

Supplemental Supporting Information for a Finding of Effect
DRAFT - PUBLIC COMMENT COPY

Project: Thomaston 26290.00
Scope: New Construction
Finding of Effect: **No Adverse Effect**

This report describes the Maine Department of Transportation and the Federal Highway Administration's compliance with Section 106 of the National Historic Preservation Act (36 CFR Part 800). It details the finding of effect to historic properties listed in or eligible for listing in the National Register of Historic Places (NRHP) that are located in the subject project's Area of Potential Effects (APE). This report also assesses how the proposed project may directly or indirectly affect and/or diminish those characteristics and aspects of integrity that qualify a historic property for inclusion in the NRHP. This report is specific to the Section 106 assessment of effects, as opposed to general environmental impacts. Consultation with coordinating agencies and the public is ongoing.

Purpose and Need

The purpose of this project is to improve connectivity and safety with the addition of sidewalks, widened shoulders, and ADA (Americans with Disabilities Act) compliant crossings.

The need for this project is due to the lack of connectivity of the existing sidewalks and lack of safe ADA compliant crossings.

US Route 1 is heavily travelled, improvements to pedestrian/bicycle safety are needed at the Fish Street crosswalk and will include ADA upgrades, sidewalk, and widened shoulders. A pedestrian/bicycle connection will provide needed improvements of connectivity from the downtown sidewalk to the Town's east side residential district and the Fort Knox Museum along Route 131.

Proposed Action

The proposed action would consist of ADA upgrades at the Fish Street crosswalk and a new 5'-6" wide sidewalk along US Route 1 from Fish Street to the intersection of Route 131 and turning southerly on Route 131 to a point just beyond the intersection of Route 131 and Old High Street and provide crosswalk across Route 131 in a manner that is safe for pedestrians with the use of Rectangular Rapid Flashing Beacons (RRFB's).

The proposed typical section will include 5'-6" paved sidewalk with Type 1 Granite Curb including curb ramps on US Route 1 at Fish Street and on Route 131 and Old High Street. Shoulder work associated with the new sidewalks will be 8' along US Route 1 and side of Route 131. 6' shoulders are proposed along the easterly side of Route 131 and Old High Street. Pavement at existing shoulders along High Street will be saw cut 2' in front of the proposed curb location to allow for installation of granite curb Type 1.

A streetlight is proposed on the northly side of US Route 1 across from the Fish Street crosswalk.

Proposed Rectangular Rapid Flashing Beacons with (RRFBs) located on each side of the Route 131 crosswalk. Each RRFB set shall have two LED lights mounted on each pole, one light aimed at the detectable warning field and the other toward the center of the adjacent lane.

Federal Action

Federal funding.

Definition of Area of Potential Effect (APE)

The proposed project is located Thomaston. The maps below show the APE.

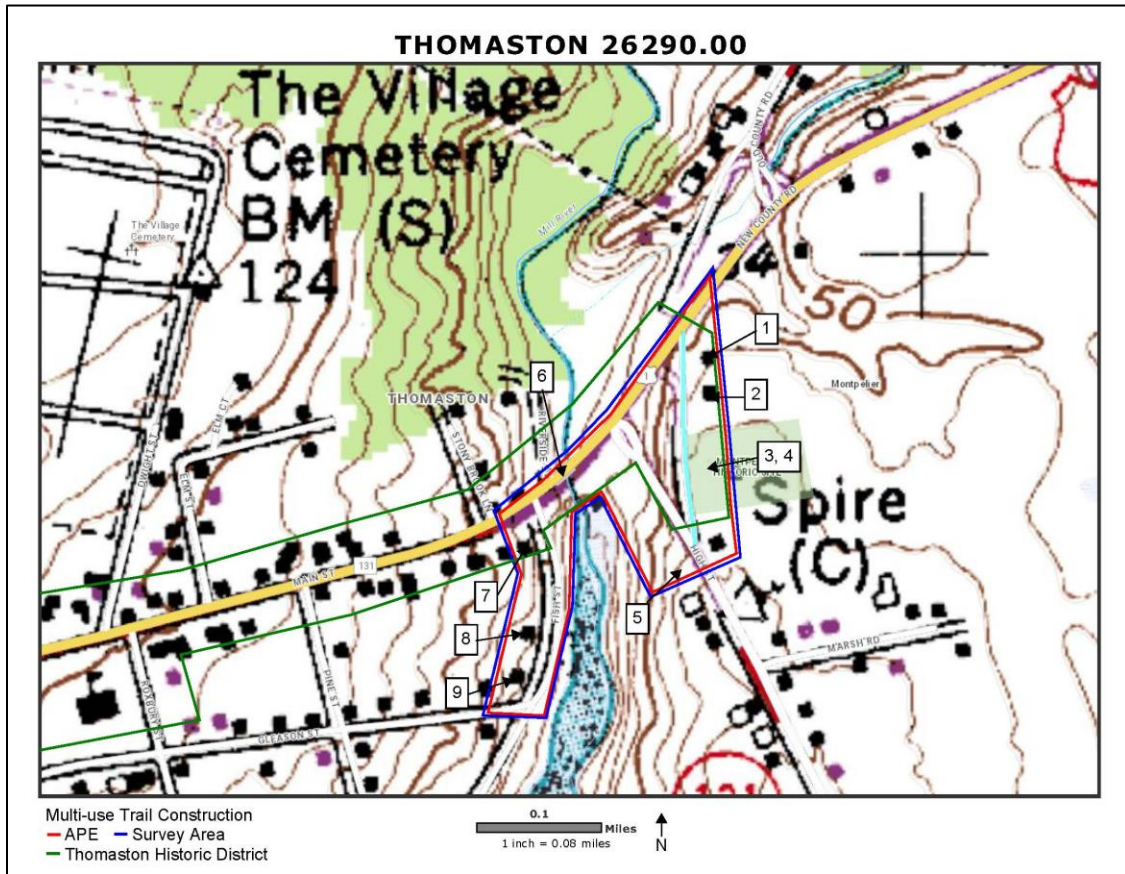


Figure 1: Thomaston 26290.00 Area of Potential Effect.

Historic Properties

The proposed project is located in Thomaston, Knox County, Maine. The descriptions are based on Maine Historic Preservation Commission (MHPC) forms.

Thomaston Historic District (Various owners, Town of Thomaston. Sta. 100+00R – 105+00R and Sta. 200+00R – 205+00R)

National Register-Eligible

Criteria A and C, Architecture, Industry and Commerce



The Thomaston Historic District is listed in the National Register. The district encompasses the historic commercial center and adjoining residential areas of Thomaston. Permanent European settlement was established in 1735, and the town was incorporated in 1777. Lime burning was the first major industry, and it led to others, including coopering and shipbuilding. Located on the St. George River, the town experienced growth in the nineteenth century as a port and

ship building center. The many fine examples of Federal, Greek Revival, Italianate and Second Empire style buildings in the district reflect the prosperity of the town. The district retains a high level of integrity, with few significant alterations or modern infill. The period of significance is c.1820-1929.

Montpelier (The General Henry Knox Museum. Sta. 103+90L – 105+30L)

National Register-Eligible

Criteria A and C, Conservation and Architecture for its association with the early twentieth century preservation movement and its unique fireproof construction, on the local level.



Montpelier is eligible for listing in the National Register under Criteria A and C, Conservation and Architecture for its association with the early twentieth century preservation movement and its unique fireproof construction, on the local level. (Criteria Consideration E does not apply as the house is older than fifty years and has significance for the period in which it was built rather than the historic period it was intended to depict.) It is a replica of the 1794 house built by Henry Knox, a Revolutionary War general and

Secretary of War under George Washington. Knox settled in Thomaston after the war on land originally purchased by his wife's, Lucy Flucker, maternal grandfather Samuel Waldo. The original mansion sat on a small hill overlooking the St. George River and was torn down in 1871 to make way for the Brunswick-Rockland Railroad. Forty years later, the General Knox Chapter of the Daughters of the American Revolution (DAR) spearheaded the movement to construct a replica of Montpelier. The group was spurred by the earlier restoration of Washington's Mount Vernon and interest in celebrating the founders of the United States that developed in the late-nineteenth

century, as well as the promise of Knox descendant Dr. Henry Thatcher Fowler to donate his collection of Knox family pieces if a fireproof house was constructed to display them. After unsuccessfully appealing to the Maine State Legislature for funds, the DAR focused on raising funds from the public. One member donated \$7000 for Boston architects William Putnam and Allen Cox to draft plans based on photographs and accounts from locals who remembered the original structure. With no photographs of the interior, the architects chose to create a classical Federal plan. Over the next ten years, the chapter acquired a site for the reconstruction and raised a total of \$10,000, much less than the estimated \$125,000 needed for construction. A new group, the Knox Memorial Association, was founded to focus on the reconstruction. The new organization was able to attract members outside of Maine, including Cyrus Curtis, the founder of the Ladies Home Journal, Saturday Evening Post, and owner of various newspapers. He ended up donating a total of \$100,000 towards the reconstruction and another \$50,000 to furnish the house. After the original lot purchased by the DAR was determined unsuitable, a new location at the end of Main Street was acquired. In 1929, the Maine Legislature appropriated \$5,000 towards the efforts and ground was broken that July. In order to resist fire, the replica was constructed of brick and steel with the exterior wood sheathing nailed to furring strips attached to the structural brick walls. The floor joists are steel and the floors are concrete overlaid with hardwood floorboards. The interior walls are plaster on wire mesh lath. The roof is supported by steel and the roof decking is gypsum plank with wire reinforcement. The building was dedicated July 25, 1931, on what would have been Knox's 181st birthday, with Maine Governor Gardiner in attendance. In 1965, the Knox Memorial Association deeded Montpelier to the State and it was operated as a museum by the Bureau of Parks and Recreation. The site was transferred to the Friends of Montpelier in 1999 and continues to operate as a museum.

Montpelier is significant for its association with a broad early twentieth century patriotic interest in the settlement of America by Europeans and the founding of the United States, as well as the effort at that time to recreate or preserve associated historic properties, the foundation of the American historic preservation movement. The building is architecturally significant as an example of high style Georgian Revival architecture and for the use of fireproof construction methods. Montpelier retains all aspects of integrity, and its period of significance is 1929.

Impacts to Property

The following addresses potential impacts to properties as a result of the proposed action.

Thomaston Historic District (Various owners, Town of Thomaston. Sta. 100+00R – 105+00R and Sta. 200+00R – 205+00R)

National Register-Eligible

Criteria A and C, Architecture, Industry and Commerce

The proposed action would result in **No Adverse Effect** to the Thomaston Historic District. The proposed action would include the construction of a new 5'-6" wide sidewalk along US Route 1 from Fish Street to the intersection of Route 131 and turning southerly on Route 131 to a point just beyond the intersection of Route 131 and Old High Street and provide a pedestrian safe ADA crosswalk across Route 131 with the use of Rectangular Rapid Flashing Beacons (RRFB's). The construction of the new sidewalk and safety upgrades would not significantly diminish the integrity of the district as there are no contributing properties adjacent to most of the proposed action and the area is already defined by hardscape and traffic infrastructure. The proposed

action poses a nominal change that does not significantly diminish the historic district's integrity of materials, setting, feeling, and association.

Montpelier (The General Henry Knox Museum. Sta. 103+90L – 105+30L)

National Register-Eligible

Criteria A and C, Conservation and Architecture for its association with the early twentieth century preservation movement and its unique fireproof construction, on the local level.

The proposed action would result in **No Adverse Effect** to the Montpelier House. The proposed actions would include the construction of a new 5'-6" sidewalk and crosswalk located at the intersection of Old High Street and High Street. This would involve the removal of an existing concrete staircase and an existing tree. A rectangular rapid flashing beacon would be located on High Street sta. 105+25L. The proposed sidewalk, crosswalk infrastructure, and concrete staircase/tree removal are not located directly adjacent to the Montpelier house and therefore would not significantly diminish the integrity of the resource as the area is already defined by hardscape. The proposed action poses a nominal change that does not significantly diminish Montpelier's integrity of materials, setting, feeling, and association.

Please Note: NRHP eligibility for Thomaston 26290.00 was determined prior to MHPC's statement on NRHP eligibility for U.S. Route 1. For the purposes of this project, MaineDOT is considering U.S. Route 1 eligible for listing in the NRHP at the project location under Criterion A, Transportation. It is MaineDOT's determination that the proposed action would not adversely effect the resource, as the action presents a nominal change to the setting and materials at the project location and would not significantly diminish the resource's overall aspects of integrity.

Archaeological Resources

There will be no archaeological resources affected by the proposed undertaking.

Avoidance and Minimization Efforts

The MaineDOT sought ways to avoid major impacts to the historic district by containing the proposed actions to areas with no historic properties or away from historic properties within the historic district. Overall, the proposed actions are sensitive additions that provide a pedestrian friendly environment in the Thomaston Historic District.

Dismissed Alternatives

The proposed 10' wide walk/bike trail was revised to a 5'-6" sidewalk with 8' shoulder after completion of conceptual alignment and preliminary slope impacts in the deep fill area along US Route 1 and Route 131.

Public Involvement

MaineDOT contacted the four federally recognized Native American tribes in Maine. The Penobscot Nation and Mi'kmaq Nation replied with no concerns about the undertaking. The other tribes did not respond.

The Towns of Thomaston was contacted at the commencement of the Section 106 review process and were asked to provide any questions or comments related to the historic review. No replies were received.

The public process is ongoing.

Proposed Materials

HMA, concrete, granite, stone dust, detectable landing pads, light pole, rectangular rapid flashing beacons (RRFBs)

Plans

Thomaston Knox County US Route 1 and Route 131, MaineDOT WIN 26290.00, December 20, 2024.

Attachments

Leith Smith, MHPC, to Julie Senk, MaineDOT, September 19, 2022

Leith Smith, MHPC, to Julie Senk, MaineDOT, November 7, 2024

Kirk Mohnney, MHPC, to Julie Senk, MaineDOT, December 19, 2022

STATE OF MAINE
Memorandum

Date: September 19, 2022

To: Julie Senk, Historic Preservation Coordinator, Maine DOT/ENV

From: J. N. Leith Smith, MHPC

Subject: Initial Archaeology Review

Project: MHPC #1599-22, WIN 26290; Thomaston

New construction of a bicycle/pedestrian pathway along Route 1, beginning at Fish Street, across Mill River to connect existing walking and biking trails to the east bank residential district and the Knox Museum on Old High Street.

Dear Julie,

Review of the above referenced proposed project submitted September 7, 2022 found no concern for potential impacts to pre-contact archaeological properties. Regarding potential impacts to post-contact historic properties, several historic buildings are depicted on the south side of the Mill River crossing. Please provide plans for the proposed work when they are available so that potential impacts can be better assessed. Once plans are received a fieldcheck may be necessary, but we will wait on that until receiving the plans.

STATE OF MAINE
Memorandum

Date: November 7, 2024

To: Julie Senk, Historic Preservation Coordinator, MaineDOT Environmental Office

From: Leith Smith, MHPC

Subject: Results of Field Check for potential archaeological concerns.

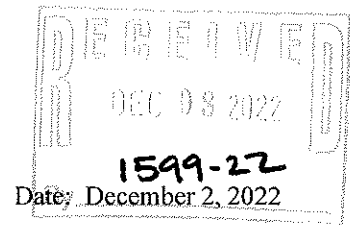
Project: MHPC #1599-22; WIN 26290.00; Thomaston
Construction of multiuse bicycle/pedestrian path on the south side of Route 1 between Fish Street and Route 131/High Street.

Dear Julie,

After reviewing our archaeological survey records and maps, including historic maps and surficial geology maps, for the above referenced proposed project, we determined that there were no concerns for potential impacts to pre-contact archaeological sites. The concern for impacts to post-contact archaeological sites was based on historic map and data that documented the presence of many buildings in this area in the 19th and early 20th centuries. A field check conducted by John Mosher 5, November, 2024 found much of the area to have been covered with fill when the present culvert was constructed in 1962. As a result the proposed work has been determined to have no impacts to post-contact archaeological sites. In following the procedures specified in the Federal Highway/MHPC/MDOT programmatic agreement, we recommend a finding of No Effect for impacts to archaeological properties.

STATE OF MAINE

Memorandum



To: Kirk F. Mohny, MHPC
From: Julie Senk, Maine DOT/ENV
Subject: Section 106 request for concurrence
Project: Thomaston 26290.00, MHPC #1599-22
Scope: Multi-use Path Construction

The Maine DOT has reviewed this project pursuant to the Maine Programmatic Agreement (PA) and Section 106 of the National Historic Preservation Act of 1966, as amended.

The proposed project consists of multi-use path construction across Mill River along US Route 1 (Main Street), Route 131 (High Street), Old High Street, and Fish Street in Thomaston.

In accordance with 36 CFR Part 800.4, the following identification efforts of historic properties were made:

- 800.4(a) (1) - The Area of Potential Effect (APE) includes properties/structures adjacent to the multi-use path and within the project limits. The project limits are defined by the path and the immediately adjacent area, as well as potential approach roadway and intersection improvements nearby. Properties/structures adjacent to this project limit are considered to be within the APE. The APE is shown as a red polygon on the attached map.
- 800.4(a) (2) - Review of existing information consisted of researching the National Register and MHPC survey databases. The Maine Historic Preservation Commission Archaeological staff is currently reviewing this undertaking.
- 800.4(a) (3) - The Town of Thomaston, along with applicable historical societies, were contacted via email and asked to comment on knowledge of, or concerns with, historic properties in the area, and any issues with the undertaking's effect on historic properties. The Town was also requested to provide information regarding local historic societies or groups. No replies were received.
- 800.4(a) (4) - Emails outlining project location and scope were sent to the 4 federally recognized Tribes in Maine. The Mi'kmaq Nation replied with no concern.
- 800.4(c) - The Maine DOT conducted historic architectural surveys within the APE to determine if properties met National Register criteria. Maine Historic Preservation Commission Archaeological staff is currently reviewing this undertaking. **The Maine DOT has determined that 3 architectural properties are contributing resources to a National Register of Historic Places-listed historic district and one is individually eligible for listing.**

In accordance with the PA and 36 CFR Part 800, please reply with your concurrence or objection to the determination of eligibility for listing in the National Register of Historic Places within 30 days. If more information is deemed necessary, please supply a list of the specific resources in question.

Please contact me at Julie.Senk@maine.gov or 592-3486 if you have any questions. Thank you.

cc: CPD e-file
enc: Architectural survey

CONCUR

Kirk F. Mohny

Kirk F. Mohny,
State Historic Preservation Officer

12/19/22
Date

STATE OF MAINE DEPARTMENT OF TRANSPORTATION

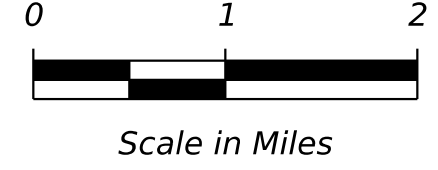
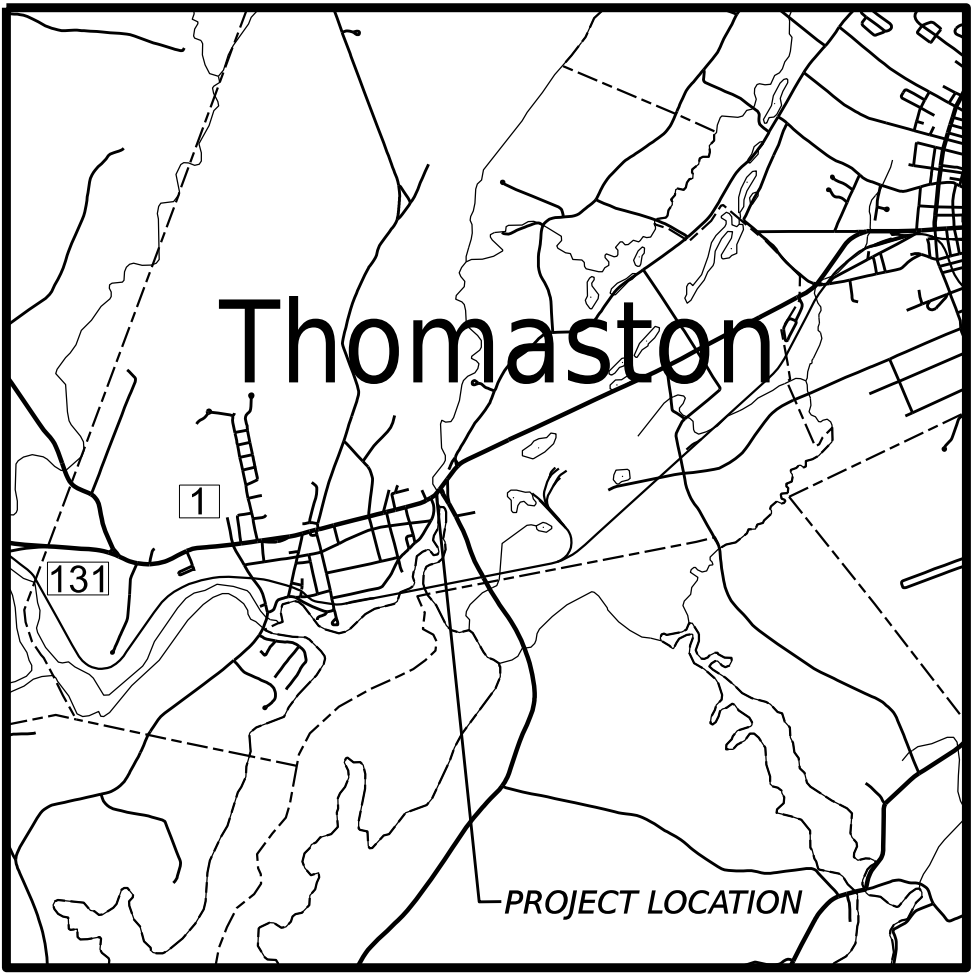
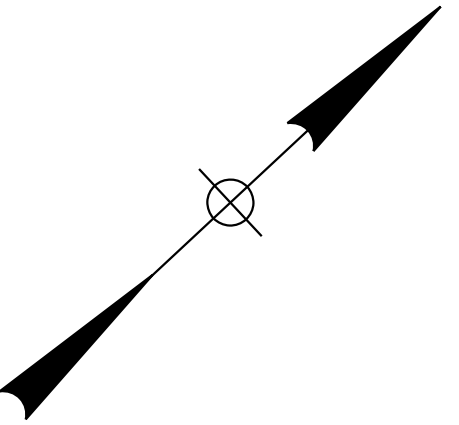
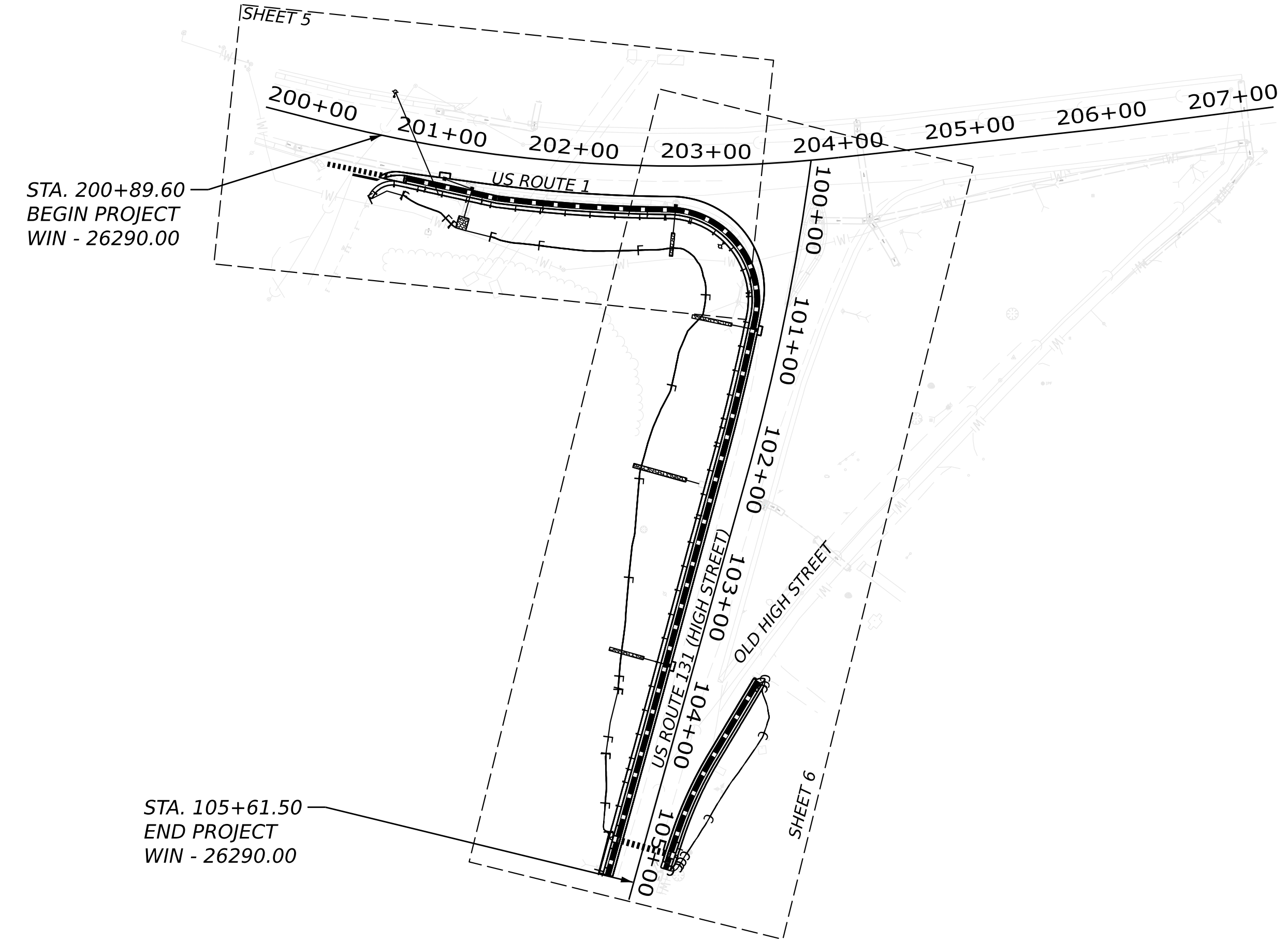


THOMASTON KNOX COUNTY US ROUTE 1 AND ROUTE 131 FEDERAL PROJECT NO. 26290000 MAINEDOT WIN 26290.00 PROJECT LENGTH: 0.25 MILES

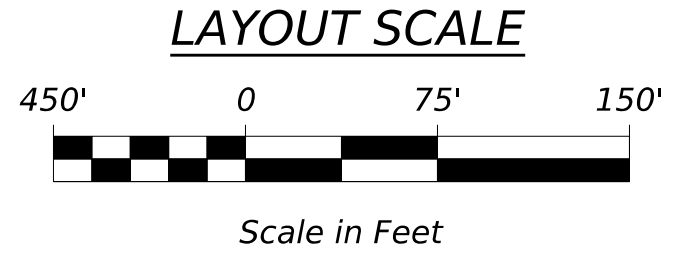
INDEX OF SHEETS	
Description	Sheet No.
Title Sheet	1
Typical Sections	2-3
General Notes	4
Plan	5-6
Cross Sections	7-23

PLAN LEGEND			
Town, County, State	-----	Catch Basins	Existing Proposed
Property Lines	- - - - -	Manholes	Existing Proposed
R/W Lines-Existing	-----	Proposed Underdrain	-----
R/W Lines-Proposed	-----	Proposed Ditch	-----
Culvert-Existing	-----	Existing Ditch	-----
Culvert Proposed	-----	Utility Poles	Existing Proposed
Curbing Existing	-----	Fire Hydrants	Existing Proposed
Curbing Proposed	-----	Existing Water Line	-----
Type 1	-----	Existing San. Sewer	-----
Type 3	-----	Existing San. Sewer Manhole	-----
Type 5	-----	Guardrail-Existing	-----
Outline of Bodies of Water	-----	Guardrail-Proposed	-----
Exposed Bedrock	-----	Centerline-Existing	----- 10+00
Buildings	-----	Centerline-Proposed	-----
Trees	Conifer Deciduous	Travelway-Existing	-----
Tree Line	-----	Travelway-Proposed	-----
Clearing Limit Line	CLL -----		
Railroad	-----		
Boring	HB-XXX-###	Probe	P-#.##X
Pavement Core	● PC-#		## = Depth
Test Pit	■ TP-XXX-###		X = W (Weathered Rock)
			R (Refusal)
			NR (No Refusal)

TRAFFIC DATA	US ROUTE 1	ROUTE 131	OLD HIGH ST.
Current (2023) AADT	11,931	5,900	117
Future (20XX) AADT	N/A	N/A	N/A
DHV - % of AADT	N/A	N/A	N/A
Design Hour Volume	N/A	N/A	N/A
% Heavy Trucks (AADT)	N/A	N/A	N/A
Directional Distribution (DHV)	N/A	N/A	N/A
Design Speed (mph)	35 MPH	35 MPH	UNPOSTED
Functional Class:	OTHER PRINCIPAL	MAJOR	LOCAL
Corridor Priority	1	3	5



LOCATION MAP

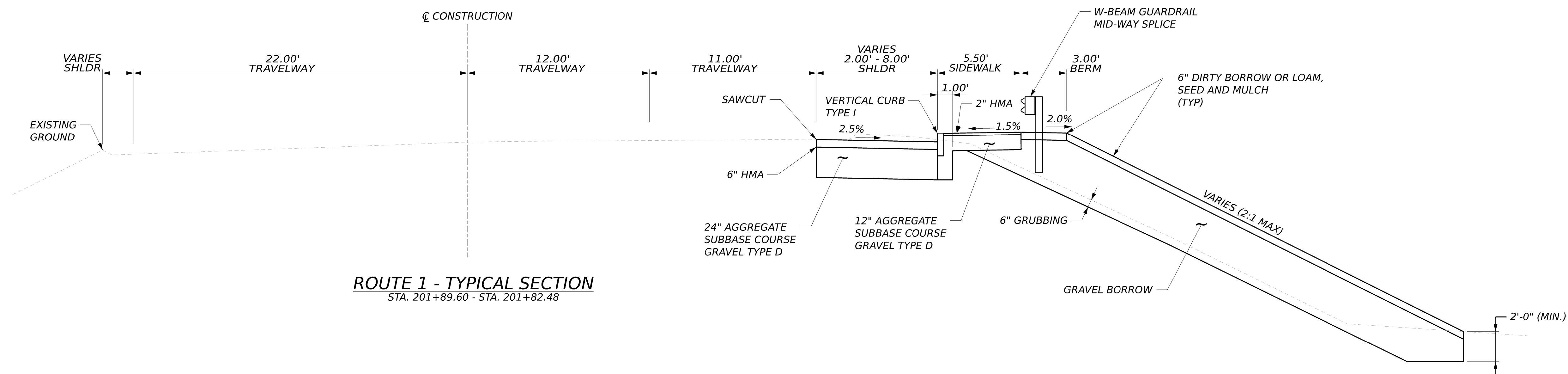
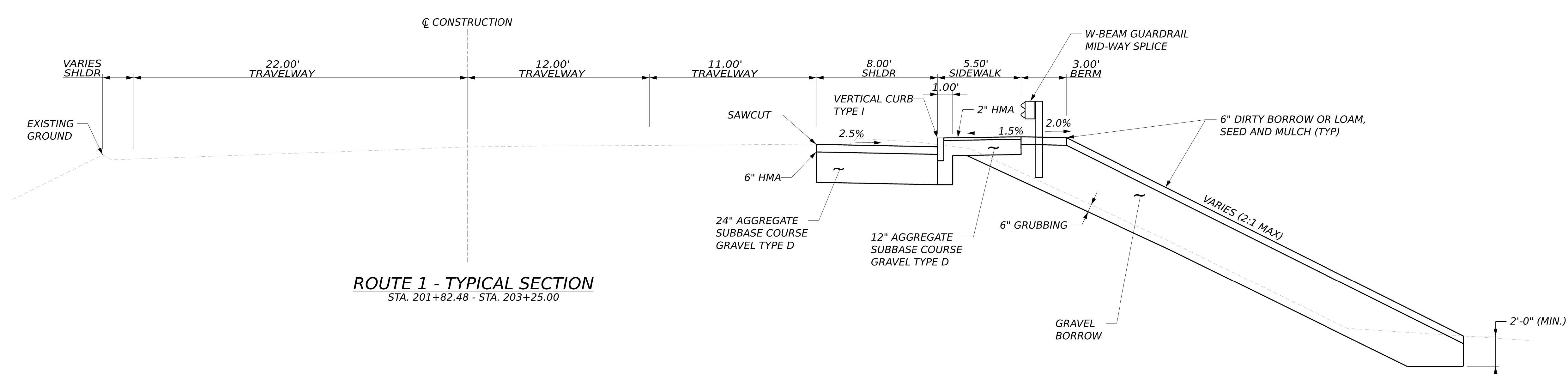


LAYOUT SCALE

PROJECT LOCATION:	INTERSECTIONS OF ROUTE 1 & 131 AND ROUTE 131 & OLD HIGH STREET
PROGRAM AREA:	MULTIMODAL PROGRAM
SCOPE OF WORK:	BICYCLE/PEDESTRIAN ON-ROAD SIDEWALK/TRAIL

26290000 WIN 26290.00

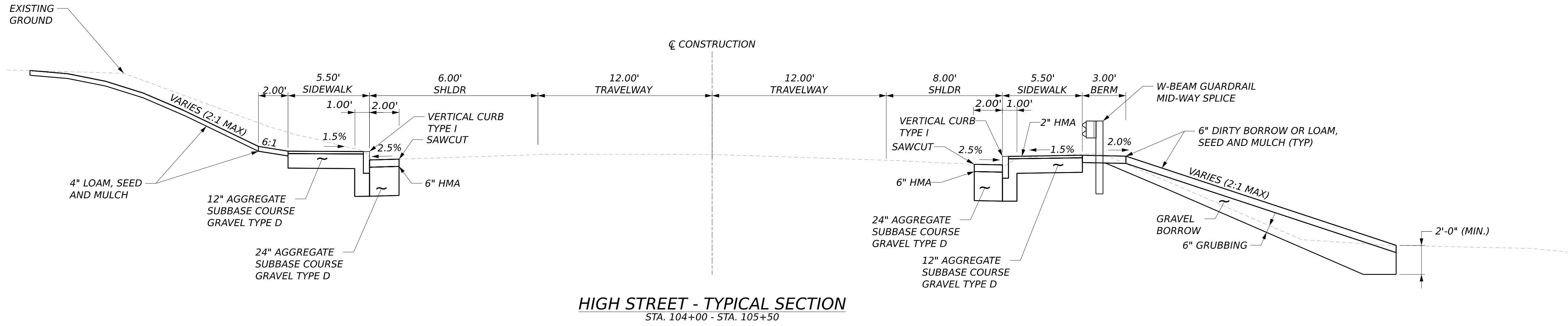
STATE OF MAINE DEPARTMENT OF TRANSPORTATION	APPROVED	DATE
COMMISSIONER:	CHIEF ENGINEER:	
PROJECT INFORMATION	SIGNATURE	P.E. NUMBER
PROGRAM	DESIGNER	CONTRACTOR
MULTIMODAL	A. GORREAU II	K. WOOD
PROJECT MANAGER	CONSULTANT	PROJECT RESIDENT
DESIGNER	KLEINFELDER	CONTRACTOR
PROJECT COMPLETION DATE		
THOMASTON US ROUTE 1 AND ROUTE 131	TITLE SHEET	
SHEET NUMBER	1	
OF	23	



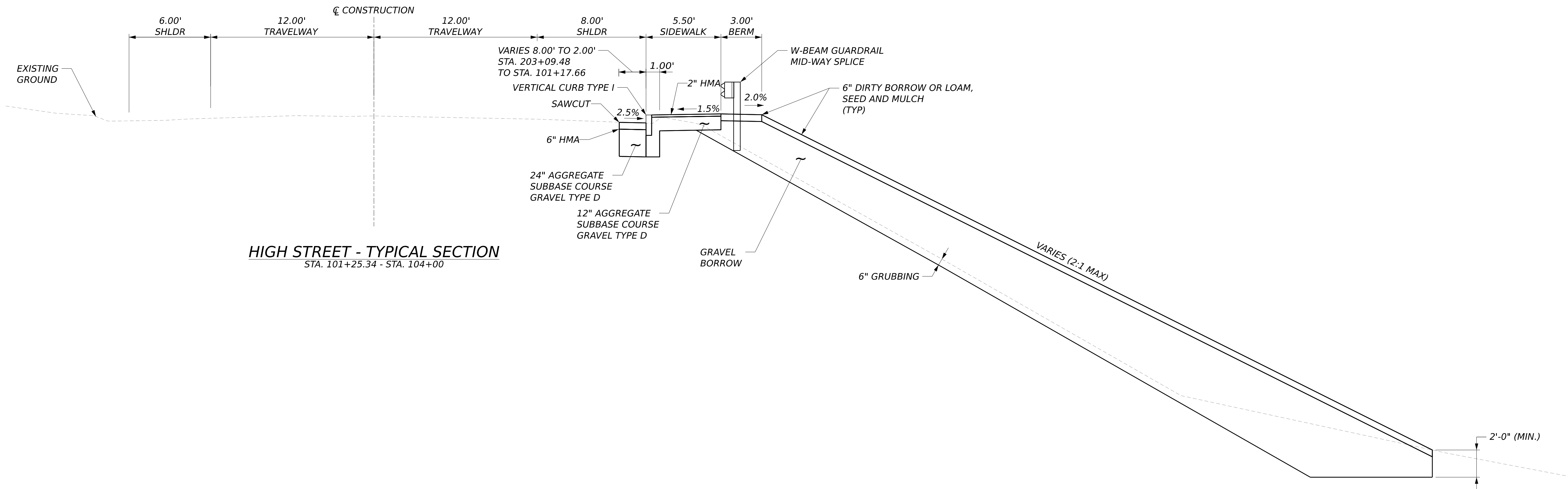
- NOTES:
1. THE PAVEMENT, BASE AND SUBBASE DEPTHS AS SHOWN ON THE PLANS ARE INTENDED TO BE NOMINAL.
 2. THE LOAM QUANTITY CALCULATION IS BASED ON A 6" LOAM OR DIRTY BORROW DEPTH. THE ACTUAL DEPTH MAY VARY. SEE GENERAL NOTE 22.
 3. THE STATIONING SHOWN UNDER EACH TYPICAL IS APPROXIMATE.

STATE OF MAINE		DEPARTMENT OF TRANSPORTATION		26290000		WIN		026290.00		HIGHWAY PLANS	
THOMASTON		US ROUTE 1 AND ROUTE 131		TYPICAL SECTIONS		SHEET NUMBER		2		OF 23	
PROJ. MANAGER	A. CORNEAU II	CHECKED-REVIEWED	K. WOOD	DESIGN-DETAILED	K. WOOD	DESIGN-DETAILED	DESIGN-DETAILED	REVISIONS 1	REVISIONS 2	REVISIONS 3	REVISIONS 4
BY	S. MARTIN	DATE	12/2024	BY	M. MATZKE	DATE	12/2024	SIGNATURE	P.E. NUMBER	DATE	

Username: SMartin Date: 12/20/2024



HIGH STREET - TYPICAL SECTION
STA. 104+00 - STA. 105+50



HIGH STREET - TYPICAL SECTION
STA. 101+25.34 - STA. 104+00

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
WIN
026290.00
HIGHWAY PLANS

SIGNATURE
P.E. NUMBER
DATE

PROJ. MANAGER	BY	DATE
A. GORNEAU	S. MARTIN	12/2024
DESIGN-DETAILED	K. WOOD	12/2024
CHECKED-REVIEWED	M. MAITRE	12/2024
DESIGN-DETAILED		
REVISIONS 1		
REVISIONS 2		
REVISIONS 3		
REVISIONS 4		
FIELD CHANGES		

THOMASTON
US ROUTE 1 AND ROUTE 131
TYPICAL SECTIONS

SHEET NUMBER
3
OF 23

Date: 1/3/2025

Username: SMartin

GENERAL NOTES

1. PAVEMENT THICKNESSES SHOWN ON THE TYPICAL SECTIONS ARE INTENDED TO BE NOMINAL.
2. TRIM ALL TREE BRANCHES TO 20 FEET ABOVE THE PAVEMENT AND 8 FEET ABOVE SIDEWALKS. A TREE SPECIALIST SHALL BE SUBCONTRACTED FOR THIS WORK AND PAYMENT WILL BE MADE BY INVOICE PLUS 5%. ANY TREE BRANCHES DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION WILL BE TRIMMED AT THE CONTRACTOR'S EXPENSE.
3. CLEARING LIMITS SHALL BE 10 FEET BEYOND AND PARALLEL TO THE CONSTRUCTION SLOPE LINES OR AS SHOWN ON THE PLANS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT.
4. ALL CLEARING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT WILL BE MADE. THE ACTUAL LINES FOR CLEARING SHALL BE ESTABLISHED IN THE FIELD BY THE CONTRACTOR AS INDICATED ON THE PLANS AND APPROVED BY THE RESIDENT.
5. STUMP REMOVAL HAS BEEN ESTIMATED UNDER STANDARD SPECIFICATIONS ITEM 201.24, REMOVE STUMP. HOWEVER, WHERE DIRECTED BY THE RESIDENT, STANDARD SPECIFICATIONS ITEM 631.20, STUMP CHIPPER RENTAL (INCLUDING OPERATOR) MAY BE USED TO REMOVE STUMPS.
6. ALL INSLOPE AND DITCHES IN CUT AREAS SHALL BE GRADED AS SHOWN ON THE TYPICALS OR FLATTER, OR AS DIRECTED BY THE RESIDENT.
7. ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN ACCEPTABLE WASTE AREAS REVIEWED BY THE RESIDENT. GRADING, SEEDING AND MULCHING OF WASTE AREAS SHALL BE CONSIDERED INCIDENTAL.
8. GRANULAR BORROW USED TO BACKFILL MUCK EXCAVATION OR IN LOW WET AREAS TO 1 FOOT ABOVE WATER LEVEL OR OLD GROUND SHALL MEET REQUIREMENTS FOR GRANULAR BORROW MATERIAL FOR UNDERWATER BACKFILL AS SPECIFIED IN STANDARD SPECIFICATIONS ITEM 703.19, GRANULAR BORROW.
9. EXISTING INSLOPES IN PROPOSED FILL AREAS SHALL BE BENCHED BY EXCAVATING STEPS OF SUFFICIENT WIDTH TO PERMIT PLACING AND COMPACTING THE FILL MATERIAL ALONG WITH THE MATERIAL REMOVED.
10. ALL PAVED WALKS SHALL BE CONSTRUCTED WITH 12 INCHES OF AGGREGATE SUBBASE COURSE GRAVEL AND 2 INCHES OF HOT MIX ASPHALT UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE RESIDENT.
11. NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT PRIOR APPROVAL OF THE RESIDENT.
12. THE CULVERT SIZES SHOWN ON THE PLANS AND CROSS SECTIONS ARE FOR SMOOTH-LINED PIPES. FOR COMPARABLE CORRUGATED SIZES, SEE THE DRAINAGE TABULATION.
13. FLAT TOPS FOR CATCH BASINS ARE NOT ALLOWED UNLESS NOTED ON THE PLANS OR DIRECTED BY THE RESIDENT.
14. ANY NECESSARY CUTTING OF EXISTING PIPES TO FIT IN AREAS OF PROPOSED CATCH BASINS WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO STANDARD SPECIFICATIONS SECTION 604, MANHOLES, INLETS AND CATCH BASINS.
15. ANY NECESSARY CUTTING OF EXISTING CATCH BASINS TO ALLOW FOR PROPOSED PIPE CONNECTIONS WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO STANDARD SPECIFICATIONS SECTION 603, PIPE CULVERTS AND STORM DRAINS OR STANDARD SPECIFICATIONS SECTION 605, UNDERDRAINS.
16. EXISTING ABANDONED WATER MAINS BROKEN BY THE CONTRACTOR DURING CONSTRUCTION SHALL HAVE THE ENDS PLUGGED WITH BRICK AND MORTAR. COST FOR ALL LABOR AND MATERIAL WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO DIRECT PAYMENT WILL BE MADE.
17. GUARDRAIL END TREATMENTS SHALL BE INSTALLED CONCURRENTLY WITH THE PLACEMENT OF EACH SECTION OF BEAM GUARDRAIL.
18. ALL EXISTING GUARDRAIL REMOVED AND NOT REUSED ON THE PROJECT WILL BECOME THE PROPERTY OF THE CONTRACTOR. REMOVAL AND DISPOSAL SHALL BE CONSIDERED INCIDENTAL TO THE GUARDRAIL ITEMS.
19. TWO REFLECTORIZED FLEXIBLE GUARDRAIL MARKERS (STANDARD SPECIFICATIONS ITEM 606.353, REFLECTORIZED FLEXIBLE GUARDRAIL MARKER) WILL BE INSTALLED AT EACH GUARDRAIL END.
20. LOAM HAS BEEN ESTIMATED FOR DISTURBED LAWN AREAS. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS NOTED ON THE PLANS OR DESIGNATED BY THE RESIDENT.
21. UNLESS OTHERWISE NOTED SEEDING METHOD NO. 1 SHALL BE UTILIZED ON ALL LAWNS AND DEVELOPED AREAS; SEEDING METHOD NO. 2 SHALL BE UTILIZED ON ALL OTHER AREAS.
22. LOAM/DIRTY BORROW SHALL BE PLACED TO A NOMINAL DEPTH OF 4 INCHES IN LAWN AREAS, 6 INCHES OVER THE GRAVEL BORROW BACKFILL, AND 2 INCHES IN ALL OTHER AREAS UNLESS OTHERWISE NOTED OR DIRECTED.
23. ANY BASE PAVEMENT NOT SURFACED BEFORE WINTER WILL REQUIRE TEMPORARY PAVEMENT MARKINGS OF PAINT, BOTH YELLOW CENTERLINE AND WHITE EDGE LINES AND WILL BE CONSIDERED PART OF STANDARD SPECIFICATIONS ITEM 627.78, TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW.
24. ANY DAMAGE TO THE SLOPES CAUSED BY THE CONTRACTOR'S EQUIPMENT, PERSONNEL, OR OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT. ALL WORK, EQUIPMENT, AND MATERIALS REQUIRED TO MAKE REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE.
25. NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT AND LAYOUT OF WORK BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
26. FINAL STRIPING FOR THE PROJECT SHALL BE DONE BY THE CONTRACTOR PER THE STRIPING LAYOUT IN THE CONTRACT DOCUMENTS OR AS PROVIDED BY THE DEPARTMENT. PAYMENT SHALL BE MADE UNDER APPROPRIATE CONTRACT ITEMS.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
WIN
026290.00
HIGHWAY PLANS

SIGNATURE
P.E. NUMBER
DATE

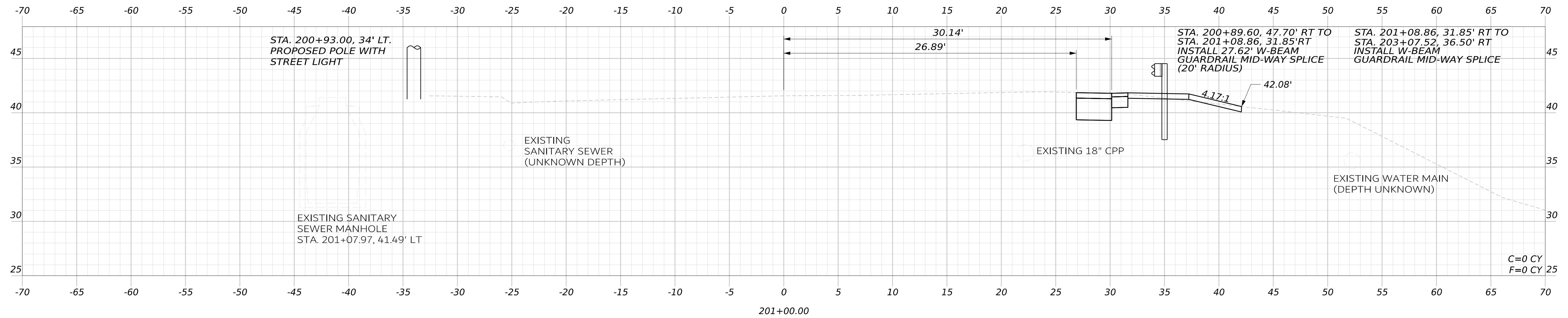
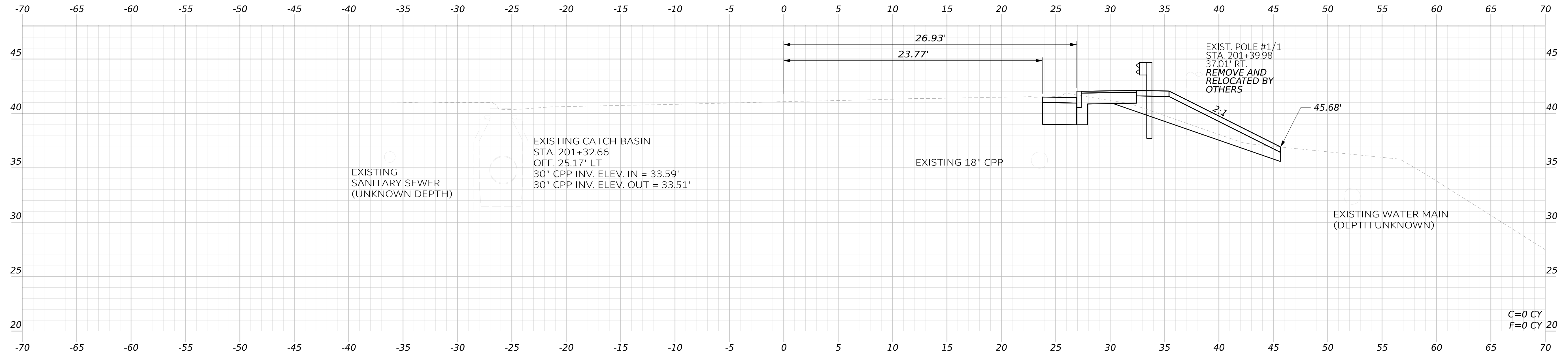
PROJ. MANAGER	A. GORNEAU II	BY	S. MARTIN	DATE	12/20/24
DESIGN-DETAILED	K. WOOD	CHECKED-REVIEWED	M. MATZKE	DATE	12/20/24
DESIGN-DETAILED	K. WOOD	DESIGN-DETAILED			
REVISIONS 1		REVISIONS 1			
REVISIONS 2		REVISIONS 2			
REVISIONS 3		REVISIONS 3			
REVISIONS 4		REVISIONS 4			
FIELD CHANGES		FIELD CHANGES			

THOMASTON
US ROUTE 1 AND ROUTE 131
GENERAL NOTES

SHEET NUMBER
4
OF 23

Date: 12/30/2024

Username: SMartin



STA. 200+55 LIMIT OF WORK
STA. 200+89.60 BEGIN PROJECT

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

26290000
WIN
026290.00
HIGHWAY PLANS

SIGNATURE

P.E. NUMBER

DATE

DATE

BY

PROJ. MANAGER: A. CORNEAU II
DESIGN-DETAILED: K. WOOD
CHECKED-REVIEWED: K. WOOD
DESIGN-DETAILED: K. WOOD

REVISIONS 1

REVISIONS 2

REVISIONS 3

REVISIONS 4

FIELD CHANGES

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

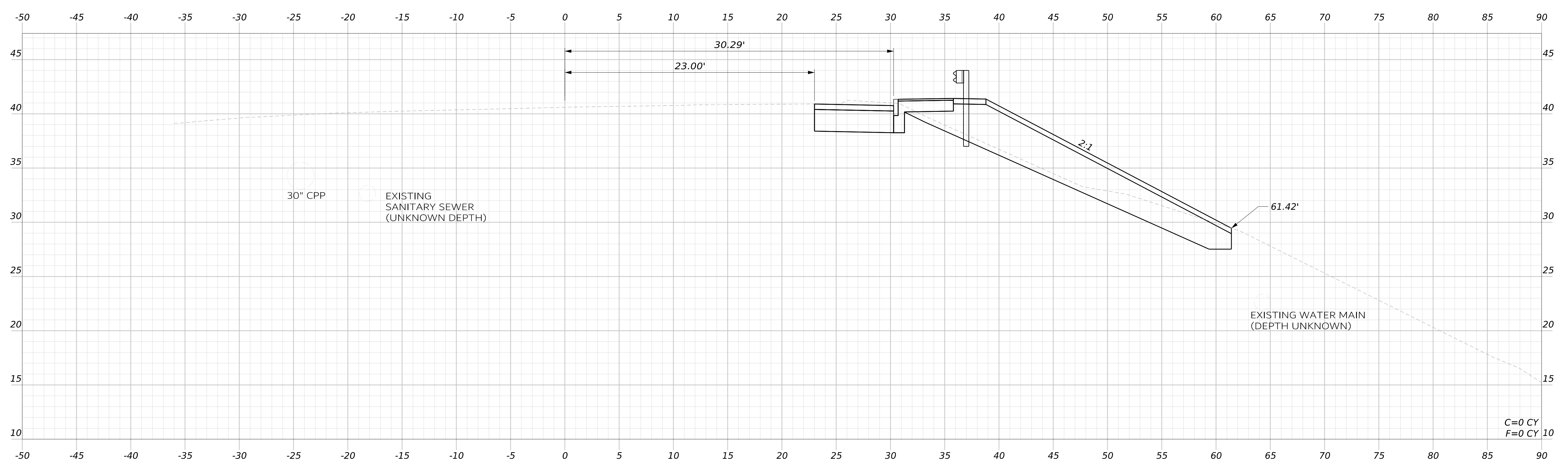
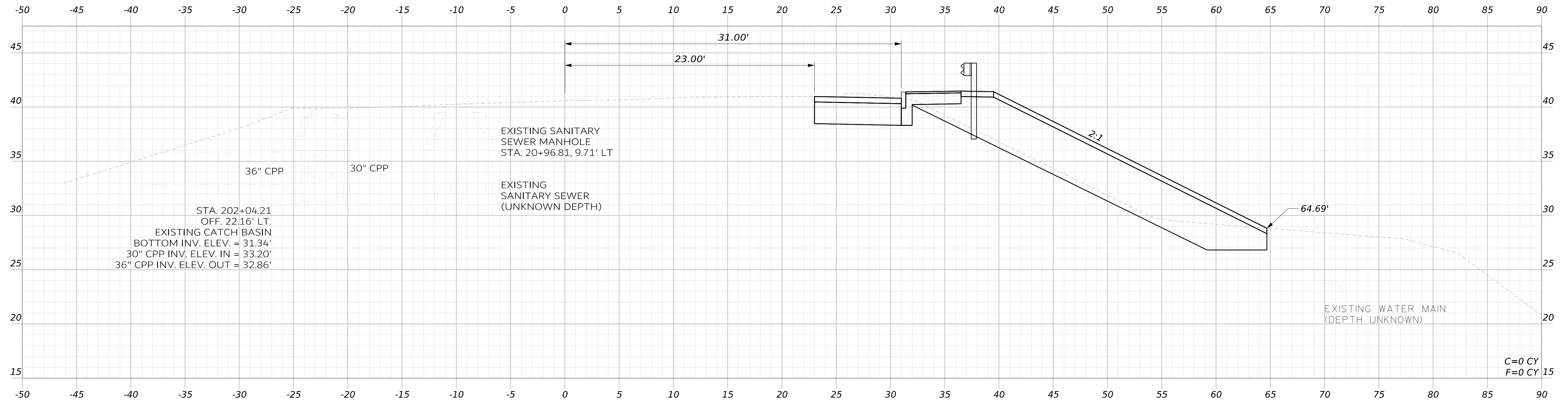
SHEET NUMBER

7

OF 23

Date: 12/30/2024

Username: SMartin



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

26290000

WIN
026290.00
HIGHWAY PLANS

SIGNATURE

DATE

BY
S. MARTIN
M. MATZKE

PROJ. MANAGER	A. CORNEAU II
DESIGN-DETAILED	K. WOOD
CHECKED-REVIEWED	K. WOOD
DESIGN-DETAILED	
REVISIONS 1	
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P.E. NUMBER

DATE

THOMASTON
US ROUTE 1 AND ROUTE 131

CROSS SECTIONS

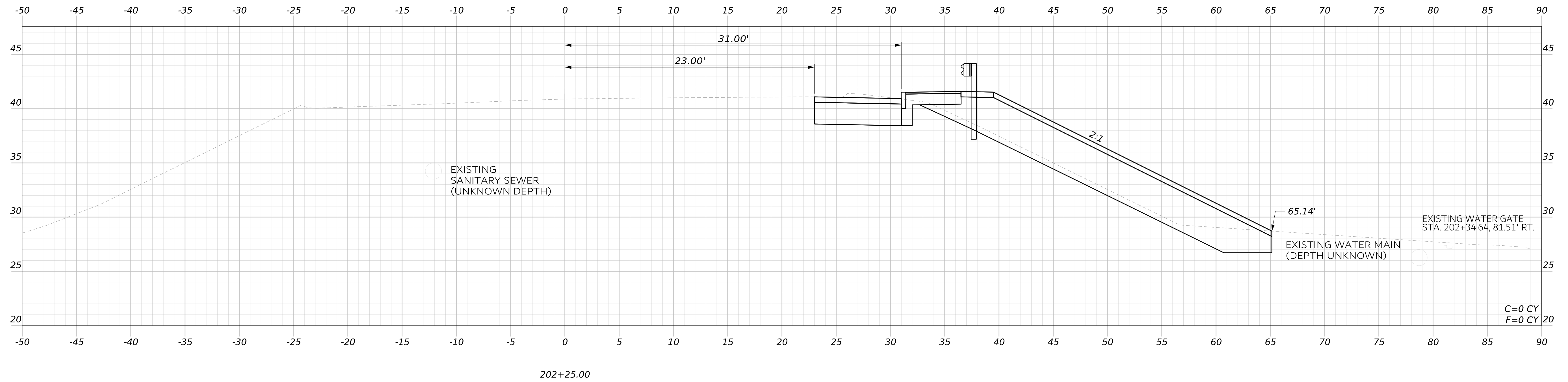
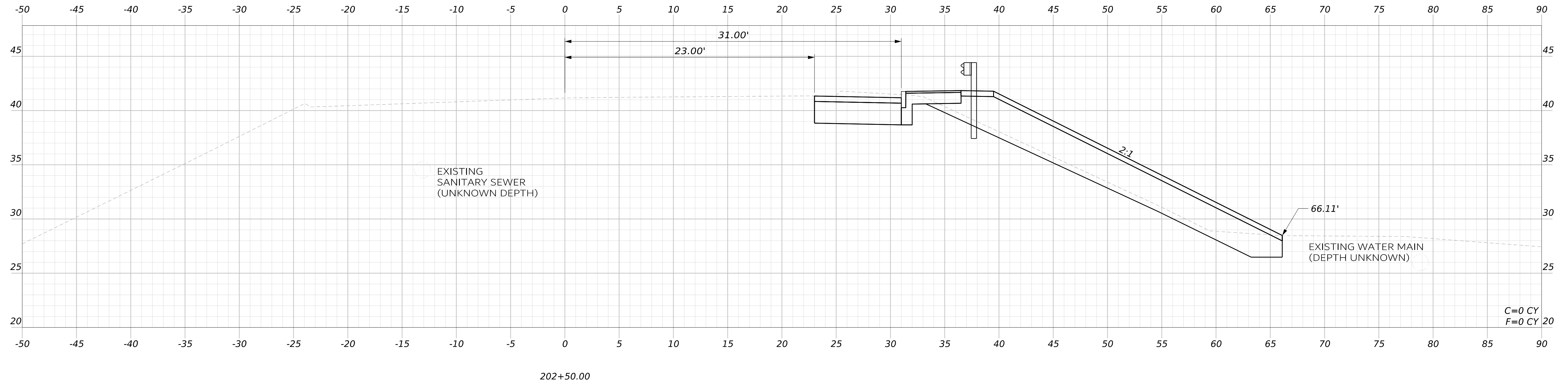
SHEET NUMBER

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OF 23

Date: 12/30/2024

Username: SMartin



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

26290000

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026290.00
HIGHWAY PLANS

SIGNATURE

DATE

BY

PROJ. MANAGER

CHECKED

DESIGNED

REVISIONS 1

REVISIONS 2

REVISIONS 3

REVISIONS 4

FIELD CHANGES

P.E. NUMBER

DATE

THOMASTON

US ROUTE 1 AND ROUTE 131

CROSS SECTIONS

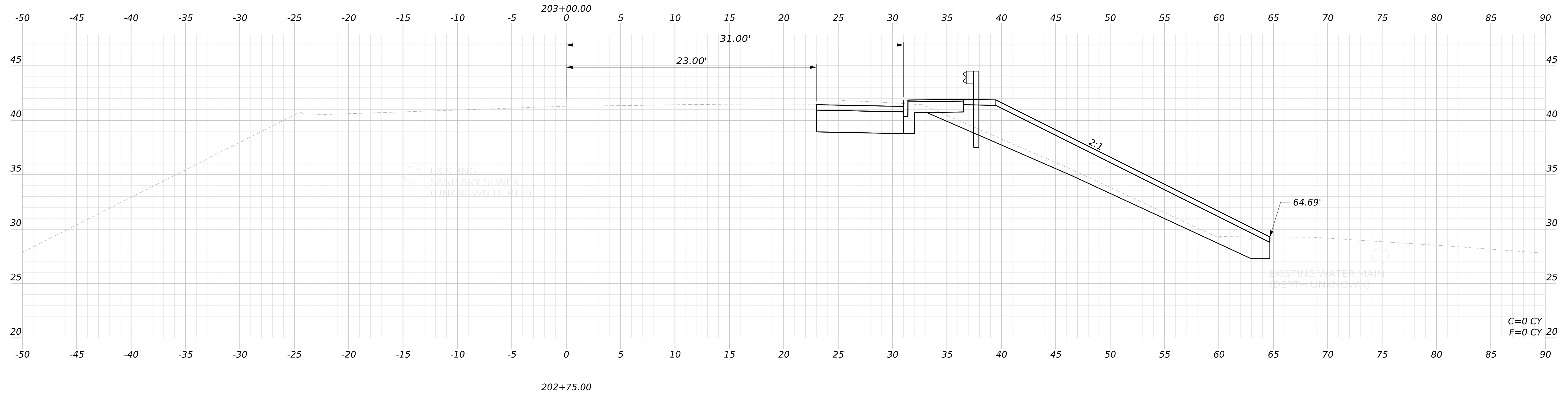
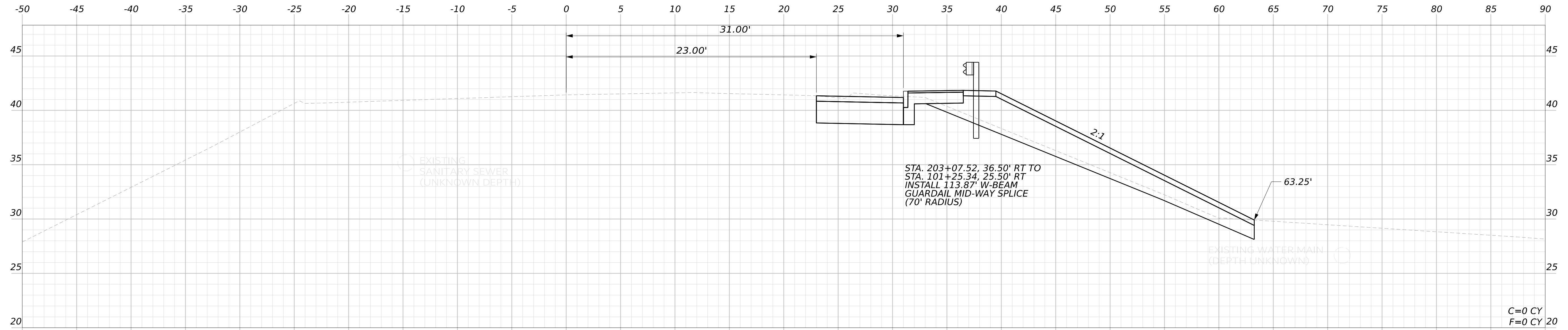
SHEET NUMBER

10

OF 23

Username: SMartin

Date: 12/30/2024



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
WIN 026290.00
HIGHWAY PLANS

SIGNATURE
P.E. NUMBER
DATE

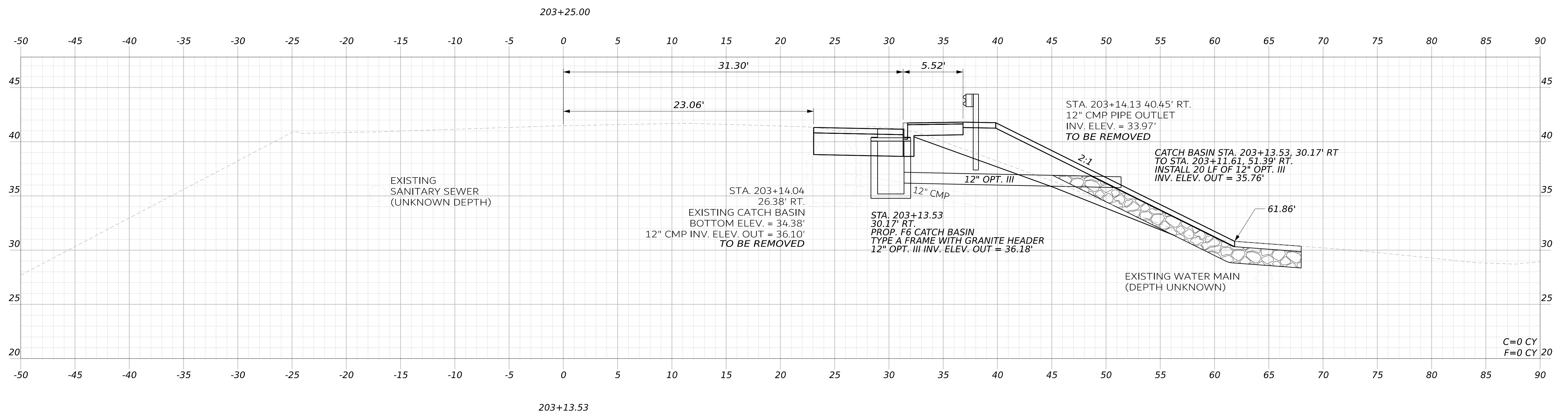
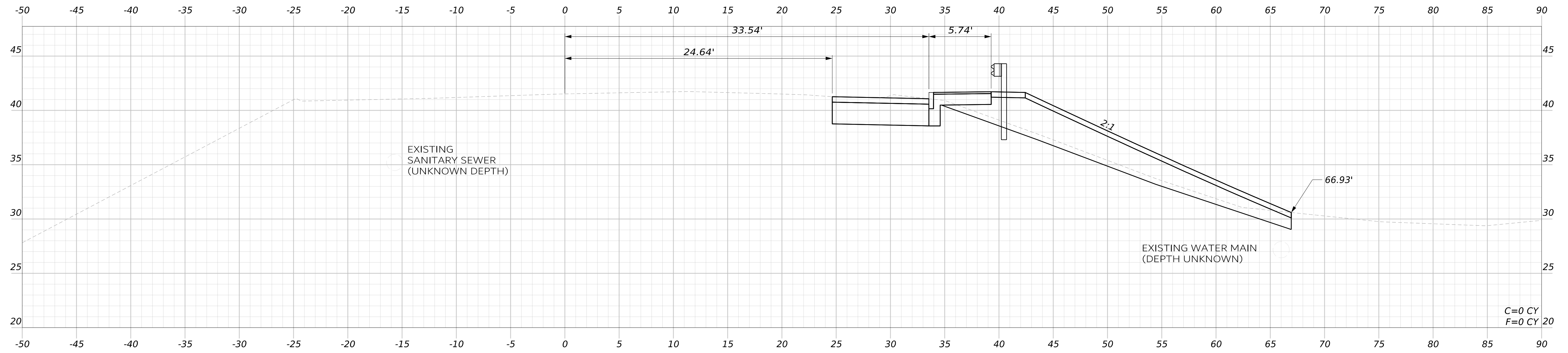
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A. CORNEAU II	S. MARTIN	12/2024
DESIGN-DETAILED	M. WOOD	12/2024
CHECKED-REVIEWED	M. WATZKE	
DESIGN-DETAILED		
REVISIONS 1		
REVISIONS 2		
REVISIONS 3		
REVISIONS 4		
FIELD CHANGES		

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
11
OF 23

PROJ. MANAGER	A. GORNEAU II	DATE	12/2024
DESIGN-DETAILED	K. WOOD	BY	S. MARTIN
CHECKED-REVIEWED	K. WOOD	DATE	12/2024
DESIGN-DETAILED			
REVISIONS 1			
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FIELD CHANGES			

**THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS**

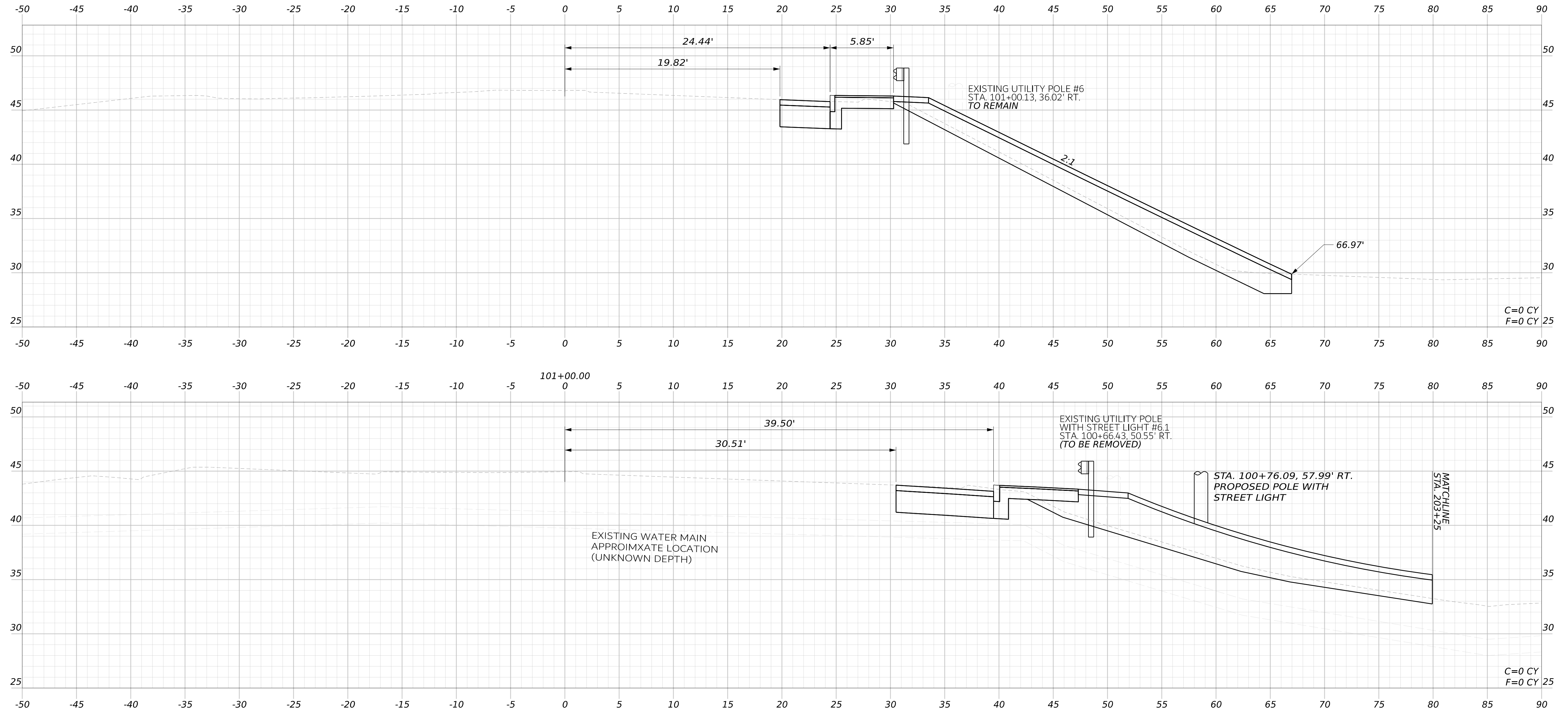


Date: 12/30/2024

Username: SMartin

Date: 12/30/2024

Username: SMartin



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

26290000
WIN
026290.00
HIGHWAY PLANS

SIGNATURE

P.E. NUMBER
DATE

PROJ. MANAGER	BY	DATE
A. CORNEAU II	S. MARTIN	12/2024
K. WOOD	M. MARTINE	12/2024

CHECKED-REVIEWED	DESIGN-DETAILED	REVISIONS
K. WOOD	K. WOOD	1
		2
		3
		4

FIELD CHANGES

THOMASTON US ROUTE 1 AND ROUTE 131

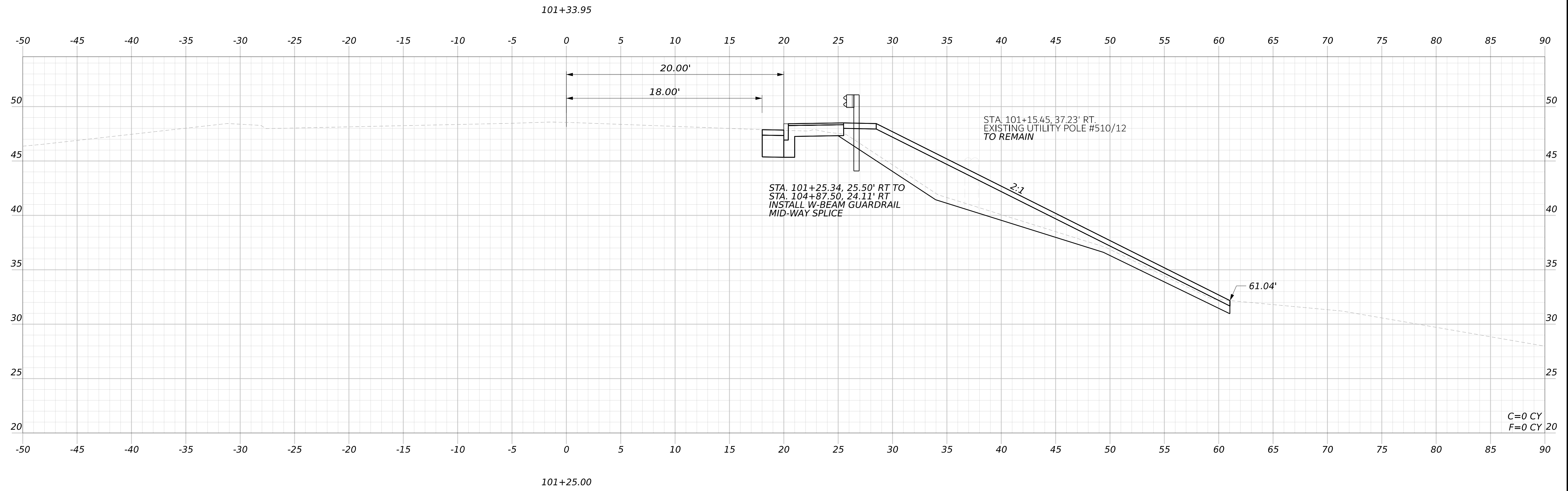
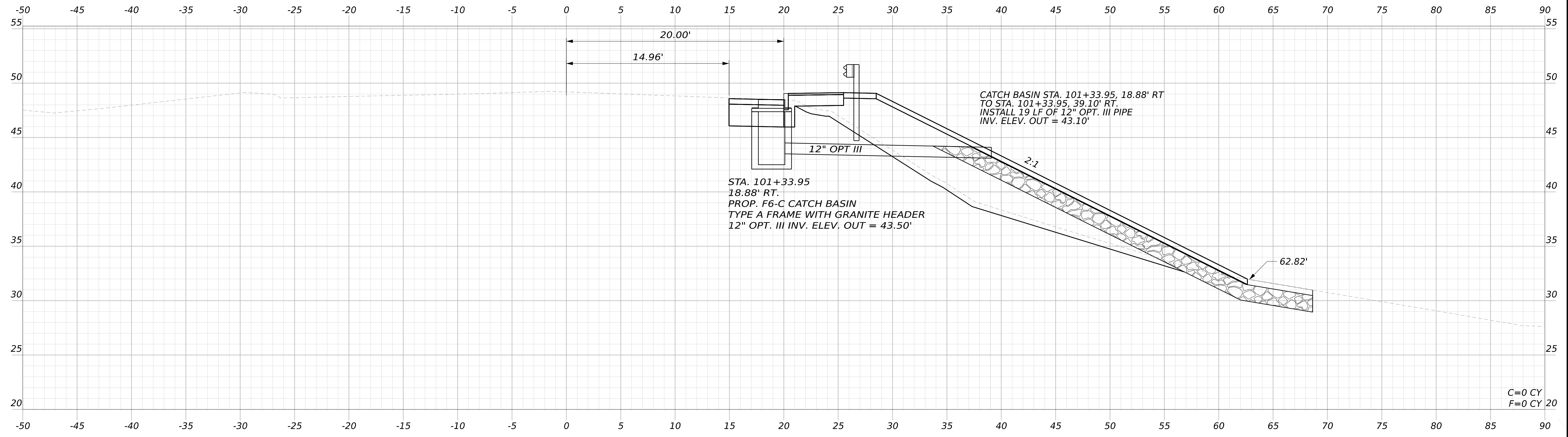
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SHEET NUMBER

13

OF 23

Username: SMartin Date: 12/30/2024



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
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HIGHWAY PLANS

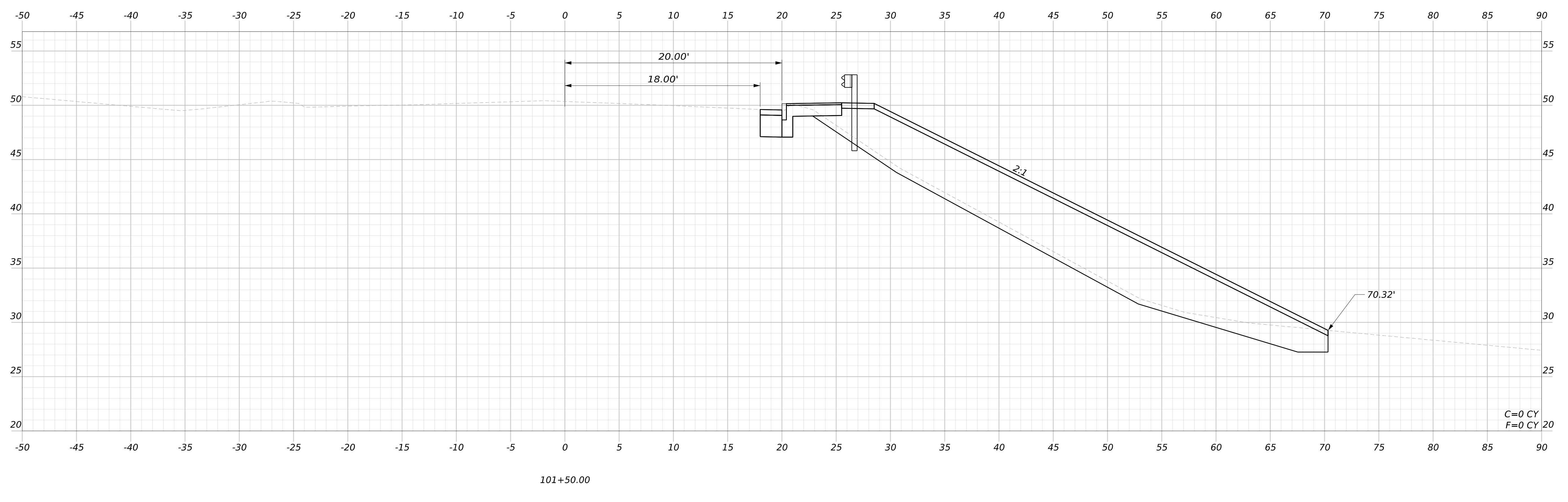
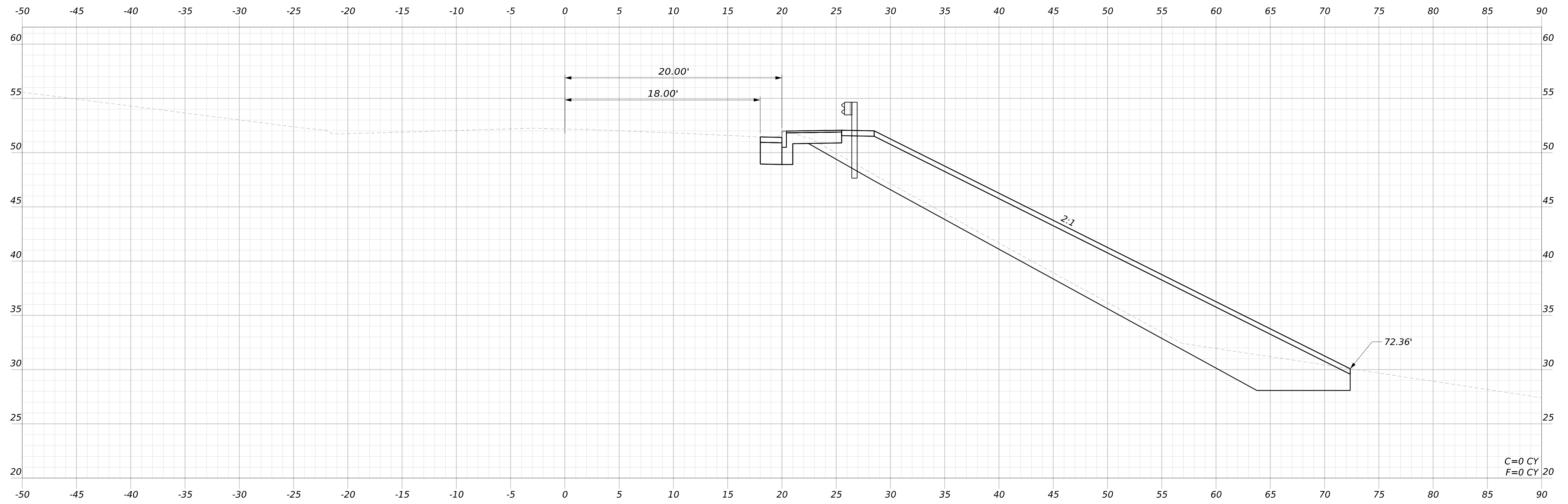
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DATE

PROJ. MANAGER	BY	DATE
A. CORNEAU II	S. MARTIN	12/2024
DESIGN-DETAILED	K. WOOD	12/2024
CHECKED-REVIEWED	M. MATTHEW	
DESIGN-DETAILED		
DESIGN-DETAILED		
REVISIONS 1		
REVISIONS 2		
REVISIONS 3		
REVISIONS 4		
FIELD CHANGES		

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
14
OF 23

Username: SMartin Date: 12/30/2024



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
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WIN
026290.00
HIGHWAY PLANS

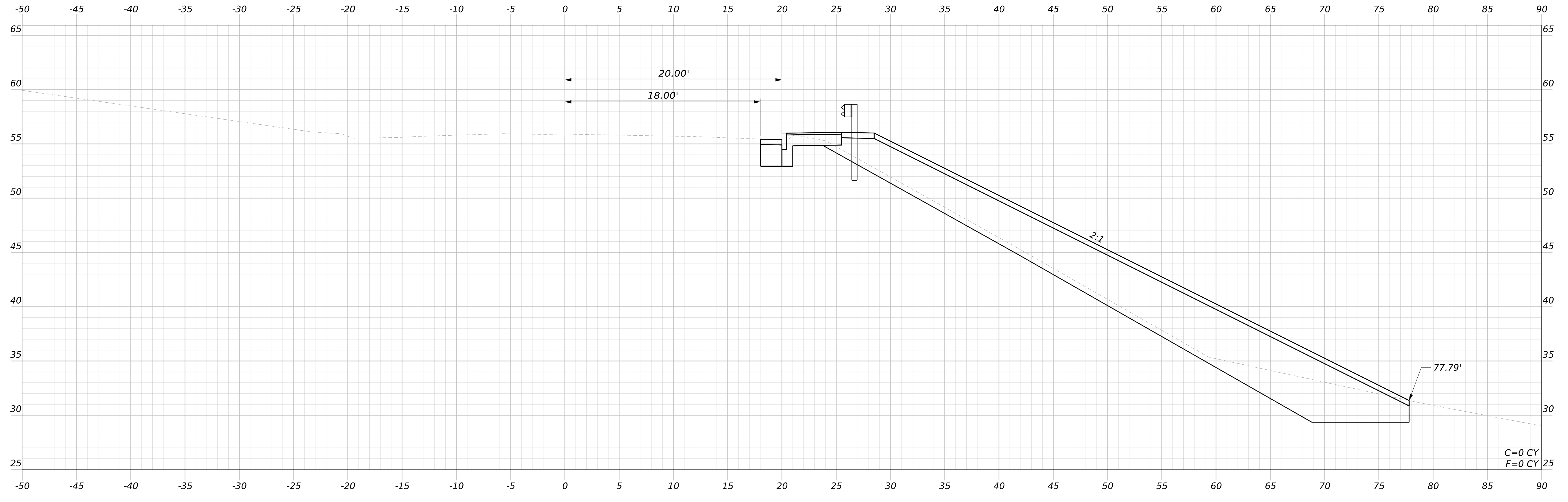
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PROJ. MANAGER	DATE
A. CORNEAU II	12/2024
DESIGN-DETAILED	S. MARTIN
CHECKED-REVIEWED	M. MATTHEW
DESIGN-DETAILED	
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REVISIONS 4	
FIELD CHANGES	

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

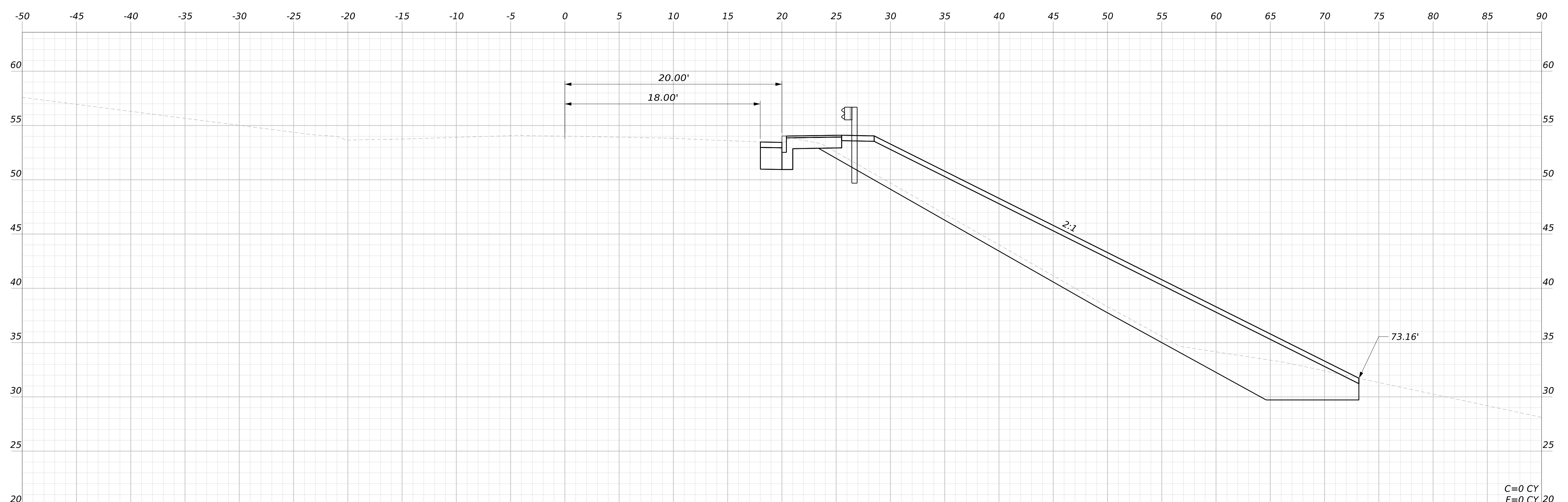
SHEET NUMBER
15
OF 23

Username: SMartin Date: 12/30/2024



102+25.00

C=0 CY
F=0 CY



102+00.00

C=0 CY
F=0 CY

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
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HIGHWAY PLANS

SIGNATURE
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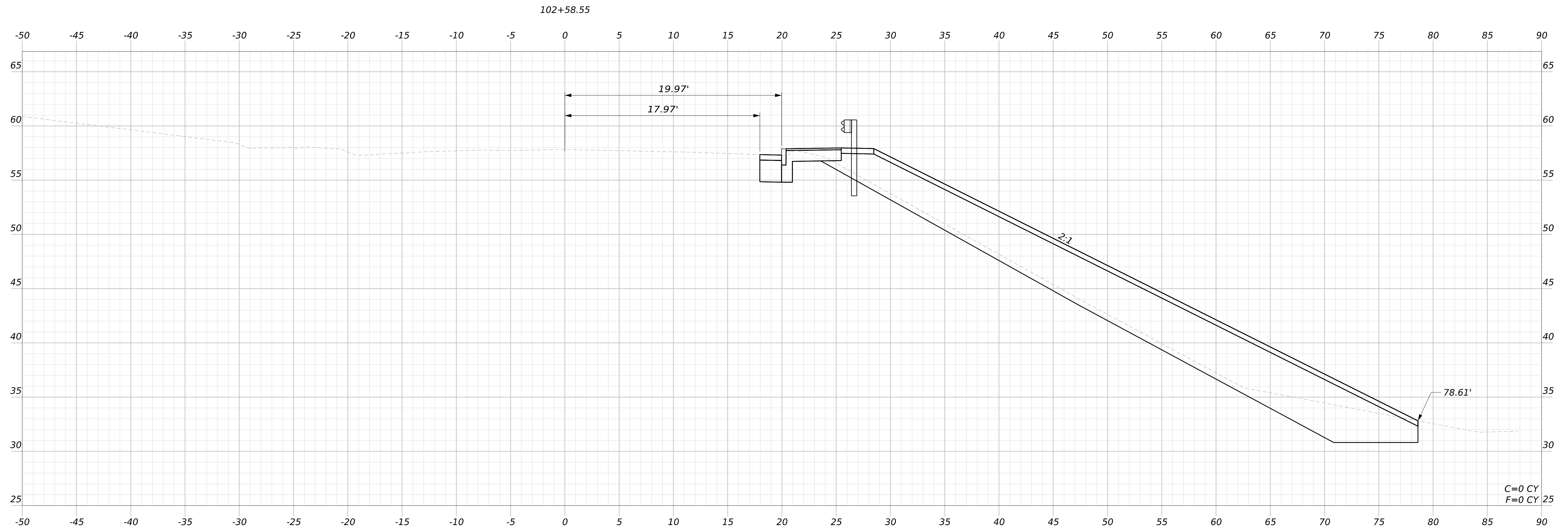
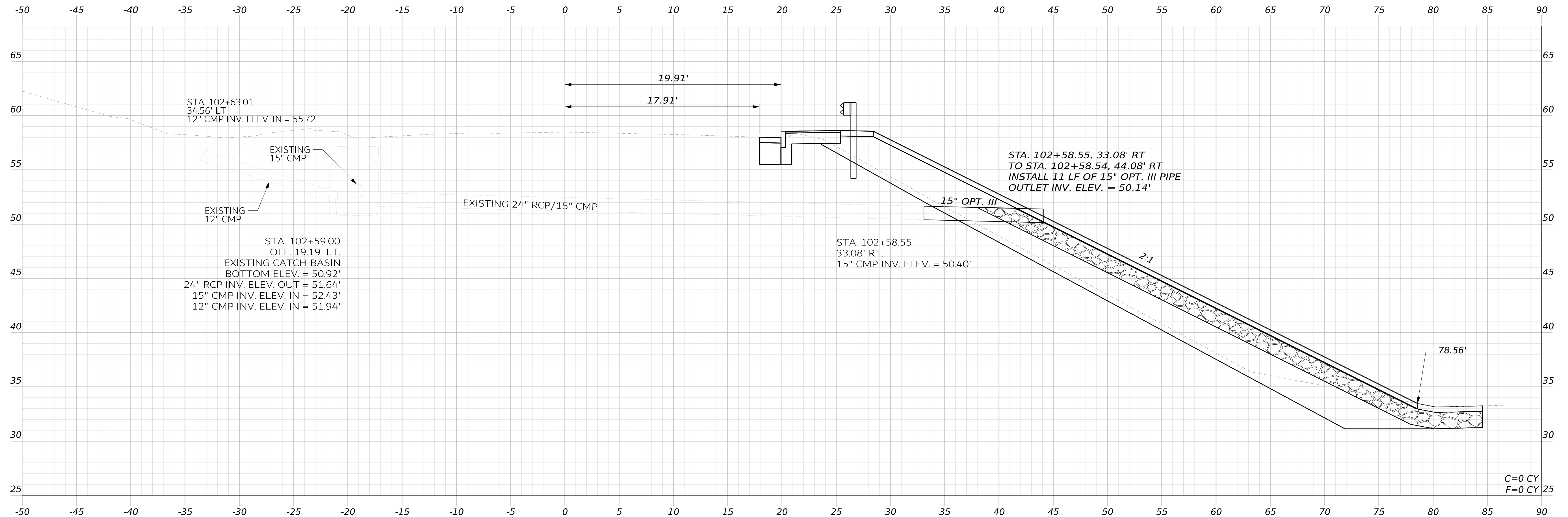
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A. GORNEAU II	12/2024
DESIGN-DETAILED	12/2024
CHECKED-REVIEWED	12/2024
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REVISIONS 2	
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FIELD CHANGES	

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

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OF 23

Username: SMartin

Date: 12/30/2024



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
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WIN
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HIGHWAY PLANS

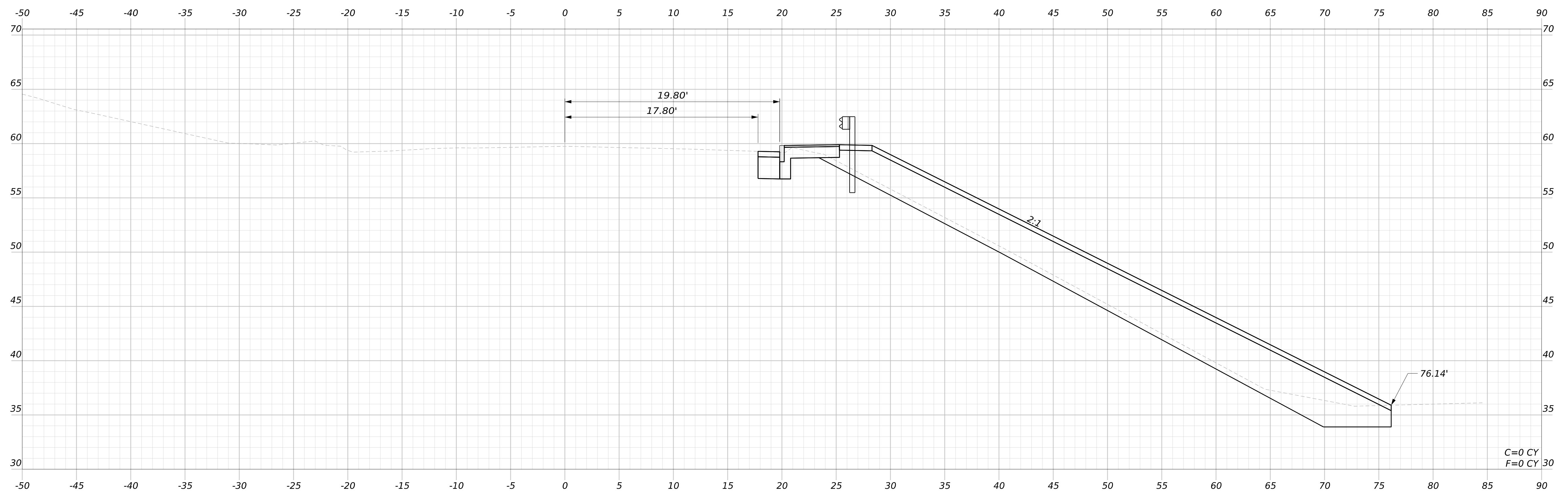
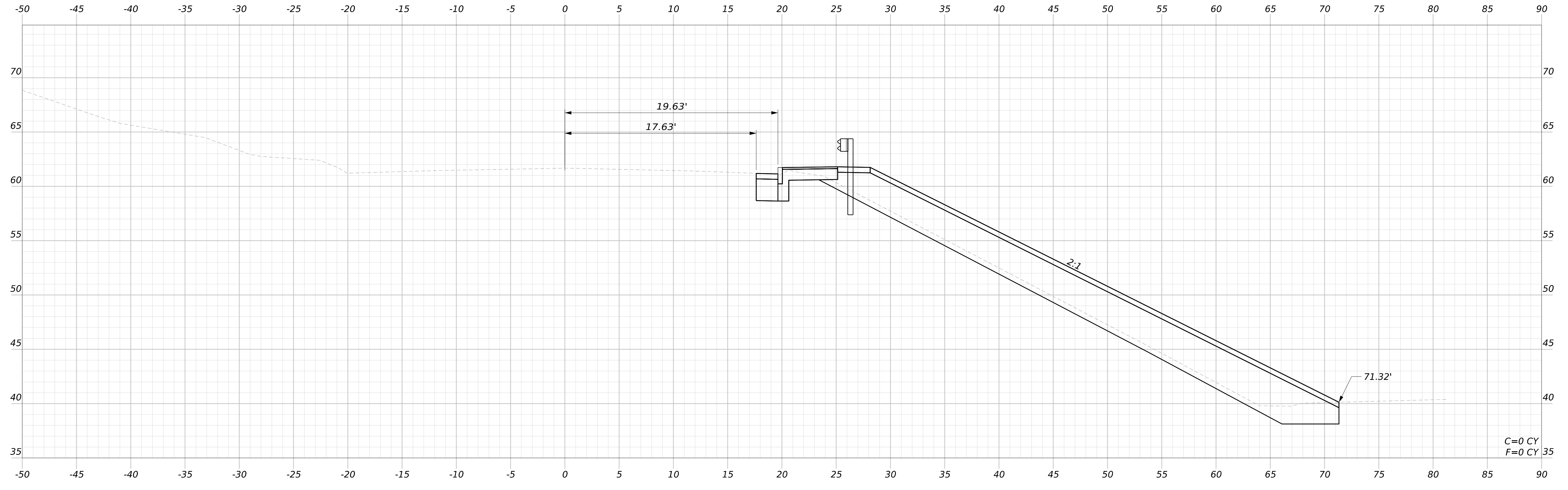
PROJ. MANAGER: A. GORNEAU II
DESIGN-DETAILED: K. WOOD
CHECKED-REVIEWED: K. WOOD
DESIGN-DETAILED: M. MATZKE
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DATE	BY	SIGNATURE	P.E. NUMBER	DATE
12/2024	S. MARTIN			
12/2024	M. MATZKE			

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
17
OF 23

Username: SMartin Date: 12/30/2024



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
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HIGHWAY PLANS

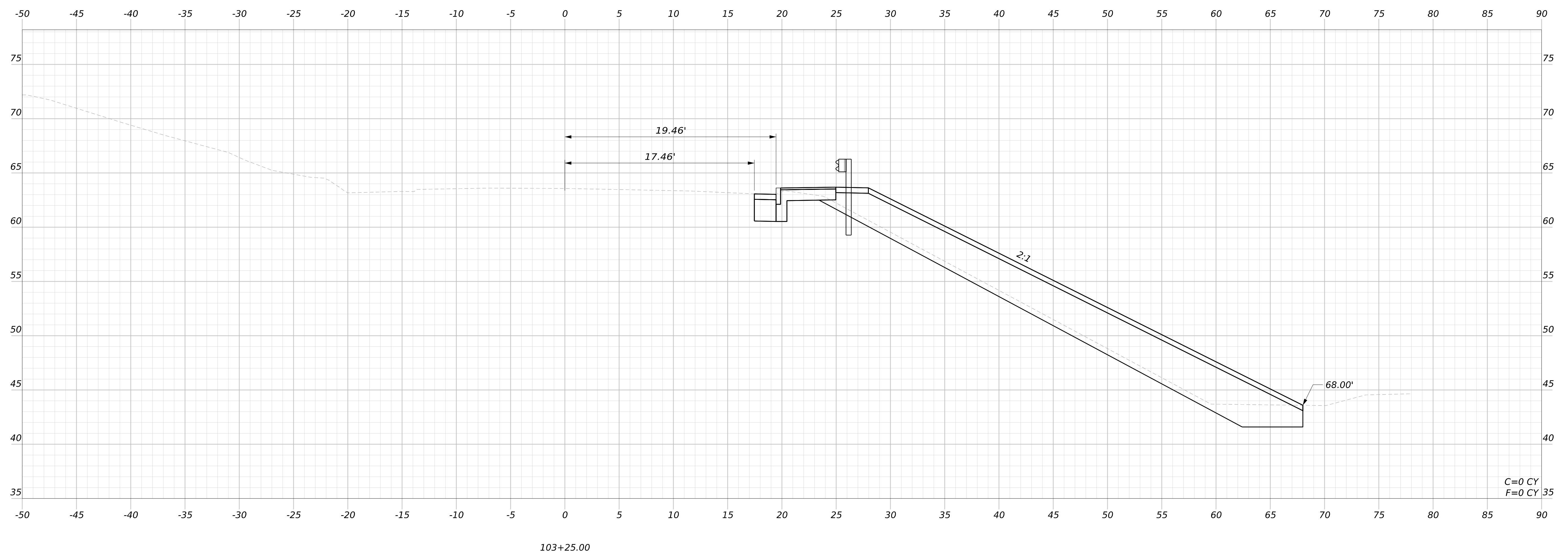
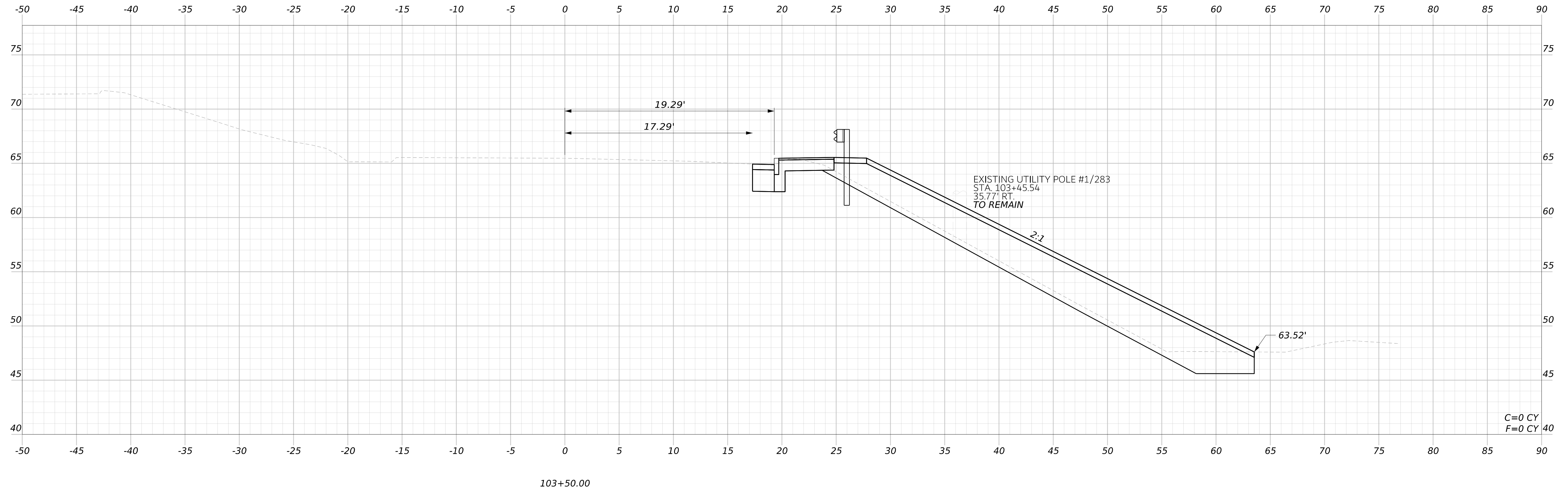
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P.E. NUMBER
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PROJ. MANAGER	BY	DATE
A. CORNEAU II	S. MARTIN	12/2024
DESIGN-DETAILED	K. WOOD	12/2024
CHECKED-REVIEWED	K. WOOD	
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DESIGN-DETAILED		
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REVISIONS 2		
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FIELD CHANGES		

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
18
OF 23

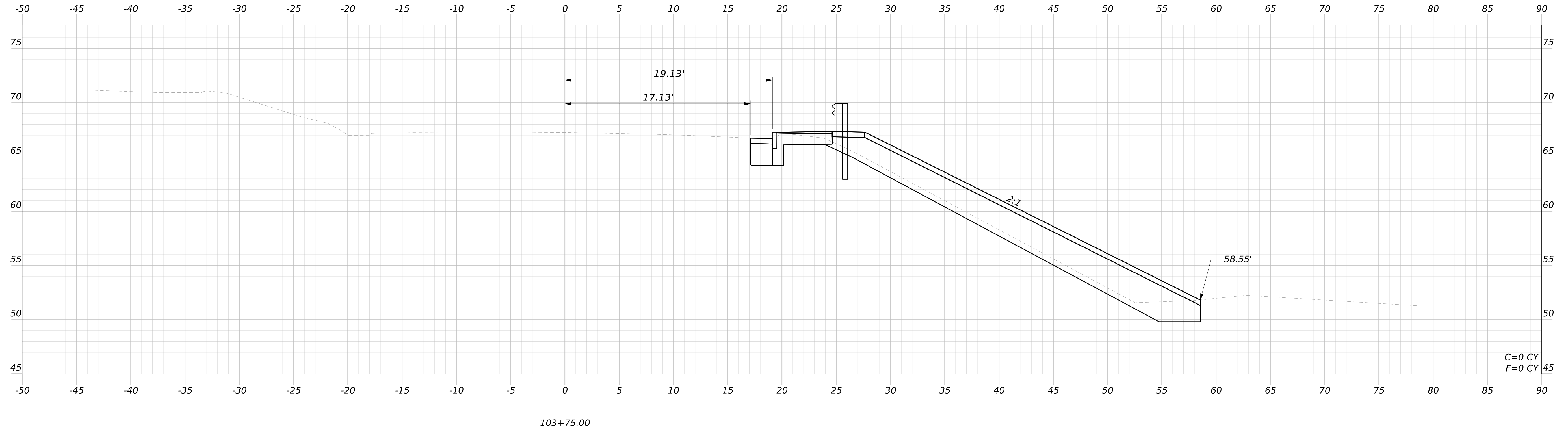
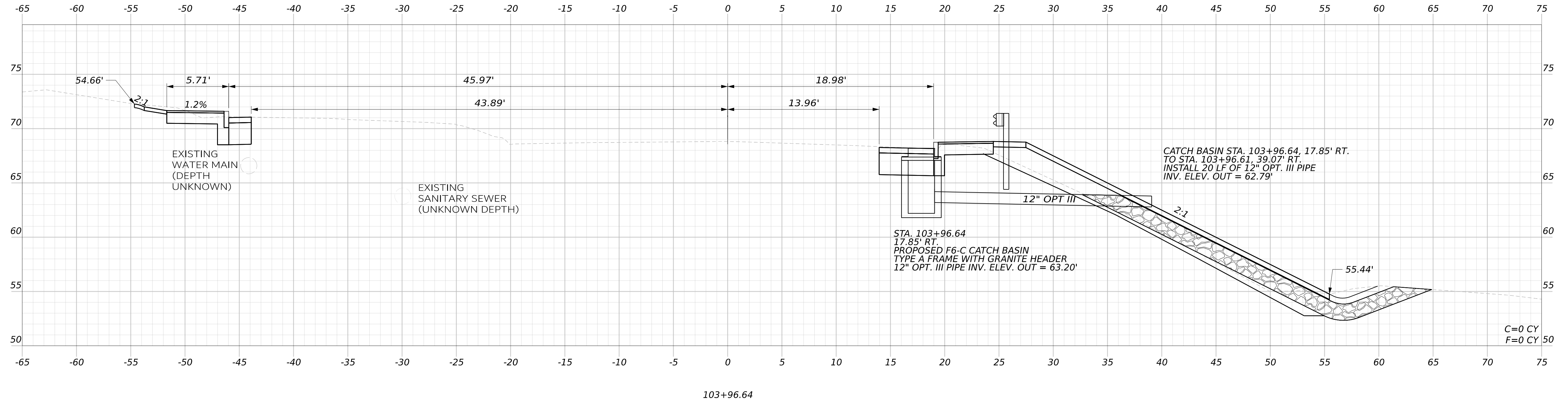
Username: SMartin Date: 12/30/2024



STATE OF MAINE DEPARTMENT OF TRANSPORTATION		26290000		WIN 026290.00		HIGHWAY PLANS	
THOMASTON US ROUTE 1 AND ROUTE 131		CROSS SECTIONS		SHEET NUMBER		19 OF 23	
PROJ. MANAGER	A. CORNEAU II	BY	S. MARTIN	DATE	12/2024	SIGNATURE	
DESIGN-DETAILED	K. WOOD	CHECKED-REVIEWED	K. WOOD		12/2024	P.E. NUMBER	
DESIGN-DETAILED	M. MATZKE	DESIGN-DETAILED				DATE	
REVISIONS 1		REVISIONS 1					
REVISIONS 2		REVISIONS 2					
REVISIONS 3		REVISIONS 3					
REVISIONS 4		REVISIONS 4					
FIELD CHANGES		FIELD CHANGES					

Date: 12/30/2024

Username: SMartin



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
WIN
026290.00
HIGHWAY PLANS

SIGNATURE
P.E. NUMBER
DATE

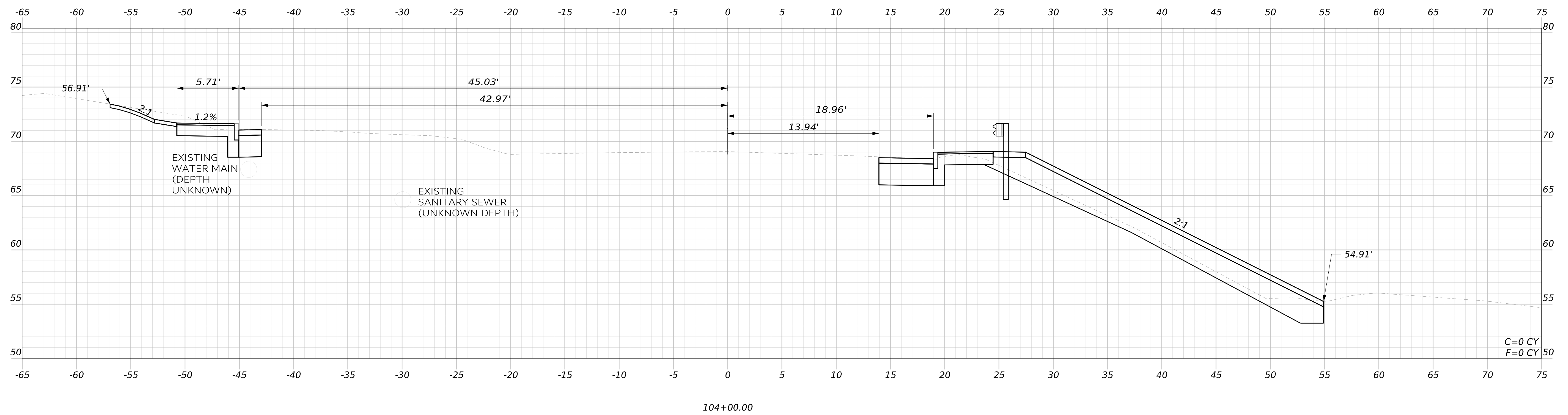
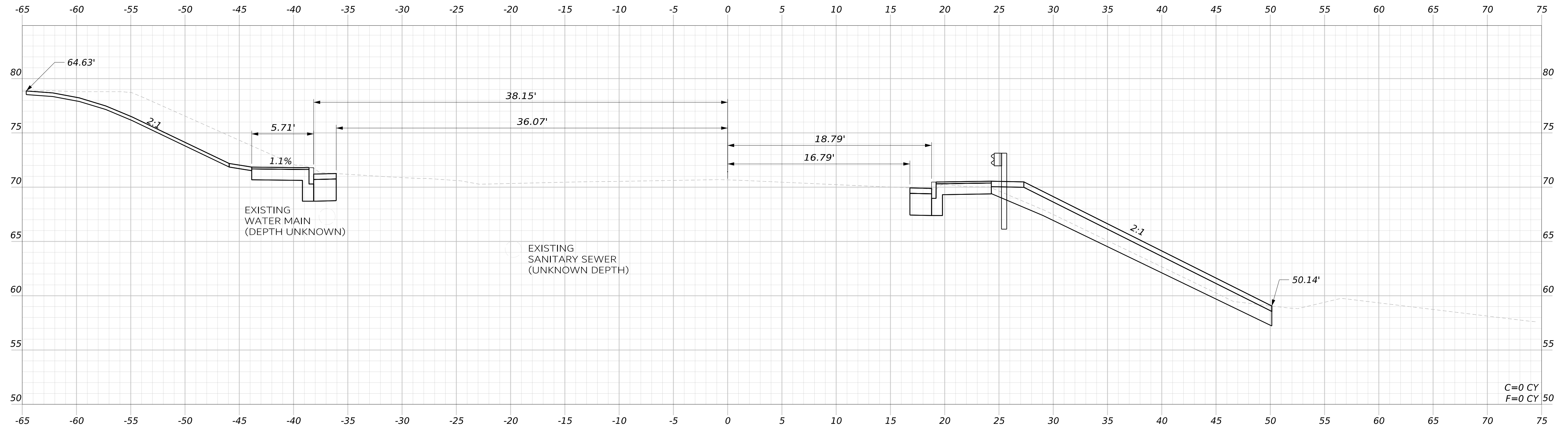
PROJ. MANAGER	BY	DATE
A. CORNEAU II	S. MARTIN	12/2024
DESIGN-DETAILED	K. WOOD	12/2024
CHECKED-REVIEWED	M. MATZKE	
DESIGN-DETAILED 02		
DESIGN-DETAILED 03		
REVISIONS 1		
REVISIONS 2		
REVISIONS 3		
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FIELD CHANGES		

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
20
OF 23

Date: 12/30/2024

Username: SMartin



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
26290000
WIN
026290.00
HIGHWAY PLANS

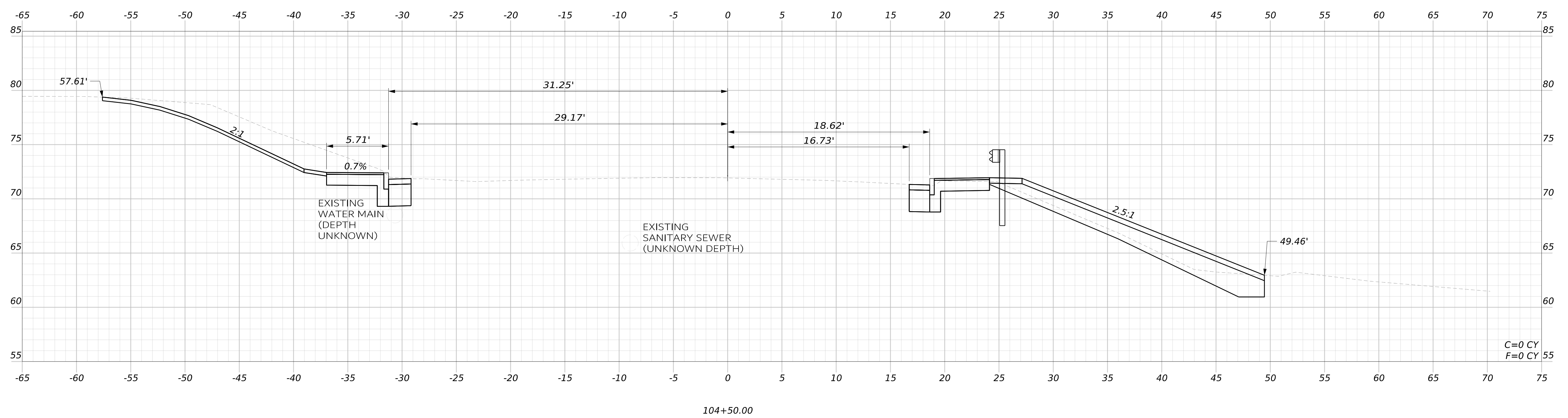
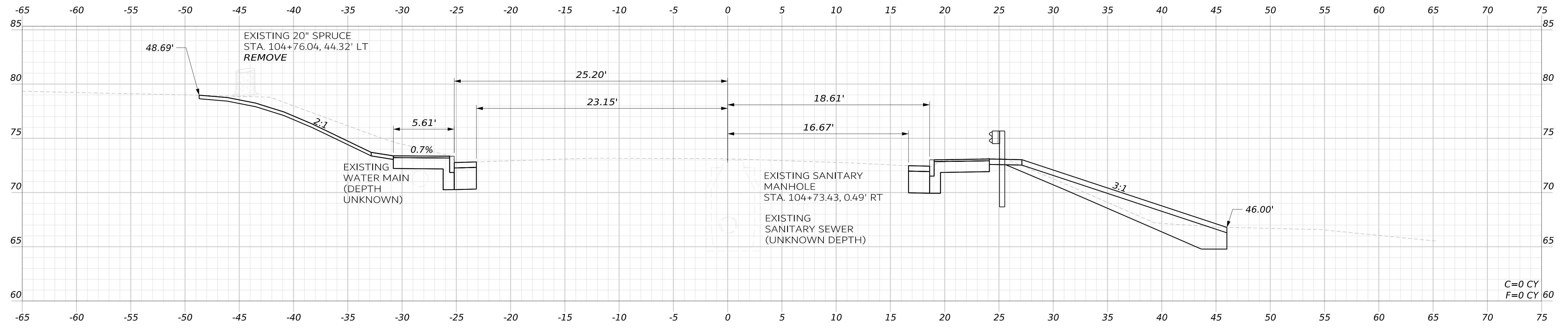
PROJ. MANAGER	A. CORNEAU II	DATE	12/2024
DESIGN-DETAILED	K. WOOD	BY	S. MARTIN
CHECKED-REVIEWED	K. WOOD		M. MATZKE
DESIGN-DETAILED			
DESIGN-DETAILED			
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THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

SHEET NUMBER
21
OF 23

Date: 12/30/2024

Username: SMartin

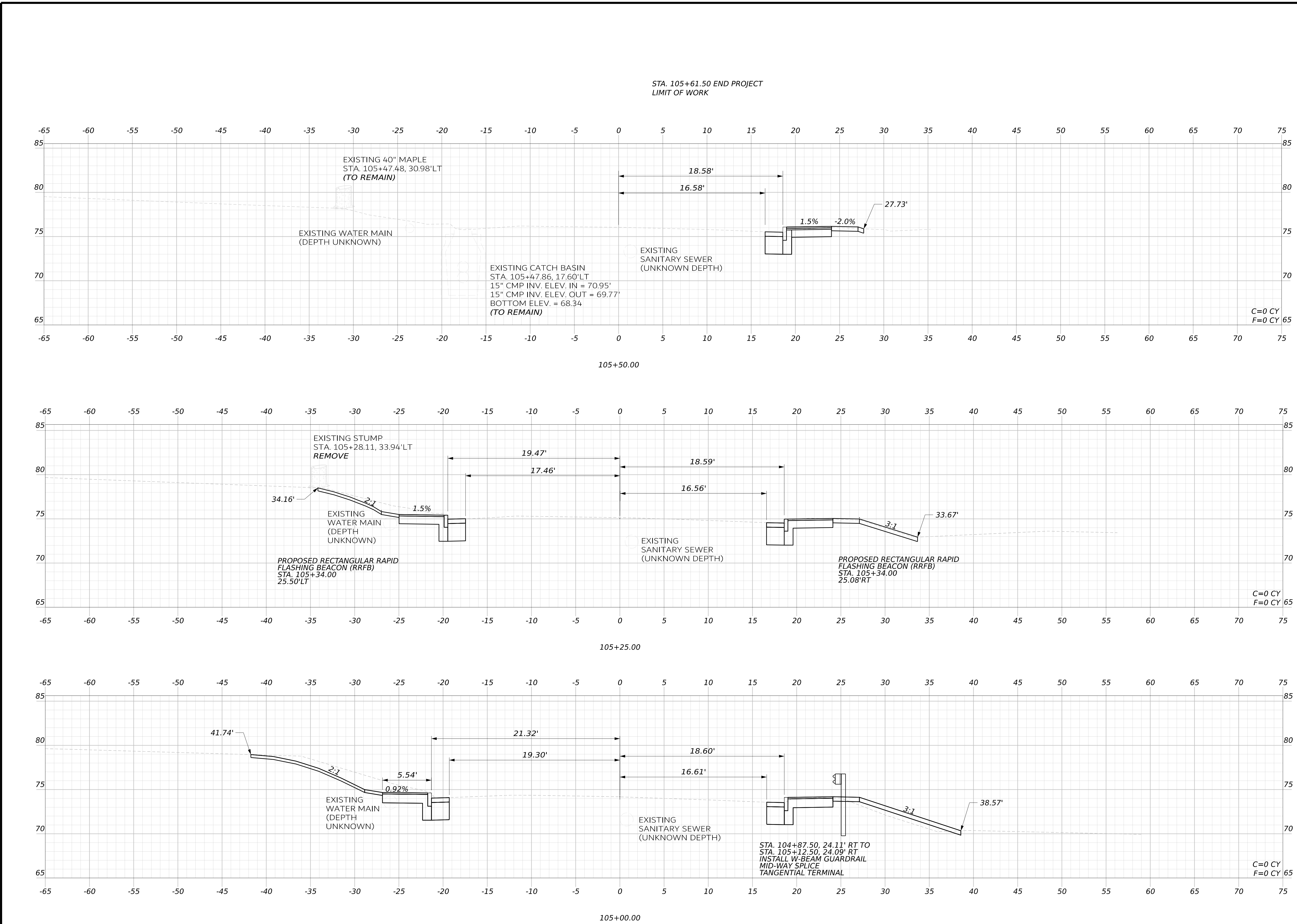


PROJ. MANAGER	BY	DATE	SIGNATURE	P.E. NUMBER	DATE
A. GORNEAU II	S. MARTIN	12/2024			
K. WOOD	M. MATZKE	12/2024			
DESIGN-DETAILED	DESIGNED	DESIGNED	DESIGNED	DESIGNED	DESIGNED
CHECKED-REVIEWED	CHECKED-REVIEWED	CHECKED-REVIEWED	CHECKED-REVIEWED	CHECKED-REVIEWED	CHECKED-REVIEWED
DESIGN-DETAILED	DESIGN-DETAILED	DESIGN-DETAILED	DESIGN-DETAILED	DESIGN-DETAILED	DESIGN-DETAILED
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REVISIONS 2	REVISIONS 2	REVISIONS 2	REVISIONS 2	REVISIONS 2	REVISIONS 2
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FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	FIELD CHANGES

THOMASTON
US ROUTE 1 AND ROUTE 131
CROSS SECTIONS

Date: 12/30/2024

Username: SMartin



STATE OF MAINE DEPARTMENT OF TRANSPORTATION		26290000		WIN 026290.00		HIGHWAY PLANS	
THOMASTON US ROUTE 1 AND ROUTE 131		CROSS SECTIONS		SHEET NUMBER		23	
OF 23		PROJ. MANAGER	A. GORNEAU II	BY	S. MARTIN M. MATZKE	DATE	12/2024 12/2024
SIGNATURE		DESIGN-DETAILED		CHECKED-REVIEWED		DESIGN-DETAILED	
P.E. NUMBER		REVISIONS 1		REVISIONS 2		REVISIONS 3	
DATE		REVISIONS 4		FIELD CHANGES			