition of the deciduous trees would not compromise the integrity of the building, as similar trees already exist at the location. Furthermore, the tree to be planted within the plaza would be a grey birch tree that would complement the existing birch trees located in the plaza. The new planter would also be located at a distance so that the new tree would not obstruct the view of the building. A 40' black mast arm, along with an 8' black pedestrian pedestal pole, would be installed between the two crosswalks. This action would not diminish the integrity of the PMA, as traffic signal equipment of similar size and appearance currently exists adjacent to the building.

#### The McLellan-Sweat Mansion (Portland Museum of Art)

*National Register-listed, National Historic Landmark*

*Criteria C, Architecture*

The proposed action would result in **No Historic Properties Affected** to the National Register-listed McLellan-Sweat Mansion. The proposed action takes place just north of the McLellan-Sweat Mansion and would make no changes adjacent to the building.

#### The L. D. M. Sweat Memorial Galleries (Portland Museum of Art)

*National Register-listed*

*Criteria C, Architecture*

The proposed action would result in **No Adverse Effect** to the National Register-listed L. D. M. Sweat Memorial Galleries. The proposed action would replace the existing sidewalk adjacent to the Galleries with in-kind brick pavers and granite curb. This action would not significantly diminish any of the Galleries' aspects of integrity.

### **Avoidance and Minimization Efforts**

Avoidance and minimization efforts have been implemented throughout the planning process and an overall effort has been made to enhance the project area and improve safety through changes that are sensitive to the historic character of the location. This includes the following:

- Granite pavers were scaled back in order maintain the existing appearance of the brick sidewalks that are immediately adjacent to the historic buildings found at the intersection. The color options for the granite pavers were minimized to light and dark gray and various paver patterns were eliminated in favor of the simplified herringbone pattern that differentiates itself from the existing brick but does not deviate too strongly. Granite curb would be used throughout to match the existing curb material.
- Replacement traffic signal equipment that closely matches the existing equipment in size, color, and appearance would be used in order to minimize effects to the Districts and historic buildings.
- Existing trees would be replaced with trees of similar appearance and size. They would also be situated so that they would not obstruct the view of historic buildings found within the project area.
- The use of street furniture was kept to a minimum.

Greater Portland Landmarks and the City of Portland's Historic Preservation Board have been involved in the planning process of the Congress Square Park Redesign project that includes improvements to the Congress Square intersection. A priority was placed on adhering to the City

of Portland's Historic Resources Design Manual with a specific focus on meeting the standards for appropriate streetscape and pedestrian improvements (see attachment). Several design components were revised in order to meet these standards, resulting in the Historic Preservation Board approving the design on November 7, 2018.

## **Dismissed Alternatives**

### No Build

The No Build alternative takes no action and does not meet the purpose and need of the project and was therefore removed from further consideration.

### Alternative 1

This alternative examined several intersection redesign options that considered both one-way and two-way operations on High Street. However, for various reasons these options did not meet the purpose and need of the project and were therefore removed from further consideration.

### Alternative 2

This alternative examined how the Congress/High intersection configuration would need to change geometrically to accommodate two-way traffic on High Street. The City felt it was prudent to examine this geometry so that major streetscape elements such traffic signal mast arm foundations could be located such that they would not need to be moved in the future. However, there are no plans at present to initiate this conversion and this alternative was therefore dismissed from further consideration.

## **Public Involvement**

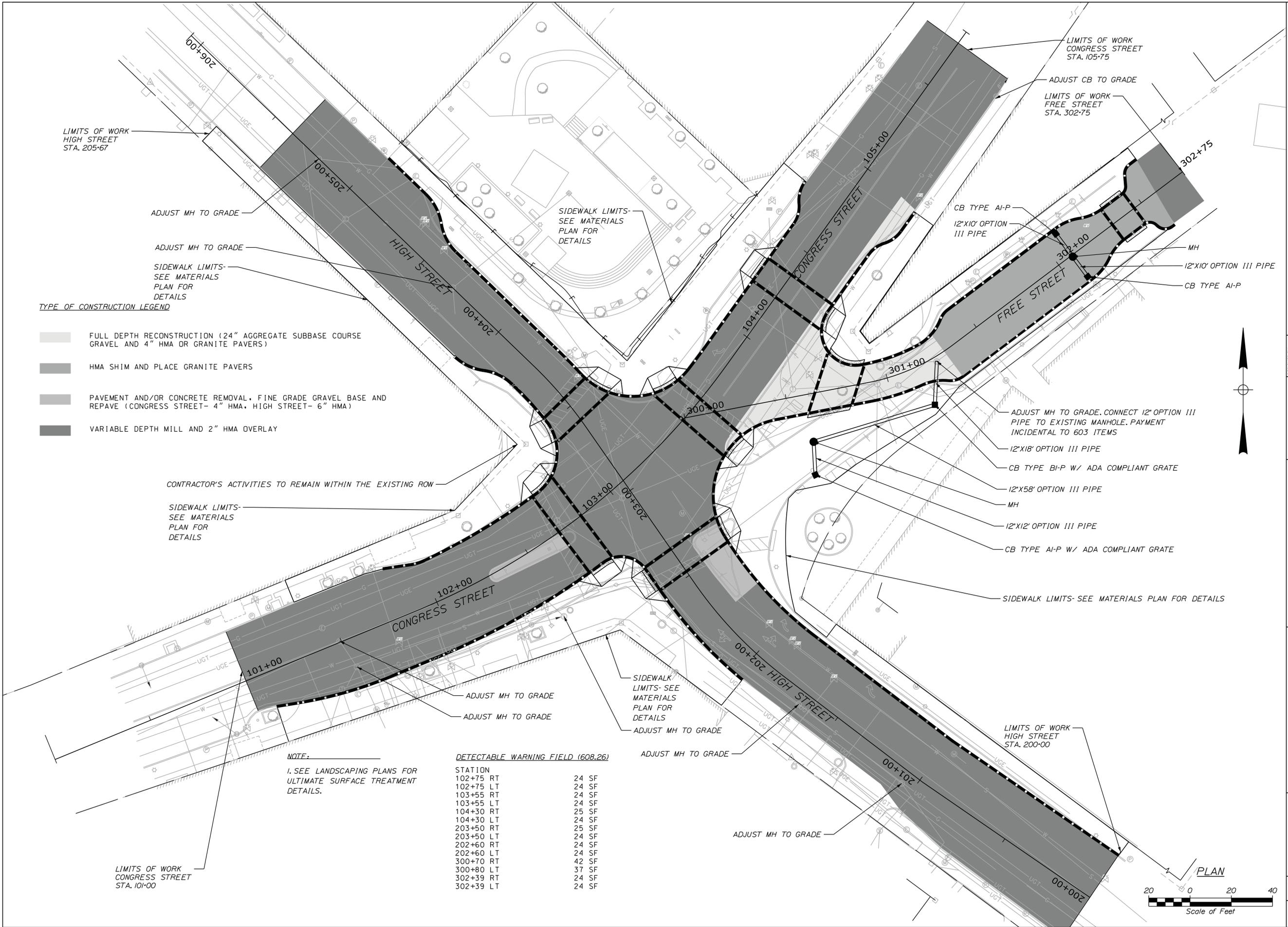
MaineDOT contacted the four federally recognized Native American tribes in Maine. The Houlton Band of Maliseets replied with no concern about the undertaking.

The City of Portland, along with applicable historical societies, was notified of the initiation of the Section 106 review and asked to provide comments and information regarding historic resources in the project area. Julie Larry of Greater Portland Landmarks (GPL) requested and received official Section 106 consulting party status. Ms. Larry also provided comments on historic resources within the project area, as well as some background on GPL's involvement with the planning process (see attachment). Comments from Ms. Larry include "We don't anticipate any effects on any building, but the public space and its walkability/safety for pedestrians is important for the vitality of the retail and commercial uses that keep the buildings active and in good repair."

The public comment period regarding effects to historic resources is ongoing.

## **Attachments**

- City of Portland, Public Services Department, Contract Drawings, Congress Square Improvements, WIN 24293.00, October 7, 2020.
- Kirk F. Mohny, MHPC, to Julie Senk, MaineDOT, July 9, 2020
- City of Portland's Historic Resources Design Manual – Streetscape and Pedestrian Improvements
- Section 106 consulting party request from Greater Portland Landmarks,



LIMITS OF WORK  
HIGH STREET  
STA. 205+67

ADJUST MH TO GRADE

ADJUST MH TO GRADE

SIDEWALK LIMITS-  
SEE MATERIALS  
PLAN FOR  
DETAILS

TYPE OF CONSTRUCTION LEGEND

- FULL DEPTH RECONSTRUCTION (24" AGGREGATE SUBBASE COURSE GRAVEL AND 4" HMA OR GRANITE PAVERS)
- HMA SHIM AND PLACE GRANITE PAVERS
- PAVEMENT AND/OR CONCRETE REMOVAL, FINE GRADE GRAVEL BASE AND REPAVE (CONGRESS STREET- 4" HMA, HIGH STREET- 6" HMA)
- VARIABLE DEPTH MILL AND 2" HMA OVERLAY

CONTRACTOR'S ACTIVITIES TO REMAIN WITHIN THE EXISTING ROW

SIDEWALK LIMITS-  
SEE MATERIALS  
PLAN FOR  
DETAILS

SIDEWALK LIMITS-  
SEE MATERIALS  
PLAN FOR  
DETAILS

ADJUST MH TO GRADE

ADJUST MH TO GRADE

SIDEWALK  
LIMITS- SEE  
MATERIALS  
PLAN FOR  
DETAILS

ADJUST MH TO GRADE

NOTE:  
1. SEE LANDSCAPING PLANS FOR  
ULTIMATE SURFACE TREATMENT  
DETAILS.

DETECTABLE WARNING FIELD (608.26)

STATION	RT	LT	SF
102+75	RT	LT	24
102+75	RT	LT	24
103+55	RT	LT	24
103+55	RT	LT	24
104+30	RT	LT	25
104+30	RT	LT	24
203+50	RT	LT	25
203+50	RT	LT	24
202+60	RT	LT	24
202+60	RT	LT	24
300+70	RT	LT	42
300+80	RT	LT	37
302+39	RT	LT	24
302+39	RT	LT	24

LIMITS OF WORK  
CONGRESS STREET  
STA. 101+00

LIMITS OF WORK  
CONGRESS STREET  
STA. 105+75

ADJUST CB TO GRADE

LIMITS OF WORK  
FREE STREET  
STA. 302+75

CB TYPE AI-P  
12'X10' OPTION  
III PIPE

MH

12'X10' OPTION III PIPE

CB TYPE AI-P

ADJUST MH TO GRADE. CONNECT 12" OPTION III  
PIPE TO EXISTING MANHOLE. PAYMENT  
INCIDENTAL TO 603 ITEMS

12'X18' OPTION III PIPE

CB TYPE BI-P W/ ADA COMPLIANT GRATE

12'X58' OPTION III PIPE

MH

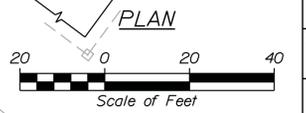
12'X12' OPTION III PIPE

CB TYPE AI-P W/ ADA COMPLIANT GRATE

SIDEWALK LIMITS-SEE MATERIALS PLAN FOR DETAILS

LIMITS OF WORK  
HIGH STREET  
STA. 200+00

ADJUST MH TO GRADE



LDD PROJECT NAME:  
CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

REFERENCES:

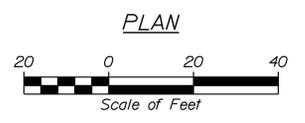
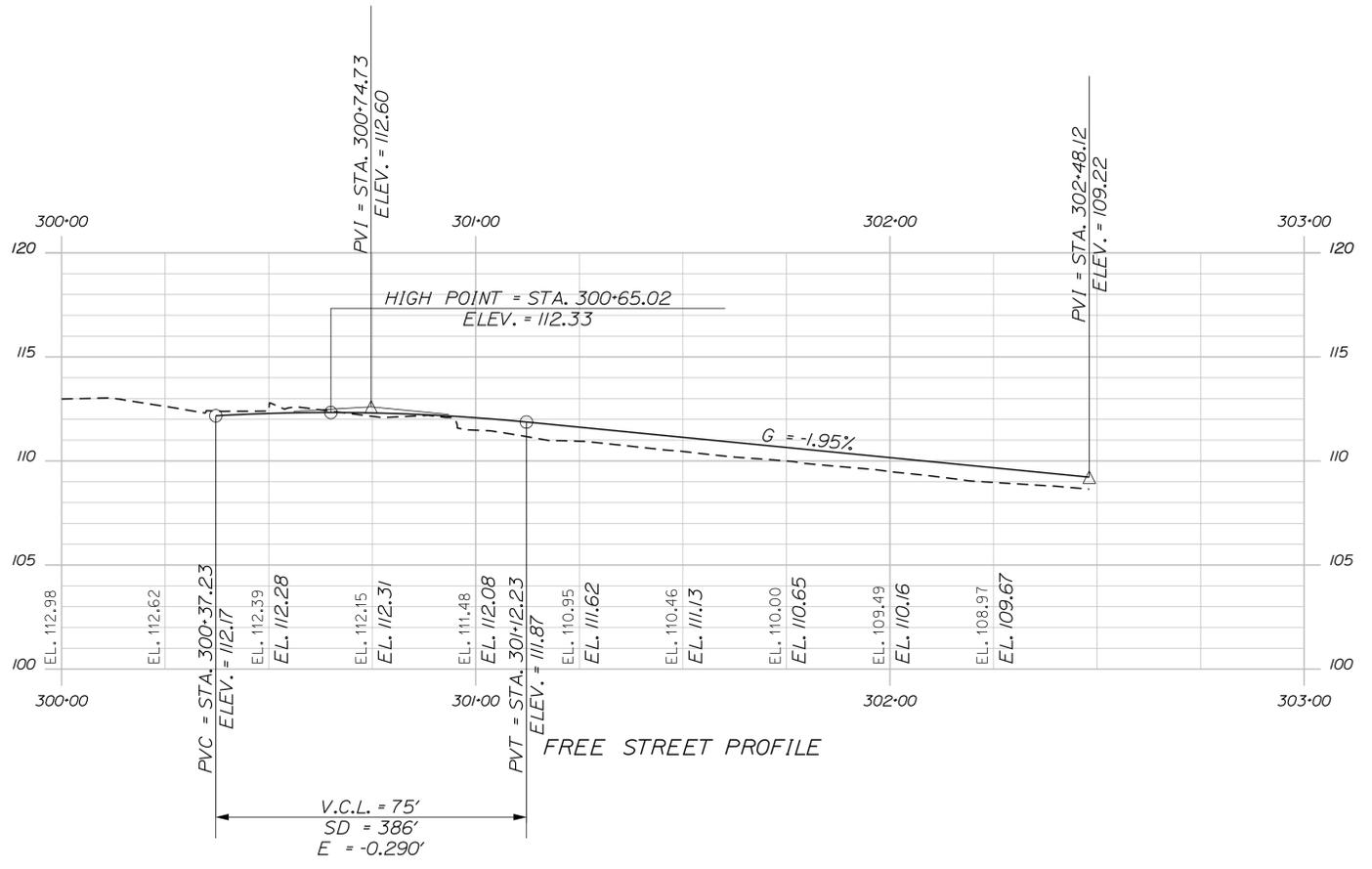
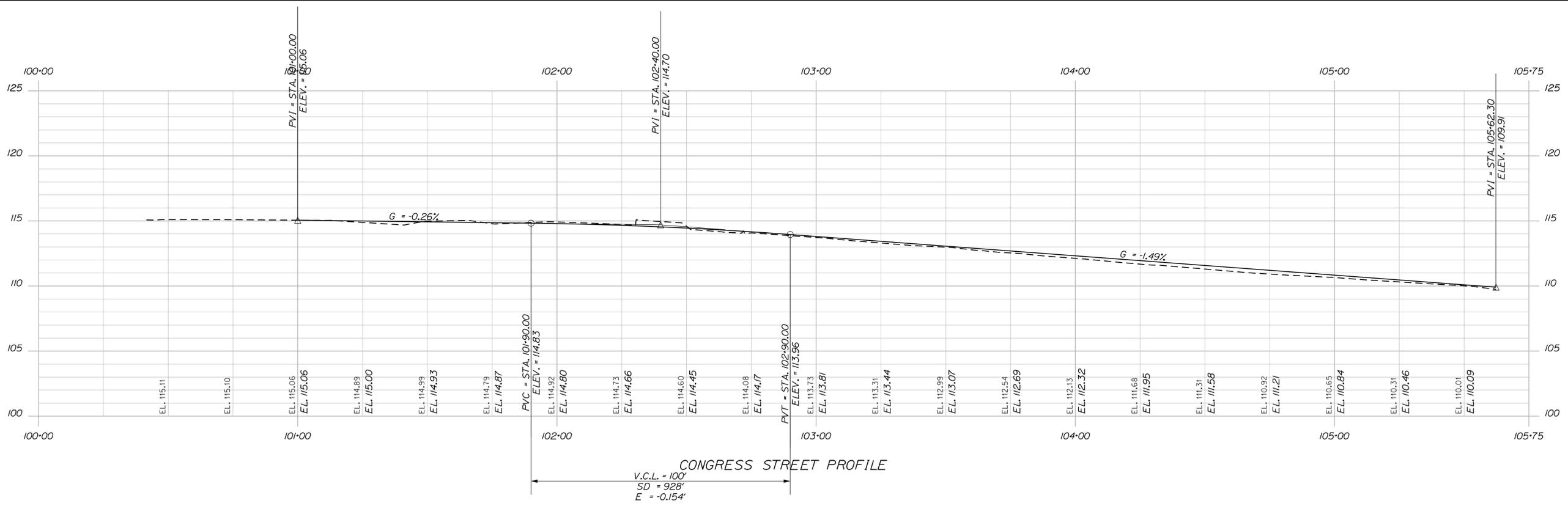
DESIGNED BY:	CHK:	DATE:
SKK	SKK	10-02-2020
DRAWN BY:	CHK:	DATE:
SKK	SKK	10-02-2020
CHECKED BY:	DATE:	SCALE:
SSS	10-02-2020	1"=20'

CONGRESS SQUARE  
IMPROVEMENTS  
GENERAL PLAN

CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION



SHEET #  
6 OF 38  
PLAN NUMBER



LOD PROJECT NAME:  
CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

REFERENCES:

DESIGNED BY:	SCK
DRAWN BY:	SCK
CHECKED BY:	SSS
SCALE:	1"=20'
DATE:	10-02-2020

CONGRESS SQUARE  
IMPROVEMENTS  
PROFILES

CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION



**STRUCTURES LIST**

STRUCTURE	DESCRIPTION	STA O/S	FOUNDATION
(A-C)	CONTROLLER CABINET	104-06.91 21.39 LT	L48"XW36"XH48"
(A-M)	25' MAST ARM	103-99.31 19.64 LT	TBD
(A-PI)	8' PEDESTAL POLE	104-23.36 22.05 LT	24" DIA.
(A-P2)	8' PEDESTAL POLE	103-66.18 21.45 LT	24" DIA.
(A-P3)	8' PEDESTAL POLE	103-44.51 35.77 LT	24" DIA.
(B-PI)	8' PEDESTAL POLE	104-24.47 36.80 RT	24" DIA.
(C-M)	40' MAST ARM	103-35.64 43.92 RT	TBD
(C-PI)	8' PEDESTAL POLE	103-49.92 35.44 RT	24" DIA.
(D-PI)	8' PEDESTAL POLE	102-81.65 38.74 RT	24" DIA.
(D-P2)	8' PEDESTAL POLE	102-68.90 33.08 RT	24" DIA.
(E-M)	45' MAST ARM	102-88.02 52.09 LT	TBD
(E-PI)	8' PEDESTAL POLE	102-66.83 29.96 LT	24" DIA.
(E-P2)	8' PEDESTAL POLE	102-85.36 36.97 LT	24" DIA.

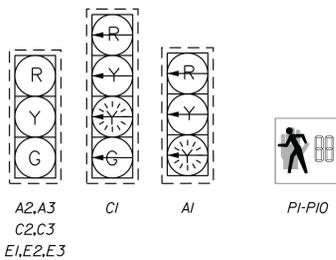
**CONDUIT SCHEDULE**

FROM	TO	TYPE	LENGTH (FT)	NO. OF RUNS
A-C	STUB	3" PVC SCH. 80	5	2
A-C	A-J1	3" PVC SCH. 80	7	3
A-C	A-J2	3" PVC SCH. 80	12	2
A-J2	A-PI	3" PVC SCH. 80	4	1
A-J2	B-PI	3" PVC SCH. 80	60	1
A-J1	A-J3	3" PVC SCH. 80	48	3
A-J3	A-P3	3" PVC SCH. 80	8	1
A-J3	A-P2	3" PVC SCH. 80	16	1
A-J3	C-J1	3" PVC SCH. 80	62	2
C-J1	C-PI	3" PVC SCH. 80	6	1
C-J1	C-M	3" PVC SCH. 80	11	1
A-J3	E-J1	3" PVC SCH. 80	60	3
E-J1	E-M	3" PVC SCH. 80	18	1
E-J1	E-J2	3" PVC SCH. 80	33	2
E-J2	E-J3	3" PVC SCH. 80	75	2
E-J3	E-J4	3" PVC SCH. 80	113	2
E-J1	E-PI	3" PVC SCH. 80	13	1
E-J1	D-J1	3" PVC SCH. 80	67	2
D-J1	D-P2	3" PVC SCH. 80	15	1
D-J1	D-PI	3" PVC SCH. 80	4	1
A-C	A-J4	3" METALLIC	55	1
A-J4	EXMH	3" METALLIC	130	1

**NOTE:**

- ONE CONDUIT ON DUAL/TRI RUNS TO REMAIN EMPTY FOR FUTURE LIGHTING WIRING
- ALL CONDUITS WHICH ARE TO REMAIN EMPTY SHALL HAVE A NYLON PULLING STRING INSTALLED.
- ALL STUBBED CONDUIT RUNS SHALL BE CAPPED AND LOCATED IN THE FIELD.
- WHERE MULTIPLE CONDUITS ARE INSTALLED IN SHARED TRENCH, CONDUIT SHALL BE SEPARATED BY A MIN. OF 3"

**PROPOSED INDICATIONS**

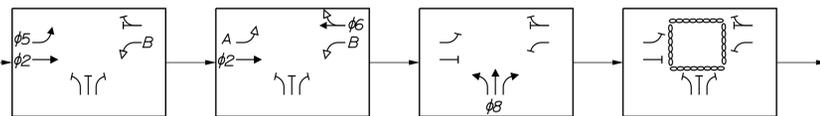


**NOTE:**

ALL VEHICLE INDICATIONS SHALL BE 12" LIGHT EMITTING DIODES (LED) WITH 5" LOUVERED BACKPLATES WITH 3" RETROREFLECTIVE BORDERS

**PREFERENTIAL PHASE SEQUENCE**

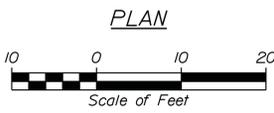
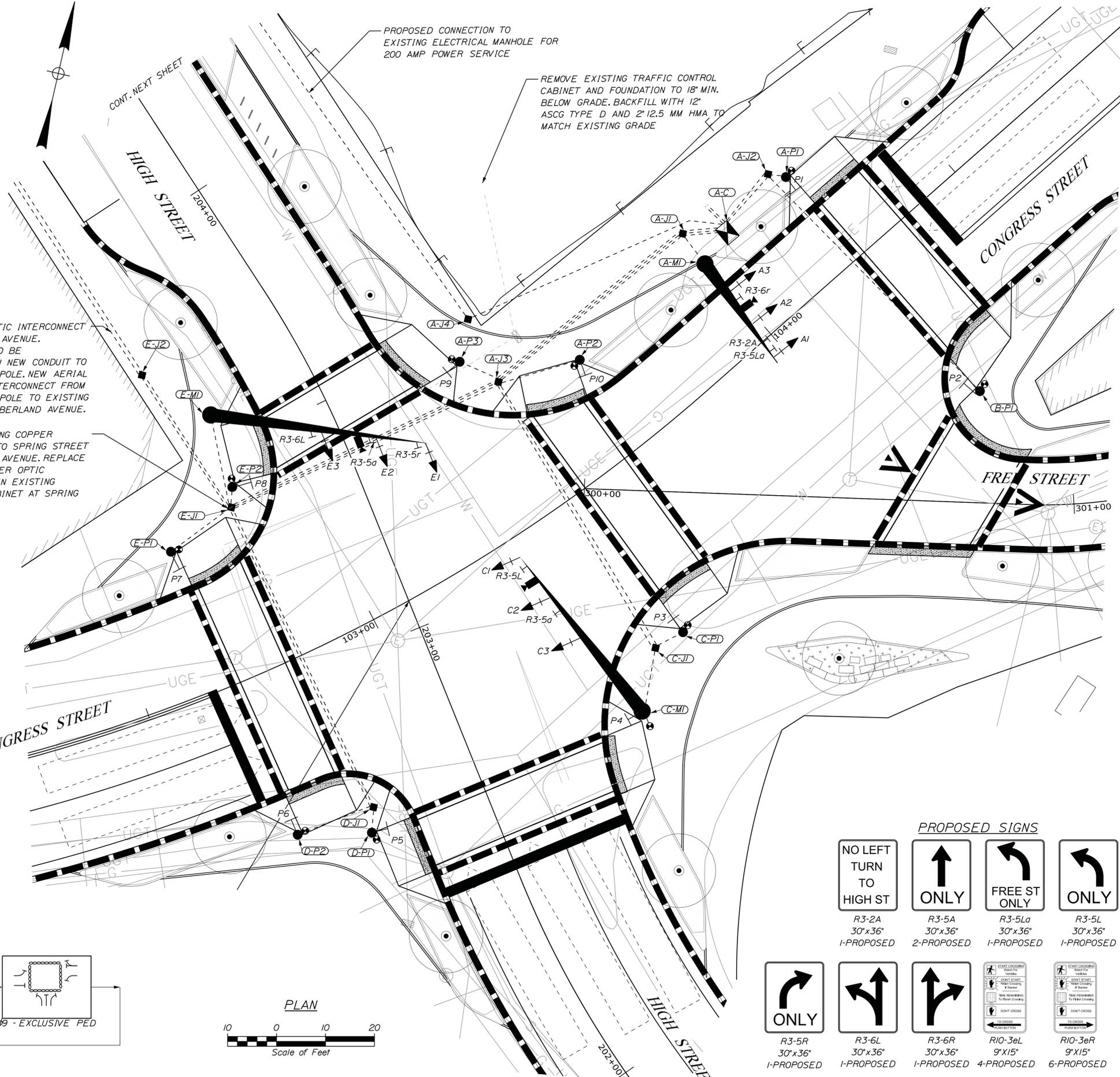
NEMA RING AND BARRIER DIAGRAM



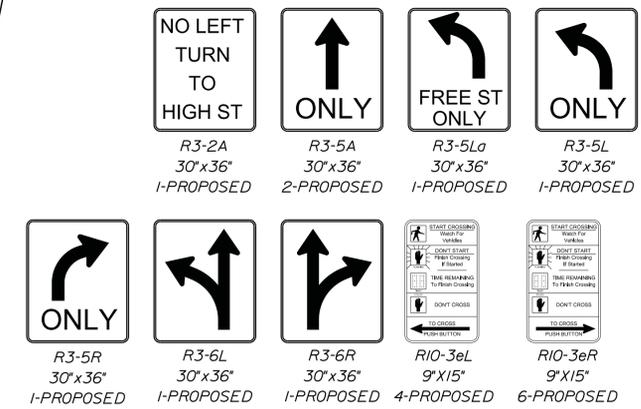
**OVERLAP PHASING**

OVL A = φ6  
OVL B = φ2

NOTE: OVERLAPS SHALL BE PROGRAMMED FOR FLASHING YELLOW ARROWS.



**PROPOSED SIGNS**



LOD PROJECT NAME:  
CONGRESS SQUARE IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

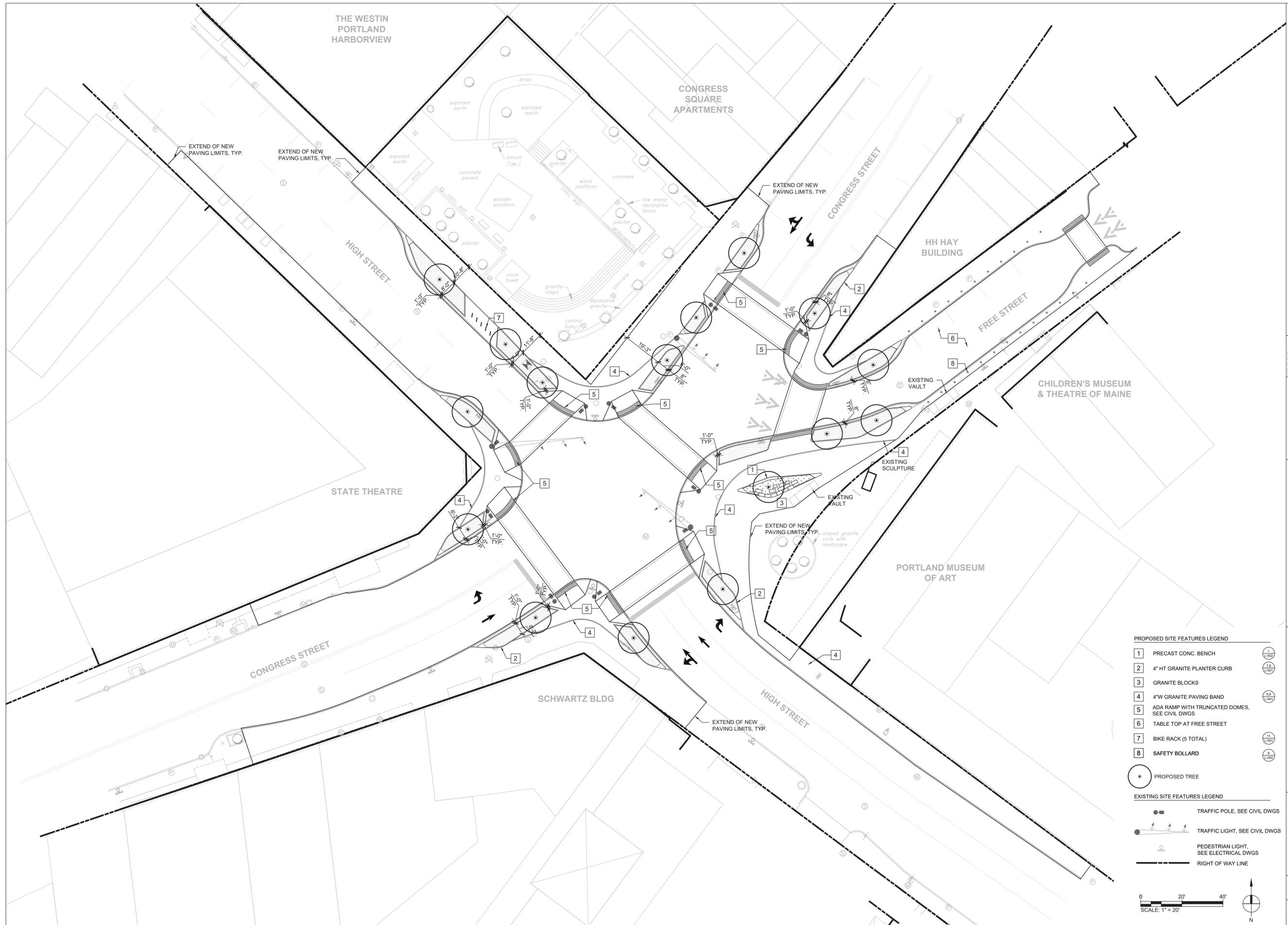
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DRAWN BY:	DHC
CHECKED BY:	SSS
SCALE:	1" = 10'
DATE:	08-31-2020

CONGRESS SQUARE IMPROVEMENTS  
TRAFFIC SIGNAL PLAN

CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION





**REFERENCES:**

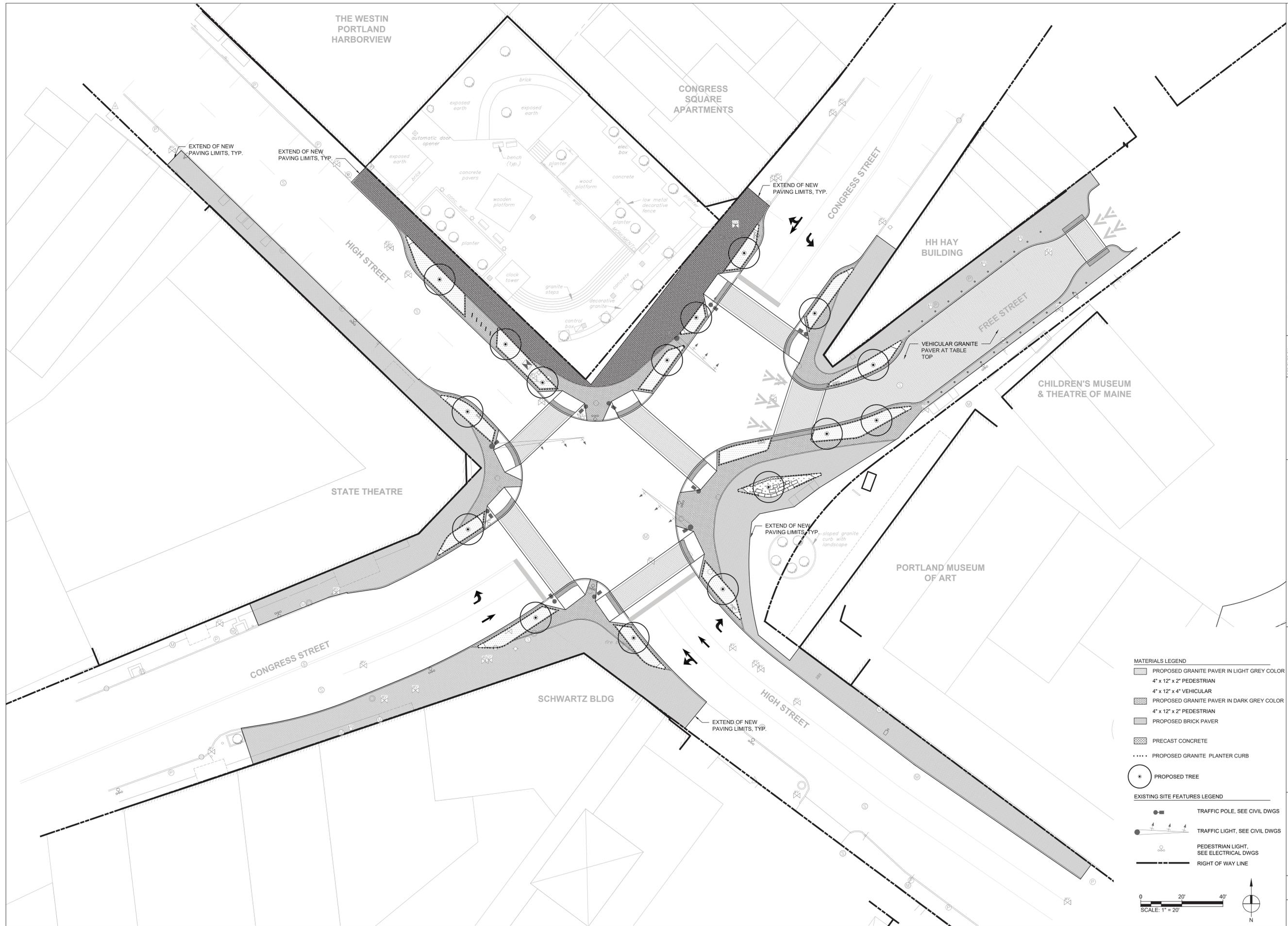
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CHECKED BY:	MC
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DATE:	08-27-2020

**CONGRESS SQUARE IMPROVEMENTS**  
STREETSCAPE AND LANDSCAPING LAYOUT PLAN

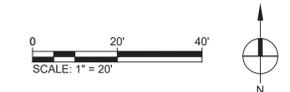
**CITY OF PORTLAND, MAINE**  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION

**LDD PROJECT NAME:** CONGRESS SQUARE IMPROVEMENTS  
**DRAWING NAME:**  
**FIELD BOOK USED:** N/A

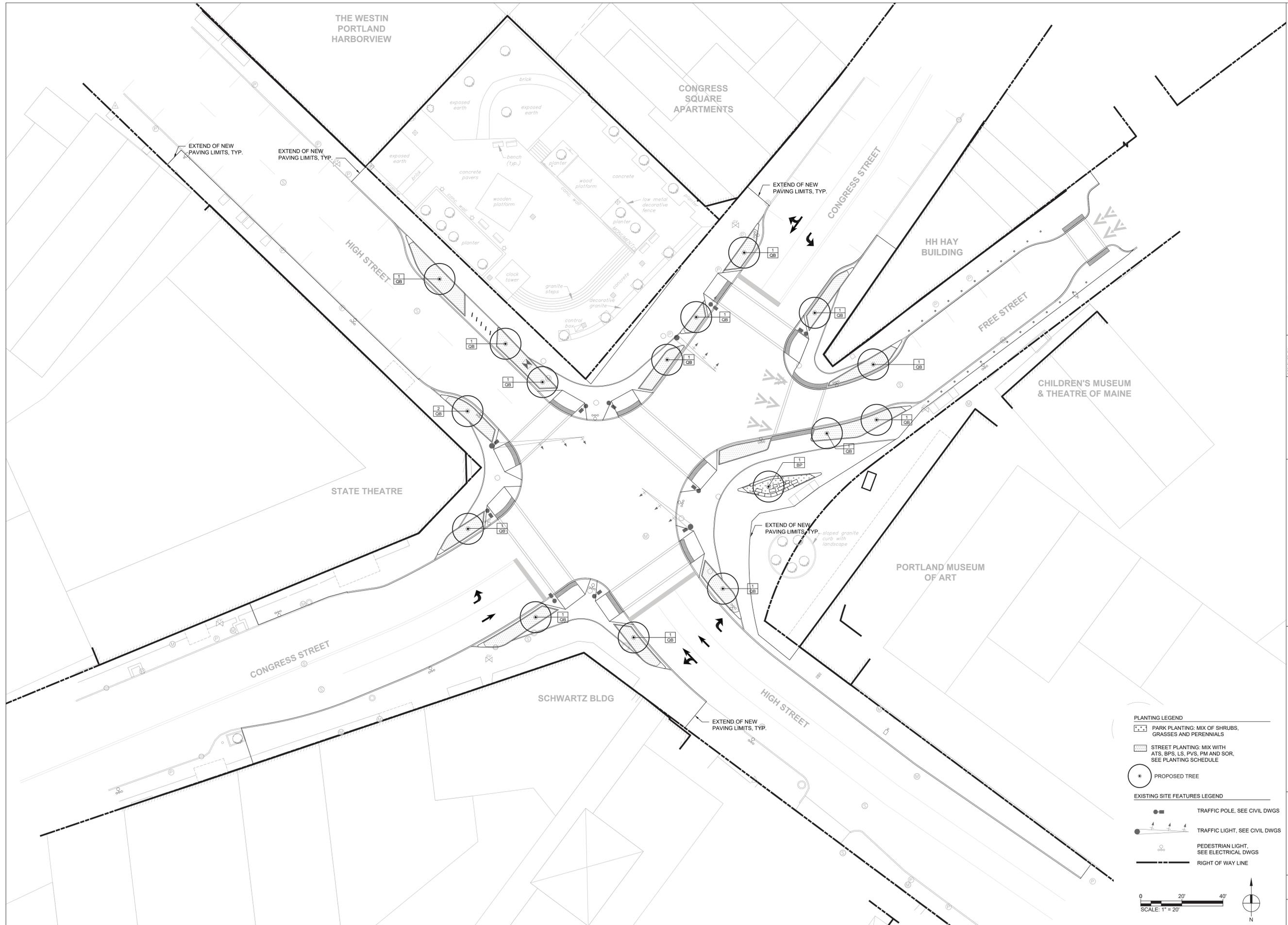
**SHEET #**  
16 OF 38  
**PLAN NUMBER**



- MATERIALS LEGEND**
- PROPOSED GRANITE PAVER IN LIGHT GREY COLOR
  - 4" x 12" x 2" PEDESTRIAN
  - 4" x 12" x 4" VEHICULAR
  - PROPOSED GRANITE PAVER IN DARK GREY COLOR
  - 4" x 12" x 2" PEDESTRIAN
  - PROPOSED BRICK PAVER
  - PRECAST CONCRETE
  - PROPOSED GRANITE PLANTER CURB
  - PROPOSED TREE
- EXISTING SITE FEATURES LEGEND**
- TRAFFIC POLE, SEE CIVIL DWGS
  - TRAFFIC LIGHT, SEE CIVIL DWGS
  - PEDESTRIAN LIGHT, SEE ELECTRICAL DWGS
  - RIGHT OF WAY LINE



<p><b>DESIGNED BY:</b> NRT</p> <p><b>DRAWN BY:</b> WRT</p> <p><b>CHECKED BY:</b> MC</p> <p><b>SCALE:</b> 1"=20'</p> <p><b>DATE:</b> 08-27-2020</p>	<p><b>REFERENCES:</b></p> <p>LDD PROJECT NAME: CONGRESS SQUARE IMPROVEMENTS</p> <p>DRAWING NAME: CONGRESS SQUARE IMPROVEMENTS</p> <p>FIELD BOOK USED: N/A</p>
<p><b>CONGRESS SQUARE IMPROVEMENTS</b></p> <p>STREETSCAPE AND LANDSCAPING MATERIALS PLAN</p>	
<p><b>CITY OF PORTLAND, MAINE</b></p> <p>PUBLIC SERVICES DEPARTMENT</p> <p>ENGINEERING DIVISION</p>	
<p>SHEET #</p> <p style="text-align: center;">17 OF 38</p> <p>PLAN NUMBER</p>	



**PLANTING LEGEND**

- PARK PLANTING: MIX OF SHRUBS, GRASSES AND PERENNIALS
- STREET PLANTING: MIX WITH ATS, BPS, LS, PVS, PM AND SOR. SEE PLANTING SCHEDULE
- PROPOSED TREE

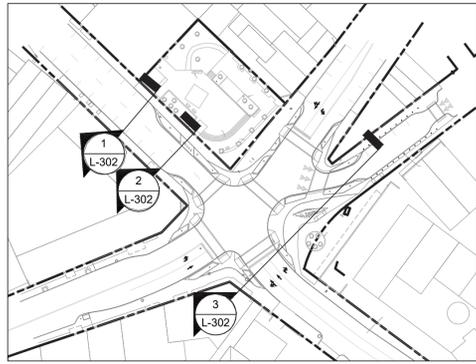
**EXISTING SITE FEATURES LEGEND**

- TRAFFIC POLE. SEE CIVIL DWGS
- TRAFFIC LIGHT. SEE CIVIL DWGS
- PEDESTRIAN LIGHT. SEE ELECTRICAL DWGS
- RIGHT OF WAY LINE

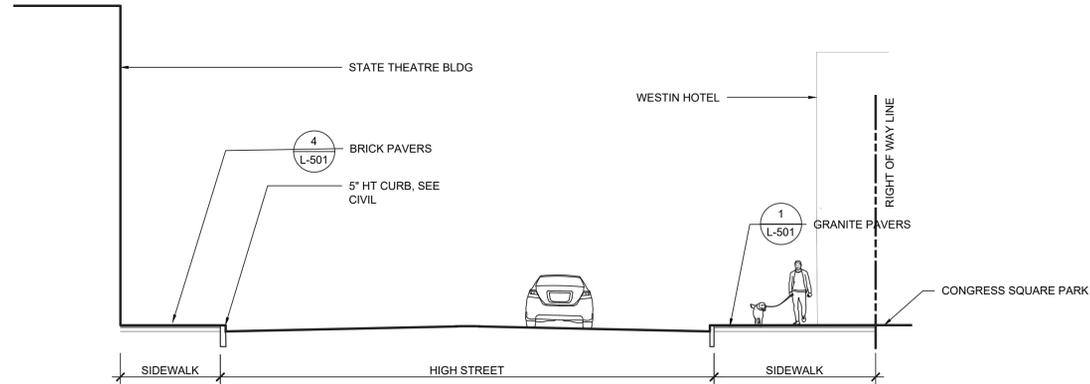
0 20' 40'  
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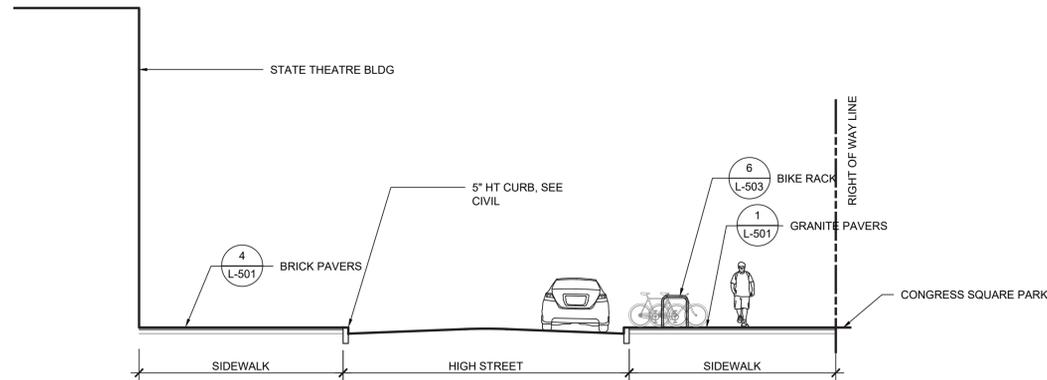
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<b>REFERENCES:</b>			
DESIGNED BY: WRT	DRAWN BY: WRT	CHECKED BY: MC	SCALE: 1"=20'
		DATE: 08-27-2020	
<b>CONGRESS SQUARE IMPROVEMENTS</b>		<b>PLANTING PLAN</b>	
CITY OF PORTLAND, MAINE PUBLIC SERVICES DEPARTMENT ENGINEERING DIVISION			
SHEET #		18 OF 38	
PLAN NUMBER			



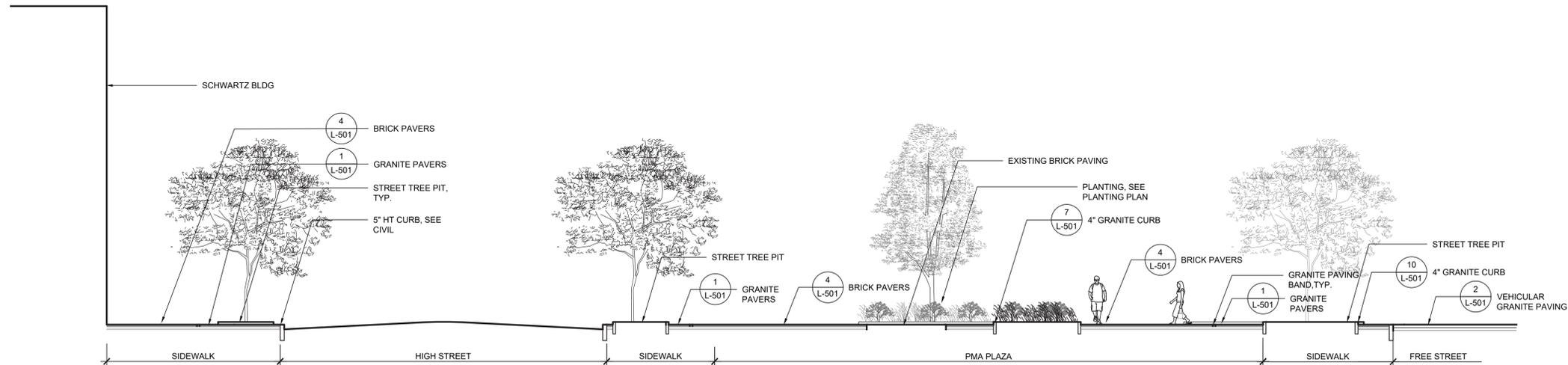
KEY PLAN



1 SECTION THROUGH HIGH STREET - CONGRESS SQUARE PARK SIDE  
SCALE: 1/8" = 1'-0"



2 SECTION THROUGH HIGH STREET - CONGRESS SQUARE PARK SIDE  
SCALE: 1/8" = 1'-0"



3 SECTION THROUGH HIGH STREET - PMA SIDE  
SCALE: 1/8" = 1'-0"

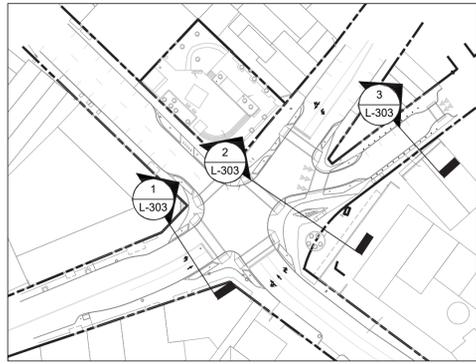
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CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

REFERENCES:

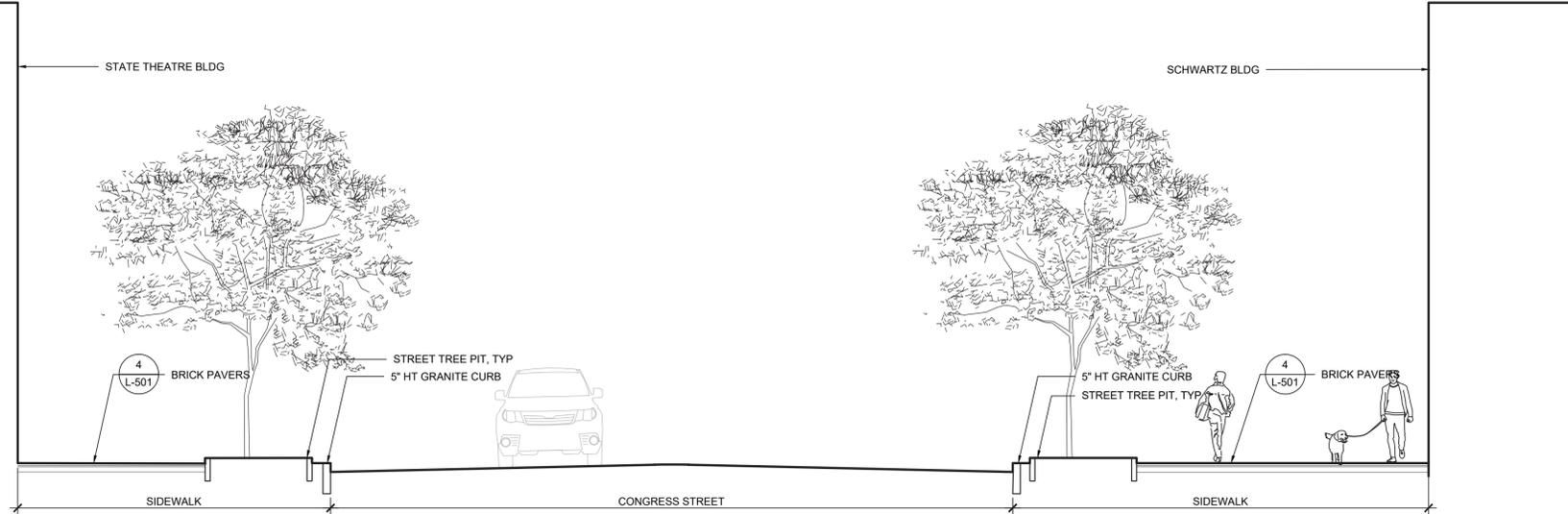
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SCALE:	1"=20'
DATE:	08-27-2020

CONGRESS SQUARE  
IMPROVEMENTS  
SECTIONS

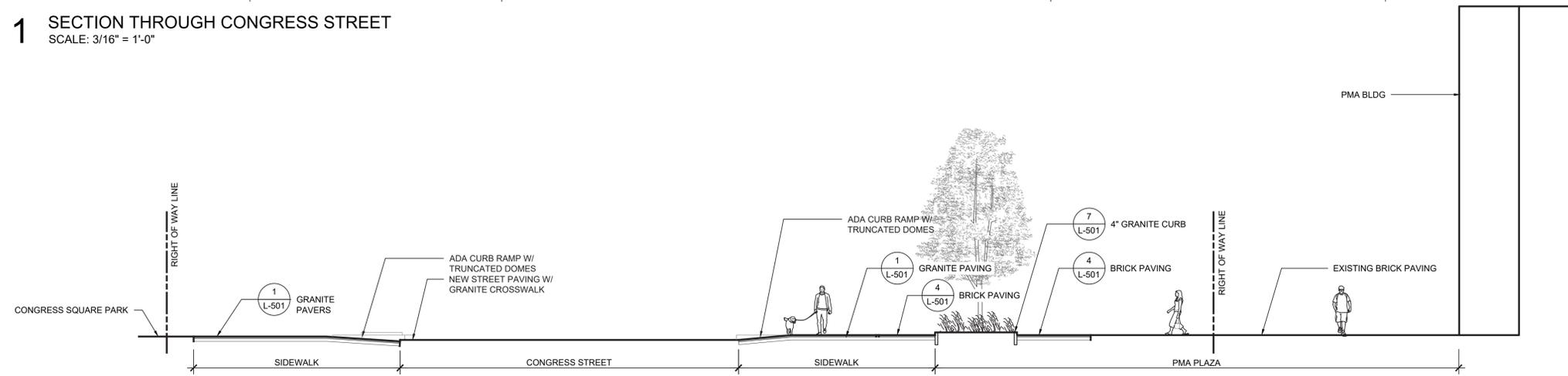
CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION



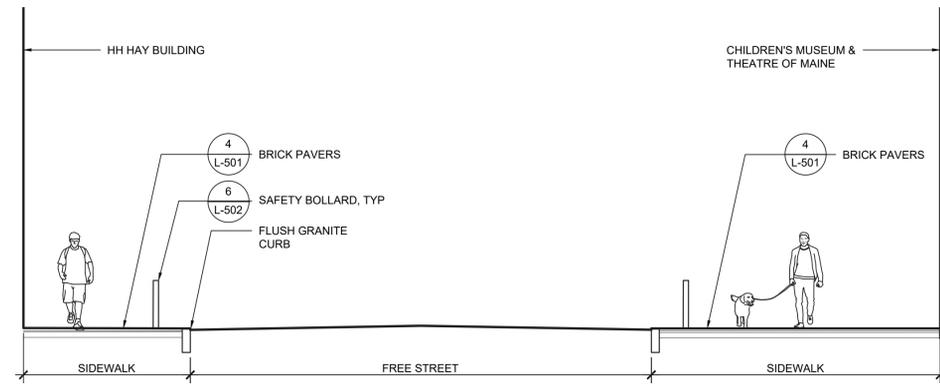
KEY PLAN



**1** SECTION THROUGH CONGRESS STREET  
SCALE: 3/16" = 1'-0"



**2** SECTION THROUGH CONGRESS STREET - PMA SIDE  
SCALE: 1/8" = 1'-0"



**3** SECTION THROUGH FREE STREET  
SCALE: 3/16" = 1'-0"

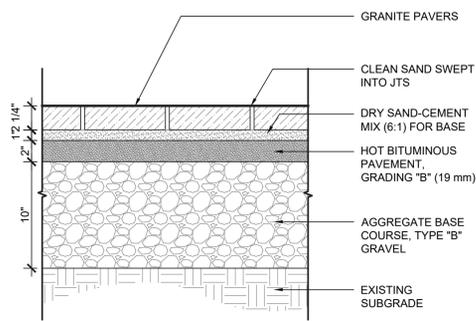
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CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

REFERENCES:

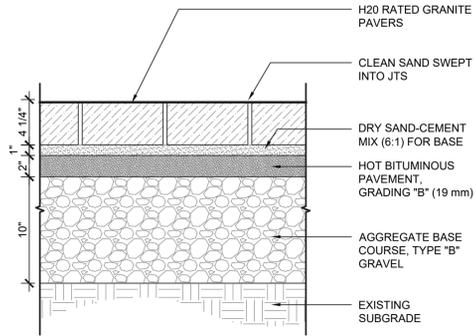
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DATE:	08-27-2020

CONGRESS SQUARE  
IMPROVEMENTS  
SECTIONS

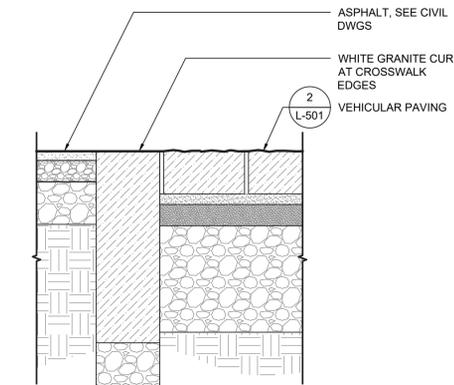
CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION



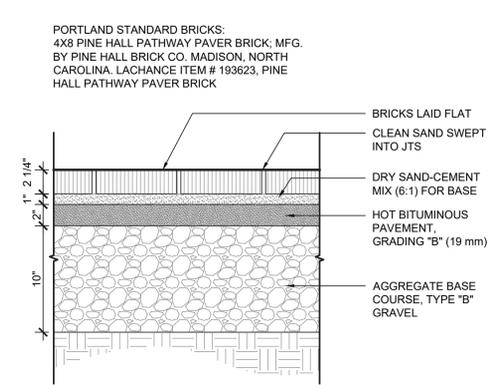
**1** PEDESTRIAN GRANITE PAVERS - SECTION  
SCALE: 1 1/2" = 1'-0"



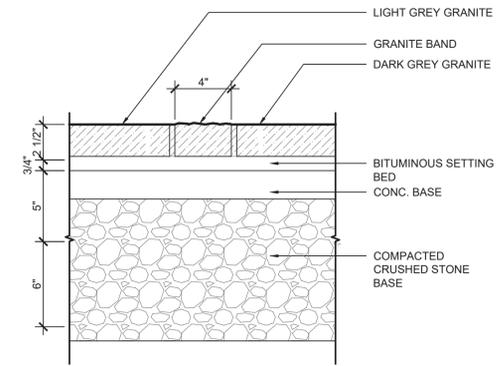
**2** VEHICULAR GRANITE PAVERS - SECTION  
SCALE: 1 1/2" = 1'-0"



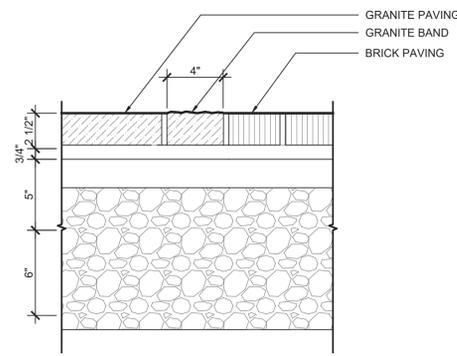
**3** EDGE AT VEHICULAR GRANITE AND ASPHALT - SECTION  
SCALE: 1 1/2" = 1'-0"



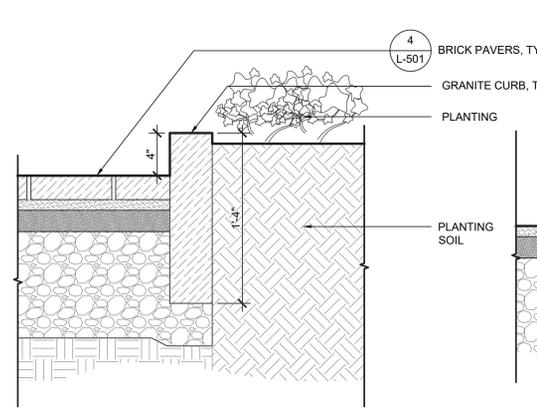
**4** BRICK PAVERS - SECTION  
SCALE: 1 1/2" = 1'-0"



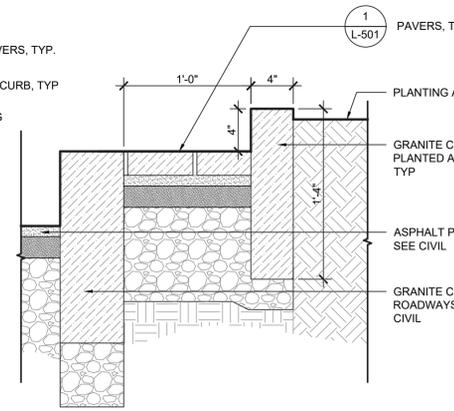
**5** BAND AT GRANITE PAVER A AND GRANITE PAVER B - SECTION  
SCALE: 2" = 1'-0"



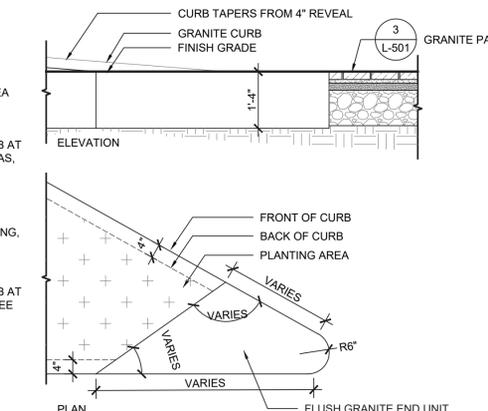
**6** BAND AT GRANITE PAVER AND BRICK PAVING - SECTION  
SCALE: 2" = 1'-0"



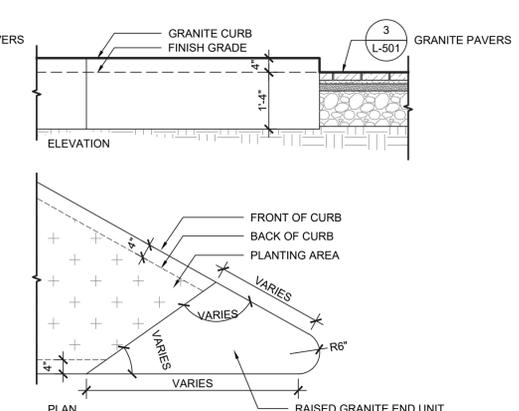
**7** GRANITE CURB AT PLANTING IN PARK - SECTION  
SCALE: 1 1/2" = 1'-0"



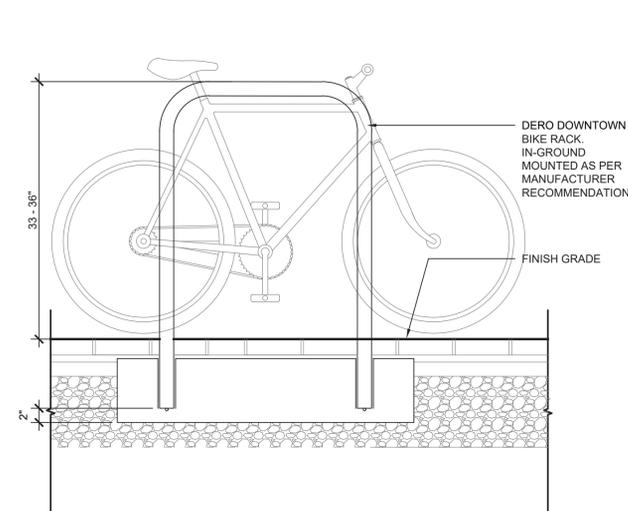
**8** GRANITE CURB AT SIDEWALK PLANTING - SECTION  
SCALE: 1 1/2" = 1'-0"



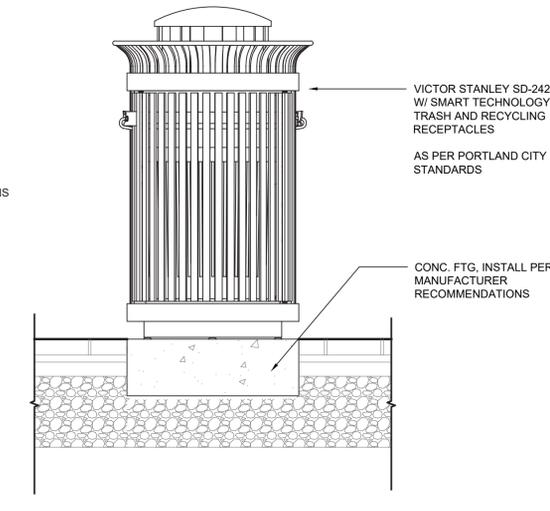
**9** GRANITE CURB END PIECE AT PARK - ELEVATION + PLAN  
SCALE: 1/2" = 1'-0"



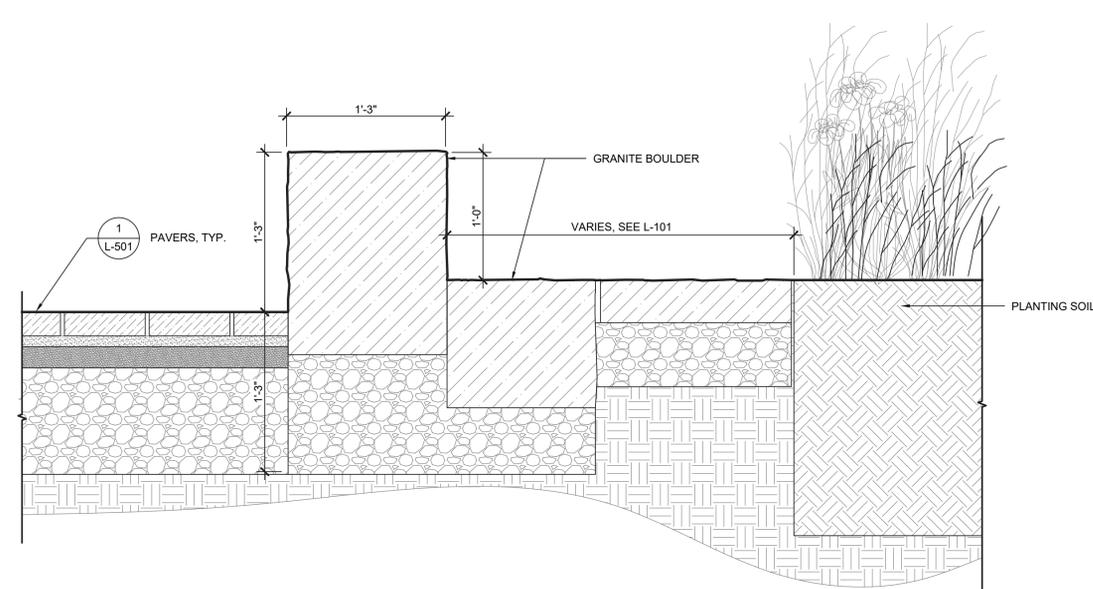
**10** GRANITE CURB END PIECE AT SIDEWALK - ELEVATION + PLAN  
SCALE: 1/2" = 1'-0"



**11** BIKE RACK, TYP - SECTION  
SCALE: 1" = 1'-0"



**12** WASTE RECEPTACLE, TYP - SECTION  
SCALE: 1" = 1'-0"



**13** STACKED GRANITE BENCH AT PMA PLAZA - SECTION  
SCALE: 1 1/2" = 1'-0"

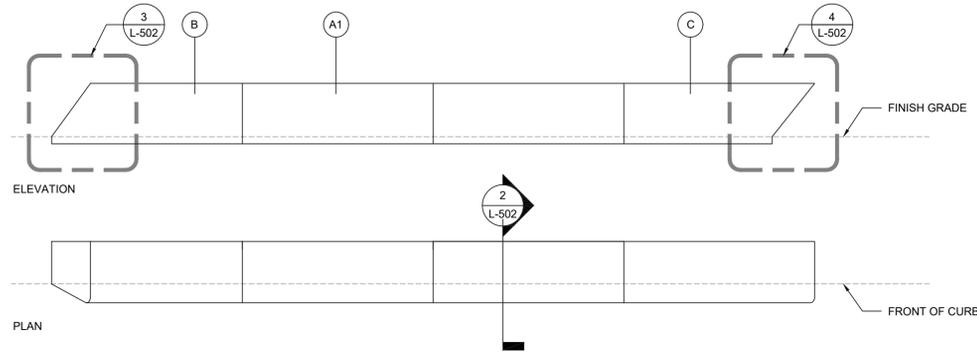
LDD PROJECT NAME:  
CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

REFERENCES:

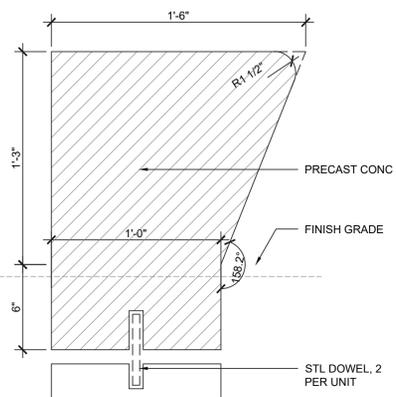
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SCALE:	1"=20'
DATE:	08-27-2020

CONGRESS SQUARE  
IMPROVEMENTS  
SITE DETAILS

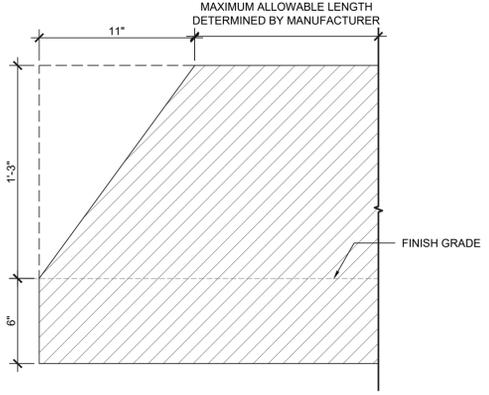
CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION



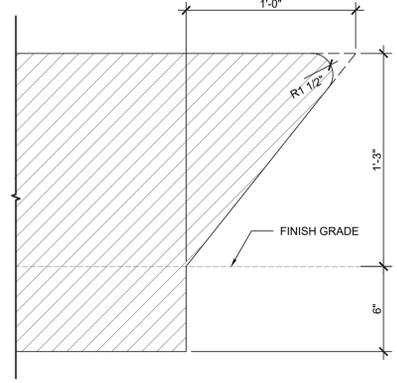
**1 PRECAST CONC. BENCH - PLAN AND ELEVATION**  
SCALE: 1/2" = 1'-0"



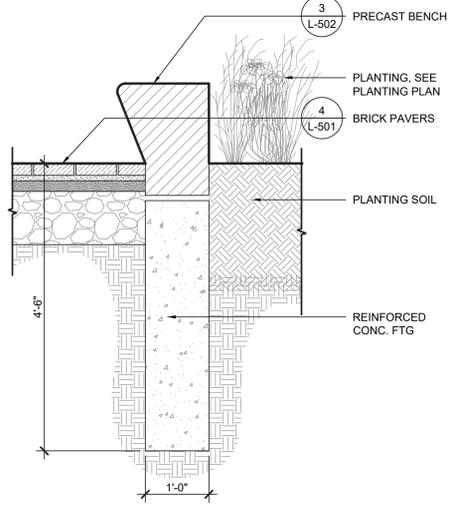
**2 PRECAST CONC. BENCH TYPE A1 - SECTION**  
SCALE: 2" = 1'-0"



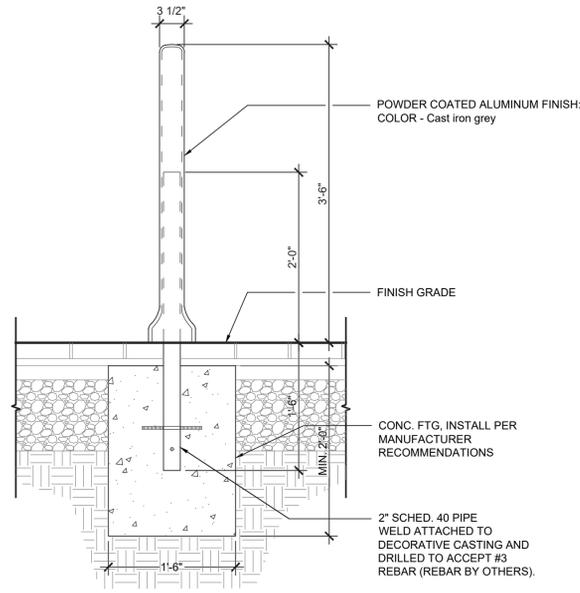
**3 PRECAST CONC. BENCH - TYPE B - ELEVATION**  
SCALE: 2" = 1'-0"



**4 PRECAST CONC. BENCH - TYPE C - ELEVATION**  
SCALE: 2" = 1'-0"



**5 PRECAST CONC BENCH, TYP - SECTION**  
SCALE: 3/4" = 1'-0"



**6 SAFETY BOLLARD, TYP - SECTION**  
SCALE: 1" = 1'-0"

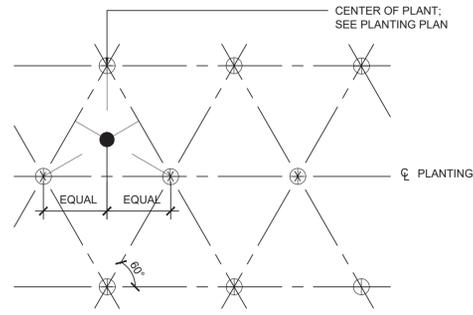
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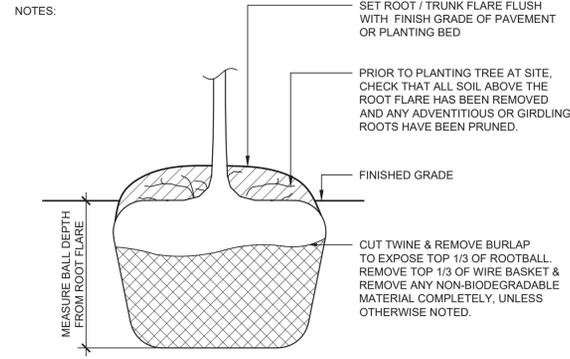
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**CONGRESS SQUARE  
IMPROVEMENTS  
SITE DETAILS**

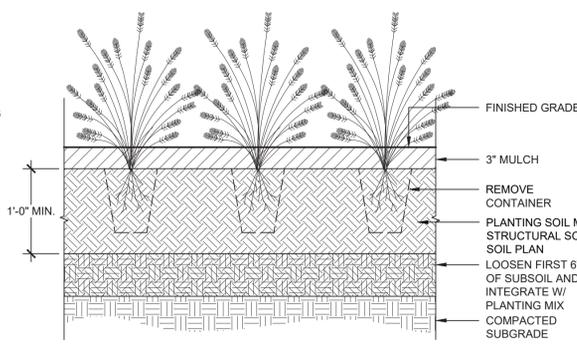
**CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION**



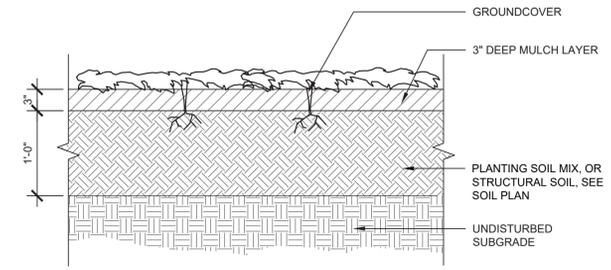
**1** PERENNIAL / ORNAMENTAL GRASS PLANTING PLAN, TYP  
N.T.S.



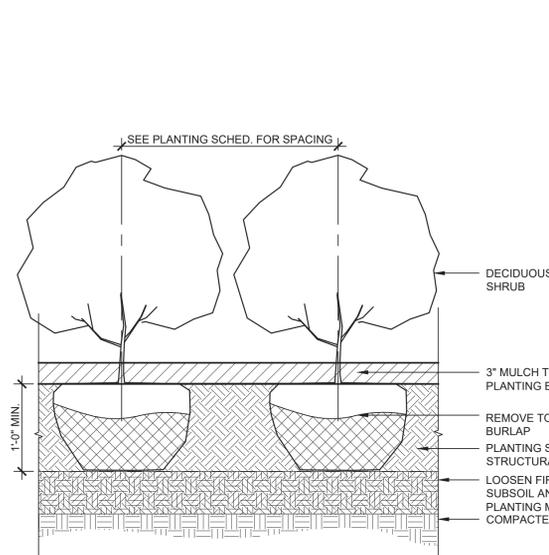
**2** ROOT BALL PREPARATION, TYP  
N.T.S.



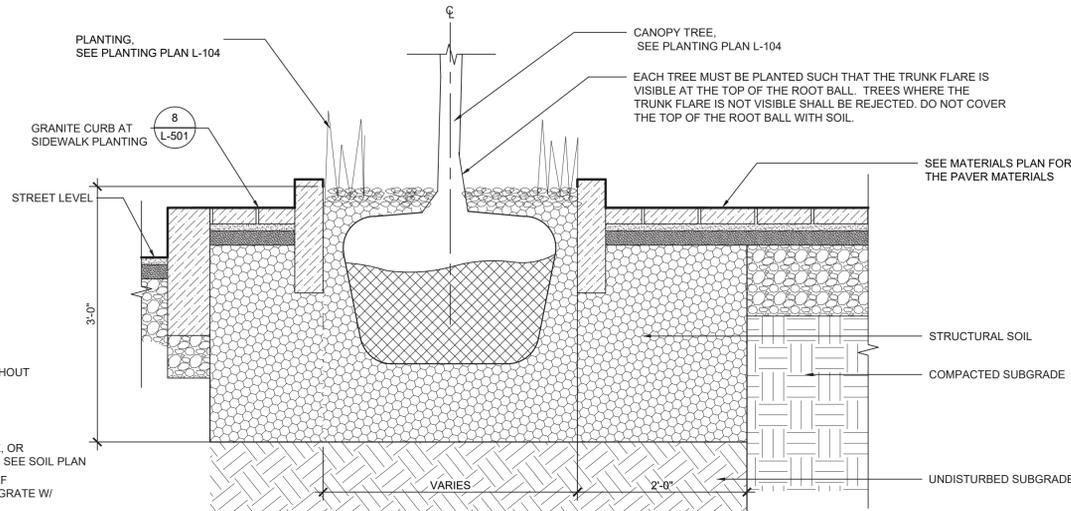
**3** PERENNIAL / ORNAMENTAL GRASS PLANTING  
SCALE: 1" = 1'-0"



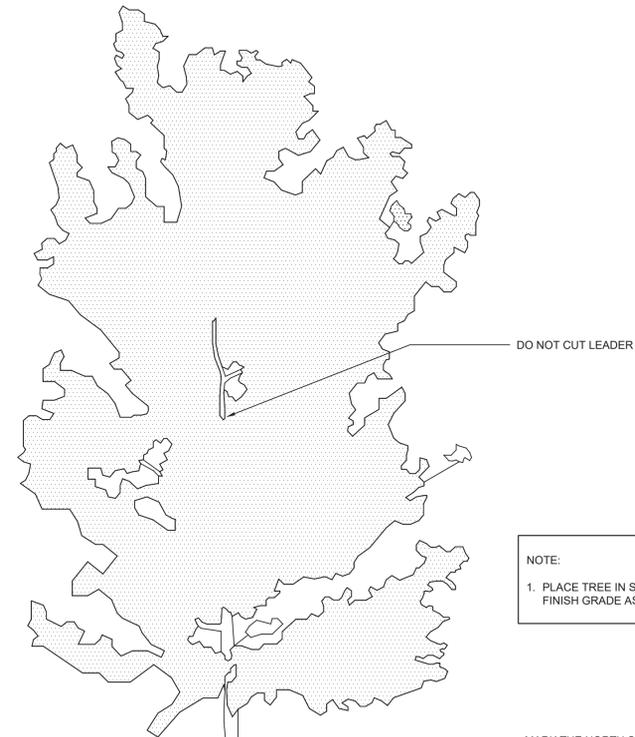
**4** GROUNDCOVER / FERNS PLANTING, TYP  
SCALE: 1" = 1'-0"



**5** SHRUB PLANTING, TYP  
N.T.S.



**6** DECIDUOUS TREE PLANTING IN STRUCTURAL SOIL AT SIDEWALK - SECTION  
SCALE: 1" = 1'-0"



**7** DECIDUOUS TREE PLANTING IN PLANTING BED - SECTION  
SCALE: 1" = 1'-0"

**PLANTING SCHEDULE**

KEY	BOTANICAL NAME	COMMON NAME	SIZE AT PLANTING	SIZE AT MATURITY	SPACING	NOTES
<b>LARGE SHADE TREES</b>						
QB	<i>Quercus bicolor</i>	SWAMP WHITE OAK	3.5"- 4" CAL.	50-60' HT.	SEE PLAN	B & B
BP	<i>Betula populifolia</i>	GREY BIRCH	3.5"- 4" CAL.	20'-40' HT	SEE PLAN	SINGLE STEM, B & B
<b>SHRUBS</b>						
CA	<i>Clethra alnifolia 'Compacta'</i>	SUMMERSWEET	12"-24" HT	24"-48" HT.	4' O.C.	B & B OR CONTAINER
CO	<i>Comptonia peregrina</i>	SWEETFERN	12"-24" HT.	24"-52" HT.	6' O.C.	B & B OR CONTAINER
IVR	<i>Ilex verticillata 'Red Sprite'</i>	WINTERBERRY	24"-30" HT	36"-64" HT.	6' O.C.	B & B OR CONTAINER
IVJ	<i>Ilex verticillata 'Jim Dandy'</i>	WINTERBERRY	24"-30" HT	36"-64" HT.	6' O.C.	B & B OR CONTAINER
RV	<i>Rosa virginiana</i>	VIRGINIA ROSE	24"-30" HT	48"-64" HT.	6' O.C.	B & B OR CONTAINER
<b>GRASSES</b>						
DC	<i>Deschampsia cespitosa</i>	TUFTED HAIR GRASS	1 GAL.	24"-36" HT	18" O.C.	
PV	<i>Panicum virgatum 'Shenandoah'</i>	SWITCH GRASS	1 GAL.	36"-64" HT.	30" O.C.	
SS	<i>Schizachyrium scoparium 'Standing Ovation'</i>	LITTLE BLUESTEM	1 GAL.	24"-48" HT.	24" O.C.	
<b>FERNS/SEDGES</b>						
AA	<i>Athyrium angustum</i>	NORTHERN LADY FERN	1 GAL.	18"-30" HT.	24" O.C.	
CA	<i>Carex appalachica</i>	APPALACHIAN SEDGE	#4 CONT.	6" HT.	12" O.C.	
CE	<i>Carex eburnea</i>	BRISTLE-LEAVED SEDGE	#4 CONT.	6"-12" HT.	9" O.C.	
CP	<i>Carex pensylvanica</i>	PENNSYLVANIA SEDGE	#4 CONT.	6"-12" HT.	9" O.C.	
PA	<i>Polystichum acrostichoides</i>	CHRISTMAS FERN	1 GAL.	12"-24" HT.	18" O.C.	
<b>PERENNIALS</b>						
ATS	<i>Amsornia tabernaemontana var. salicifolia</i>	BLUESTAR	1 GAL.	24"-36" HT.	30" O.C.	
BPS	<i>Baptisia 'Purple Smoke'</i>	FALSE INDIGO	1 GAL.	36"-48" HT.	36" O.C.	
LS	<i>Liatris spicata</i>	BLAZING STAR	#4 CONT.	24"-48" HT.	12" O.C.	
PM	<i>Pycnanthemum muticum</i>	MOUNTAIN MINTS	1 GAL.	24"-36" HT.	36" O.C.	
SO	<i>Symphoricarum oblongifolium 'October Skies'</i>	AROMATIC ASTER	1 GAL.	18"-24" HT.	18" O.C.	

LDD PROJECT NAME:  
CONGRESS SQUARE  
IMPROVEMENTS  
DRAWING NAME:  
FIELD BOOK USED:  
N/A

**REFERENCES:**

DESIGNED BY:  
WRT  
DRAWN BY:  
WRT  
CHECKED BY:  
MC  
SCALE:  
1"=20'  
DATE:  
08-27-2020

**CONGRESS SQUARE  
IMPROVEMENTS  
PLANTING DETAILS**

**CITY OF PORTLAND, MAINE  
PUBLIC SERVICES DEPARTMENT  
ENGINEERING DIVISION**

# STATE OF MAINE

## MEMORANDUM

July 9, 2020

To: Julie Senk, ENV/Maine Department of Transportation  
From: Kirk F. Mohny, State Historic Preservation Officer *KFM*  
Subject: WIN 24293.00,Portland, Intersection Improvements; MHPC# 0742-20

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In response to your recent request, I have reviewed the information received June 11, 2020 to continue consultation on the above referenced undertaking pursuant to the Maine Programmatic Agreement and Section 106 of the National Historic Preservation Act of 1966, as amended.

Based on the information provided, the Commission agrees with the MDOT's conclusion that the following properties in the project's area of potential effect are eligible for listing in or are listed in the National Register of Historic Places:

- Spring Street and Congress Street Historic Districts (SM#1, 4-14,16-27, 30-31, 33-35)- NR listed
- H.H. Hay Block (SM# 13)- individually listed
- Portland Museum of Art-Charles Shipman Payson Building (SM# 17)- individually listed
- Portland Museum of Art-L.D.M. Sweat Memorial Galleries (SM #18 and 19)- individually listed, National Historic Landmark

The Commission concludes that the following properties are not eligible for listing in the National Register

- William V. Jacobs House (SM# 29) non-contributing to Spring Street Historic District

No other properties within the APE are eligible for listing in the National Register of Historic Places.

Please contact Megan M. Rideout of our office if we can be of further assistance in this matter.

# STANDARDS: STREETScape AND PEDESTRIAN IMPROVEMENTS

# 8

## 1. General

Retain the distinctive historic features of the streetscape, including walkways, alleys, lighting, signage, planters, landscaping, curbing, and paving that give the district its distinguishing character. Where streetscape or pedestrian improvements are proposed which were not historically a part of the site, such improvements shall be compatible with the existing historic character.

## 2. Pedestrian Amenities

- a. While pedestrian amenities must be compatible with the City's historic character, variations shall be permitted in order to respect the vitality and the variety of the City's different thoroughfares and neighborhoods.
- b. Different types of public spaces should respond to the following general performance criteria:
  - Historic commercial streets shall be treated simply with maximum open sidewalk space, limited obstructions on the ground, and pedestrian preference for street crossing.
  - Historic, non-commercial pedestrian streets and walks shall have a similar scale, more intimate design using textures and smaller elements that stimulate interest along the path.
  - Waterfronts were not typically pedestrian spaces in historic Portland but should be opened up to the public wherever possible due to their historic interest and value as a public amenity.

- Parking areas must be carefully designed and landscaped due to their large size and visual impact upon visitors and residents.
- Parks should play a special role in historic interpretation and provide day-time cultural activity for the District as well as relief from paved areas.

## 3. Streetscape

### a. Paving and Planting

- a. The existing streetscape should be enriched, especially around historic buildings and heavily used pedestrian areas. Historically appropriate improvements should create some consistency while avoiding complete uniformity.
- b. Historic paving features should be retained wherever possible and incorporated into the streetscape improvements. For example, where brick sidewalks existed historically, brick shall continue as the material of choice.
- c. Subtle variations in paving patterns and materials shall be used to enrich sidewalks and plazas, such as highlighting patterns in street lights, trees, furniture, street crossings, and entryways.
- d. Planting shade trees and shrubs shall be encouraged where they would enhance the historic character and create more inviting spaces. Removal of healthy trees over 3" caliper shall be discouraged, except where they threaten existing structures or personal safety.

**b. Street Furniture**

- a. Placement of street furniture which is appropriate to the context, attractive, and durable shall be encouraged. Placement of furniture should be based upon careful study of how people tend to use a street.

**c. Lights, Signs, and Traffic Signals**

- a. Public signs shall utilize compatible graphics, colors, proportions, dimensions, and fabrication methods in order to create greater consistency and improve their compatibility with their historic setting.
- b. Street lights shall be designed to harmonize with their surroundings, and traffic signal poles and mounts shall be as unobtrusive as possible, both physically and visually.

## Application to become a Section 106 Consulting Party

Section 106 of the National Historic Preservation Act of 1966 requires agency officials to consider the effects of their actions on historic properties. The Section 106 process seeks to incorporate historic values into project planning through consultation among agencies, and other parties, with an interest in the effects of the undertaking on historic properties. Individuals or organizations with a demonstrated interest in the effects of the undertaking on historic properties can become a Section 106 consulting party. The various consulting parties work together to discuss options, provide multiple viewpoints, and strive to seek common agreement on the incorporation of historic preservation values into the project.

If you are interested in becoming a consulting party for the undertaking, please provide your information at the bottom of this form and return to:

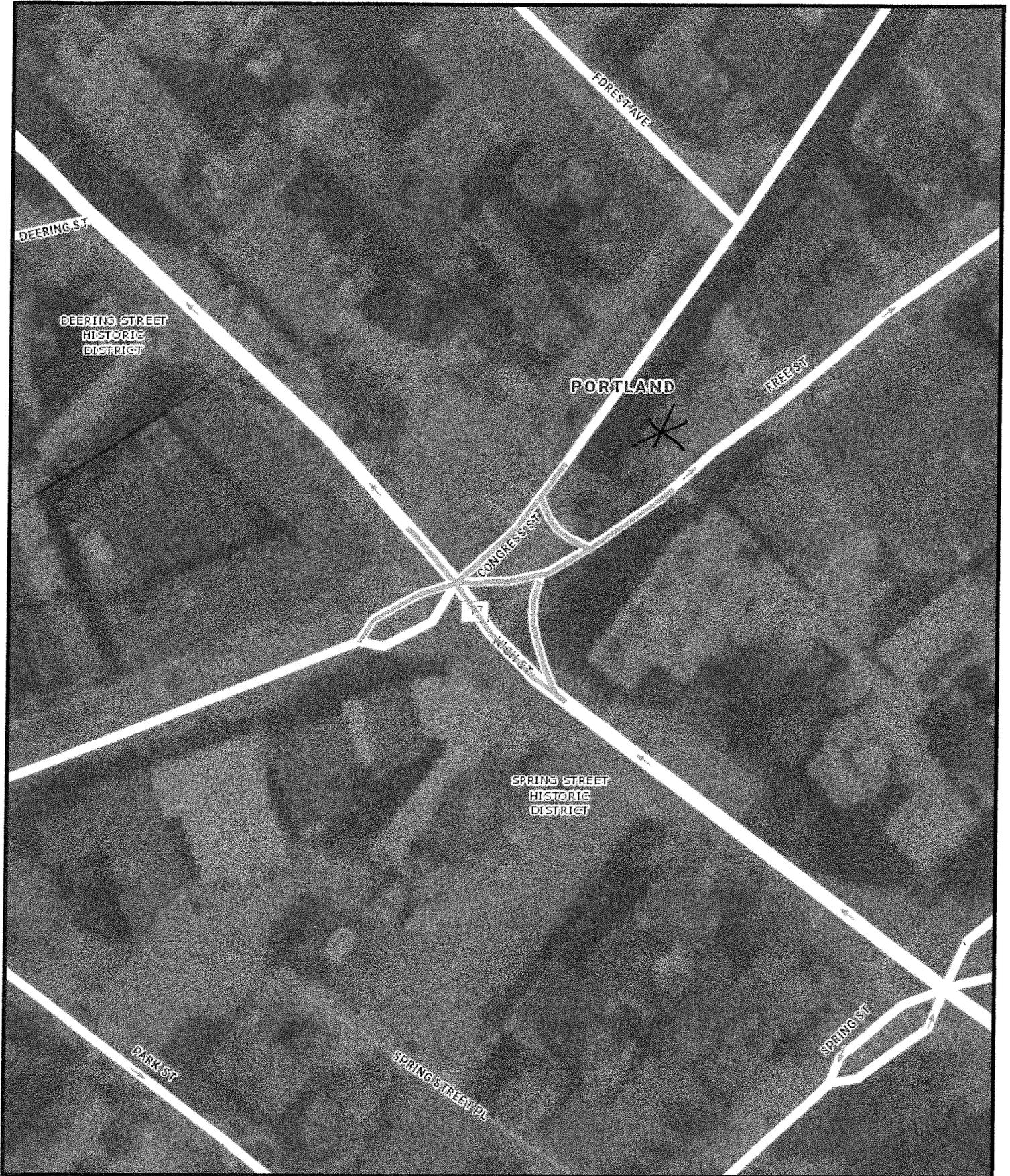
Maine Department of Transportation  
16 State House Station Environmental Office  
Augusta, Maine 04333

MaineDOT WIN Number:	24293
Name:	Julie Harry Greater Portland Landmarks-
Address:	93 High St Portland ME
Email Address:	jlarry@portlandlandmarks.org

### Concerns Regarding the Effect of the Project on Historic Properties:

Landmarks holds an exterior easement on 588-594 Congress St. (marked w/ a *). As neighbors with a legal interest in an abutting property we have participated in the lengthy discussion about the future of Congress Sq.
The square is part of the Congress Street Local Historic District and the city's plans were reviewed by the Historic Preservation Board; we would ask if the MDT plans to deviate from those plans to have a workshop with the board for public discussion.
We don't anticipate any effect on any building, but the public space and its walkability/safety for pedestrians is important for the vitality of the retail + commercial uses that keep the buildings active + in good repair.

# PORTLAND 24293.00



The Maine Department of Transportation provides this publication for information only. Reliance upon this information is at user risk. It is subject to revision and may be incomplete depending upon changing conditions. The Department assumes no liability if injuries or damages result from this information. This map is not intended to support emergency dispatch.

0.02  
Miles  
1 inch = 0.02 miles

Date: 3/5/2020  
Time: 2:14:15 PM