

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



GRAY

CUMBERLAND COUNTY

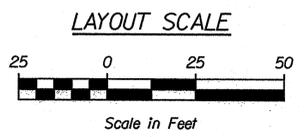
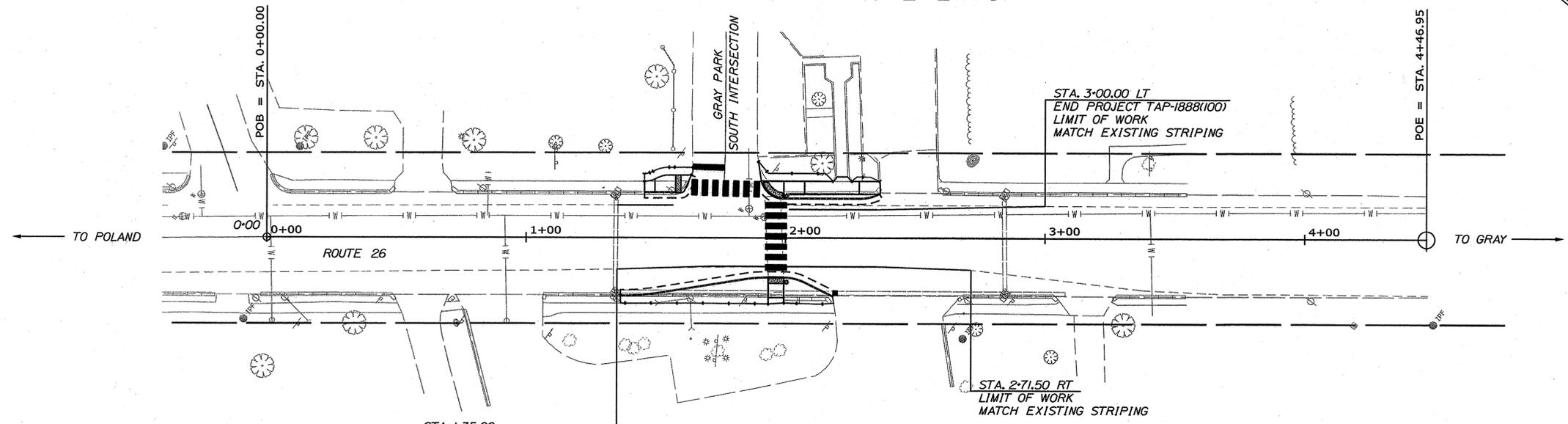
ROUTE 26

TAP-1888(100)

PROJECT LENGTH: 0.03 MILES
(CROSSWALK IMPROVEMENTS)

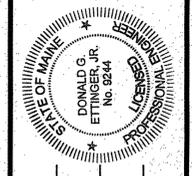
PLAN LEGEND	
Town, County, State	Centerline-Existing
Property Lines	Centerline-Proposed
R/W Lines-Existing	Travelway-Existing
R/W Lines-Proposed	Travelway-Proposed
Culvert-Existing	Railroad
Culvert Proposed	Catch Basins
Curbing Existing	Manholes
Curbing Proposed	Proposed Underdrain
Type 1	Proposed Ditch
Type 3	Existing Ditch
Type 5	Utility Poles
Outline of Bodies of Water	Fire Hydrants
Ledge	Existing Water Line
Buildings	Existing San. Sewer
Trees	Existing Force Main
Tree Line	Existing San. Sewer Manhole
Clearing Limit Line	Guardrail-Existing
	Guardrail-Proposed
	Guardrail-Cable, Other

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PROJECT LOCATION:	Intersection of Route 26 and the Southerly Intersection of Gray Park at Fiddlehead Center for the Arts, in Gray
PROGRAM AREA:	Multimodal Program
SCOPE OF WORK:	Crosswalk and ADA Ramp Improvements

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	DATE 4/13/16
APPROVED	DATE 4/13/16
COMMISSIONER	CHIEF ENGINEER



SIGNATURE	P.E. NUMBER	DATE
[Signature]	3-3116	3-31-16

PROGRAM	MULTIMODAL
PROJECT MANAGER	BRIAN KEEZER
DESIGNER	D. ETTINGER
PROJECT RESIDENT	GORRILL PALMER
CONTRACTOR	
PROJECT COMPLETION DATE	

GRAY ROUTE 26 (SHAKER ROAD)	TITLE SHEET
WIN 018881.00	

SHEET NUMBER	1
	OF 6



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207.657.6910

Date: 3/31/2016

Username: jwinchenboch

Division: HIGHWAY

Filename: ...\\00\Highway\001L_Title.dgn

GENERAL NOTES:

1. ALL UTILITY FACILITIES SHALL BE ADJUSTED BY THE RESPECTIVE UTILITIES UNLESS OTHERWISE NOTED.
2. THE LOCATION OF THE EXISTING UTILITIES AND DRAINAGE SHOWN ON THE PLANS WERE COMPILED FROM FIELD SURVEY. LOCATIONS ARE APPROXIMATE AND NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED.
3. LOCATION OF EXISTING RIGHT OF WAY IS SHOWN ON THE GENERAL PLAN.
4. THE PAVEMENT SAW CUT LINES ARE DEFINED ON THE TYPICAL SECTIONS. CONTRACTOR SHALL NOTE THE PAVEMENT SAW CUT LIMITS ARE PROVIDED FOR RECONSTRUCTION AREAS ONLY AND ARE NOT SHOWN FOR PROPOSED DRAINAGE WORK.
5. ANY NECESSARY FINE GRADING OR RECOMPACTION OF EXISTING GRAVEL SHALL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED INCIDENTAL TO ITEM 304.10.
6. PAVEMENT CUT LINES SHALL BE NEAT, CLEAN AND STRAIGHT AS DIRECTED BY THE RESIDENT. PAYMENT FOR CUTTING OF EXISTING PAVEMENT SHALL BE INCIDENTAL TO 403 ITEMS.
7. ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN APPROVED WASTE AREAS REVIEWED BY THE RESIDENT. GRADING, SEEDING AND MULCHING OF WASTE AREAS SHALL BE CONSIDERED INCIDENTAL.
8. CATCH BASIN AND RIM ELEVATIONS NOTED ON THE PLANS ARE THE TOP OF GRATE ELEVATIONS AT THE CENTER OF GRATE. CATCH BASIN OFFSET LOCATIONS ARE MEASURED TO THE CENTER OF THE GRATE.
9. EXISTING CULVERTS TO REMAIN SHALL BE CLEANED AS DIRECTED BY THE RESIDENT. PAYMENT WILL BE MADE UNDER ITEM 631.32 CULVERT CLEANER (INCLUDING OPERATOR).
10. NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT PRIOR APPROVAL OF THE RESIDENT.
11. REMOVAL OF EXISTING CURBING SHALL BE CONSIDERED INCIDENTAL TO ITEM 203.20 - COMMON EXCAVATION.
12. LOAM HAS BEEN ESTIMATED FOR 100% OF THE DISTURBED SLOPE AREA. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS DIRECTED BY THE RESIDENT.
13. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION'S BEST MANAGEMENT PRACTICES FOR EROSION CONTROL AND SEDIMENT CONTROL, FEBRUARY, 2008.
14. ANY DAMAGE TO THE SLOPES OR PAVEMENT CAUSED BY THE CONTRACTOR'S EQUIPMENT, PERSONNEL, OR OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT. ALL WORK, EQUIPMENT, AND MATERIALS REQUIRED TO MAKE REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE.
15. NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
16. "UNDETERMINED LOCATIONS" SHALL BE DETERMINED BY THE RESIDENT.
17. DO NOT EXCAVATE FOR AGGREGATE SUBBASE COURSE WHERE EXISTING MATERIAL IS SUITABLE AS DETERMINED BY THE RESIDENT.
18. IN AREAS WHERE THE RESIDENT DIRECTS THE CONTRACTOR NOT TO EXCAVATE TO THE SUBGRADE LINE SHOWN ON THE PLANS, PAYMENT FOR REMOVING EXISTING PAVEMENT, GRUBBING, SHAPING, DITCHING, AND COMPACTING THE EXISTING SUBBASE AND LAYERS OF NEW SUBBASE 6 INCHES OR LESS THICK WILL BE MADE UNDER APPROPRIATE EQUIPMENT RENTAL ITEMS.
19. BI-C CATCH BASINS SHALL BE SHAPE WITH HAUNCHED CONE, AS PER STANDARD DETAIL 604(02), UNLESS OTHERWISE NOTED ON THE PLANS.
20. ANY NECESSARY CUTTING OF EXISTING PIPES TO FIT IN AREAS OF PROPOSED CATCH BASINS WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO ITEM 604.
21. ALL SIGNING AND STRIPING MATERIALS AND PLACEMENT SHALL CONFORM TO THE MAINE DOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS, NOVEMBER 2014, AND WITH THE FEDERAL HIGHWAY ADMINISTRATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", MOST CURRENT EDITION.
22. ALL DISTURBED SLOPE AREAS SHALL BE LOAMED AND SEEDED. UNLESS OTHERWISE NOTED, SEEDING METHOD NO. 1 SHALL BE UTILIZED.
23. THE CONTRACTOR SHALL CONTACT DIG-SAFE AND APPROPRIATE AUTHORITIES PRIOR TO ANY SUBSURFACE ACTIVITIES.
24. ANY TREE TRIMMING AND CLEARING SHALL BE CONSIDERED INCIDENTAL. THE ACTUAL LINES FOR CLEARING AND LOCATIONS FOR TREE TRIMMING SHALL BE ESTABLISHED BY THE CONTRACTOR AND APPROVED IN THE FIELD BY THE RESIDENT.
25. DRIVEWAY ACCESSES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES.
26. PROPERTY LINE AND R.O.W. MONUMENTS SHALL NOT BE DISTURBED BY CONSTRUCTION. IF DISTURBED, THEY SHALL BE RESET TO THEIR ORIGINAL LOCATIONS AT THE CONTRACTOR'S EXPENSE, BY A MAINE PROFESSIONAL LAND SURVEYOR.
27. ALL NEW SIGNS SHALL HAVE HIGH INTENSITY REFLECTIVE SHEETING IN ACCORDANCE WITH MAINE DOT SPECIFICATIONS. WHEN WOOD POSTS ARE USED THEY SHALL BE PRESSURE TREATED.
28. THE CONTRACTOR SHALL COMPLETE THE WORK WITHIN THE RIGHT-OF-WAY, AND WILL BE RESPONSIBLE IF THEY TRESPASS ON PRIVATE PROPERTY, UNLESS WORK IS SHOWN OTHERWISE ON THE PLANS.
29. ALL PAVEMENT MARKINGS THAT CONFLICT WITH THE PROPOSED MARKINGS SHALL BE REMOVED IN ACCORDANCE WITH MAINE DOT SPECIFICATIONS.
30. ALL EXISTING SIGNS SHALL BE RELOCATED, AS REQUIRED OR REMOVED AS NOTED. PAYMENT SHALL BE INCIDENTAL TO THE 203.20 PAY ITEM.
31. STATIONS REFERENCED ARE APPROXIMATE.
32. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF ALL TREES AND SHRUBS ON THE PROJECT WHICH ARE NOT TO BE REMOVED. STORAGE OF SUPPLIES AND EQUIPMENT ARE NOT ALLOWED UNDER THE DRIP-LINE OF TREES TO REMAIN.
33. CONTRACTOR SHALL NOT PARK, IMPEDE ACCESS OR STORE EQUIPMENT/MATERIAL ON ADJACENT TOWN OR PRIVATELY OWNED LAND WITHOUT WRITTEN CONSENT FROM THE TOWN OR LAND OWNER.
34. ALL WORK CONTEMPLATED UNDER THIS CONTRACT SHALL BE GOVERNED BY AND BE IN CONFORMITY WITH THE MAINE DOT STANDARD SPECIFICATIONS (REVISION OF NOVEMBER 2014) AND THE STANDARD DETAILS (REVISION OF NOVEMBER 2014) EXCEPT PROVISIONS AND ANY AND ALL CORRECTIONS, REVISIONS OR ADDITIONS ISSUED BY MAINE DOT.
35. CROSSWALK PAVEMENT MARKINGS SHALL BE TAPE IN GROOVED PAVEMENT. ALL OTHER PAVEMENT MARKINGS SHALL BE PAINT.
36. FINAL STRIPING FOR THE PROJECT SHALL BE DONE BY THE CONTRACTOR PER THE STRIPING PLAN IN THE CONTRACT DOCUMENTS. PAYMENT SHALL BE MADE UNDER APPROPRIATE CONTRACT ITEMS.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
TAP-1888(100)
WIN 018881.00

PROJ. MANAGER	CATHERINE RAND	DATE	03/16
DESIGN-DETAILED	JSW	BY	JSW
CHECKED-REVIEWED	DGE	DATE	03/16
DESIGN-DETAILED		SIGNATURE	
REVISIONS 1		P.E. NUMBER	
REVISIONS 2		DATE	
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

GRAY
ROUTE 26 (SHAKER ROAD)
GENERAL NOTES

SHEET NUMBER
2
OF 6

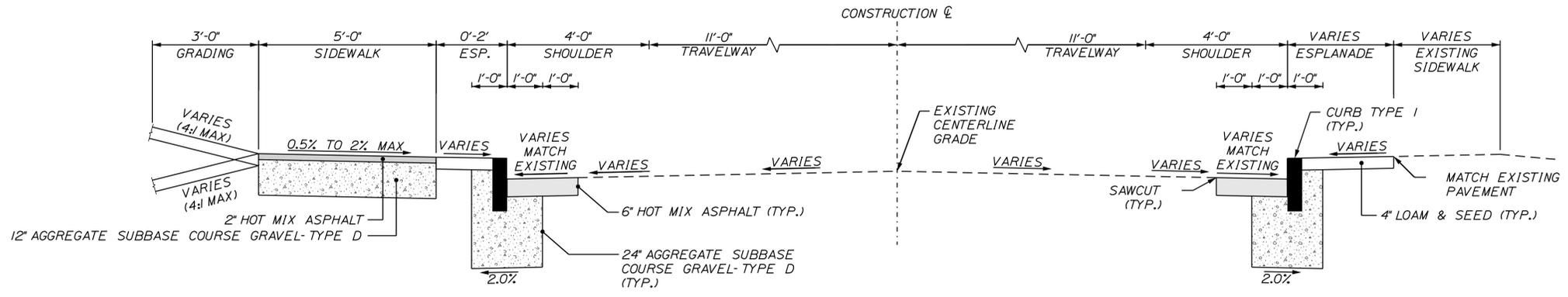


Date: 3/31/2016

Username: jwinchenbach

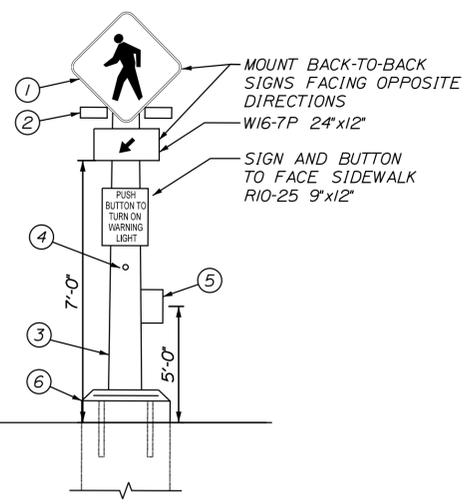
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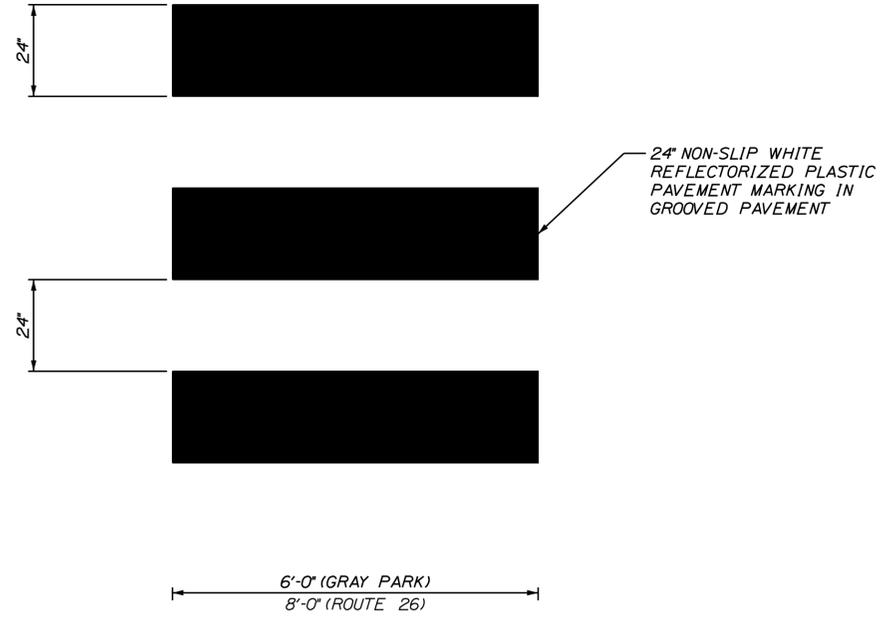


**TYPICAL SECTION
CURB & SIDEWALK WORK**
STA. 1+36 TO STA. 2+18 RT
STA. 1+46 TO STA. 2+36 LT

- CONSTRUCTION NOTES:**
1. 30"x30" SIGN W11-2
 2. 12" YELLOW RECTANGULAR RAPID FLASHING BEACONS (BLACK HOUSING) SET TO HIGH VISIBILITY STROBE PATTERN - MOUNTED IN BOTH DIRECTIONS
 3. MAINEDOT PEDESTAL POLE
 4. PEDESTRIAN PUSH BUTTON (NON AUDIBLE)
 5. POLE MOUNTED NETWORK CONTROLLER
 6. 18" CONCRETE FOUNDATION PER MAINEDOT STANDARDS
- GENERAL NOTES:**
1. ALL LIGHTS AND PEDESTRIAN PUSH BUTTONS SHALL BE JSF TECHNOLOGIES OR APPROVED EQUAL
 2. POLE SHALL BE PAINTED MATTE BLACK
 3. LIGHTS SHALL FLASH FOR 30 SECONDS (AS NOTED)
 4. SIGNS W11-2 AND W16-7P SHALL BE FLUORESCENT YELLOW-GREEN BACKGROUND COLOR.
 5. SEE SPECIAL PROVISION 643.



**RECTANGULAR RAPID FLASHING BEACON ASSEMBLY
(SOLAR POWERED, RADIO ACTIVATED)**
STA. 2+00.33, 17.00' RT
STA. 2+00.33, 16.25' LT



CROSSWALK STRIPING

- NOTES:**
1. THE PAVEMENT, BASE AND SUBBASE DEPTHS AS SHOWN ON THE PLANS ARE INTENDED TO BE NOMINAL.
 2. CROWNS FOR BOTH NORMAL AND SUPERELEVATION SECTIONS FOR ALL COURSES OF SUBBASE AND PAVEMENT SHALL BE STRAIGHT.
 3. THE ALGEBRAIC DIFFERENCE BETWEEN THE SHOULDER AND TRAVELWAY CROSS SLOPES "ROLLOVER" SHALL NOT EXCEED 8%.
 4. THE STATIONING SHOWN UNDER EACH TYPICAL IS APPROXIMATE.
 5. DO NOT EXCAVATE FOR AGGREGATE SUBBASE COURSE GRAVEL WHERE EXISTING MATERIAL IS SUITABLE AS DETERMINED BY THE RESIDENT.
 6. FULL DEPTH PAVEMENT (6") SHALL BE PROVIDED IN SHOULDERS WITHIN LIMITS OF WORK.

PROJ. MANAGER	CATHERINE RAND	BY	DATE
DESIGN-DETAILED	JSW	JSW	03/16
CHECKED-REVIEWED	DGE		03/16
DESIGN-DETAILED			
REVISIONS	1		
REVISIONS	2		
REVISIONS	3		
REVISIONS	4		
FIELD CHANGES			

SHEET NUMBER

3

OF 6

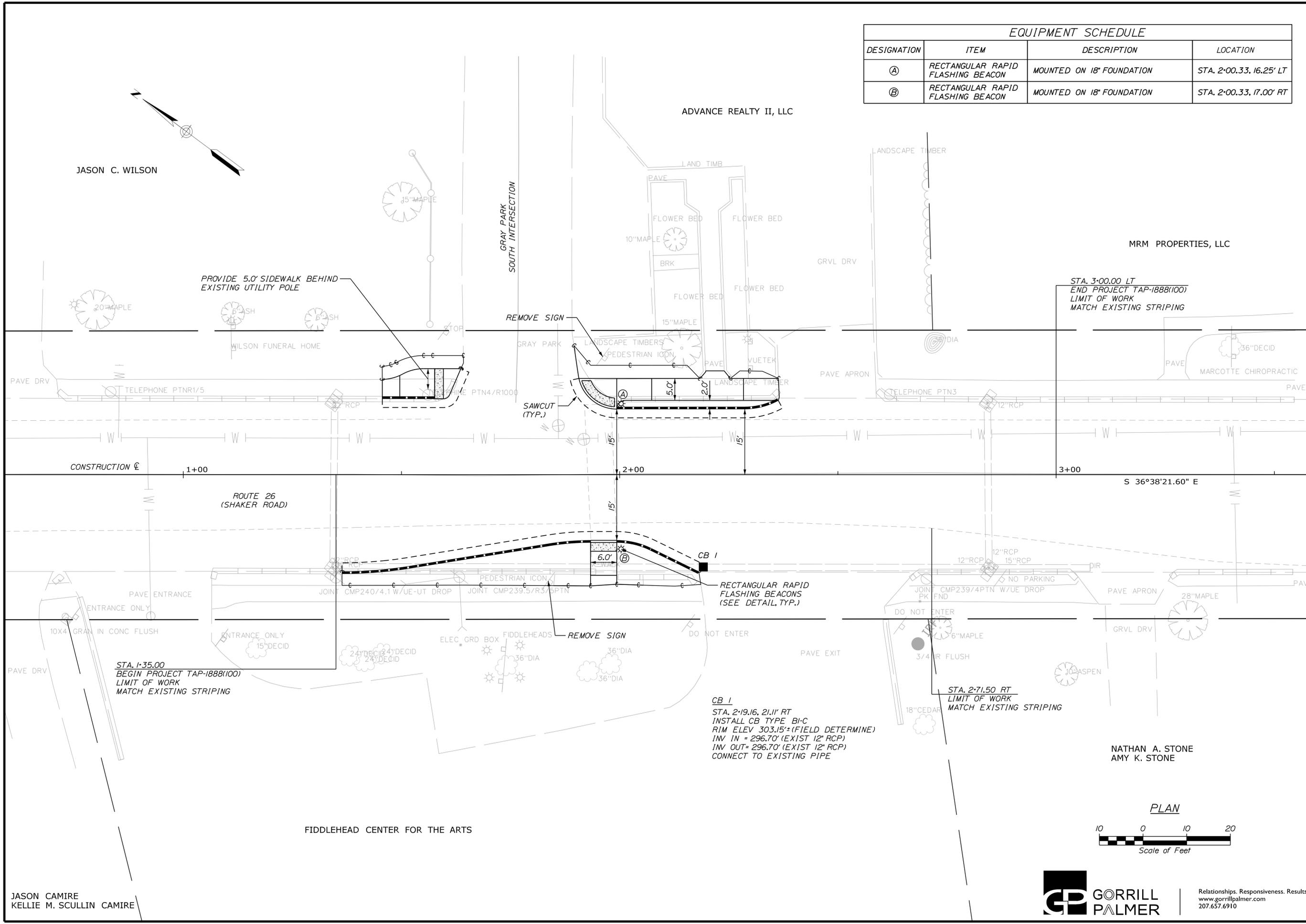
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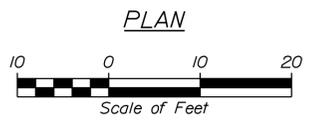
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 Division: HIGHWAY
 Username: jwinchenbach
 Date: 3/31/2016

EQUIPMENT SCHEDULE			
DESIGNATION	ITEM	DESCRIPTION	LOCATION
(A)	RECTANGULAR RAPID FLASHING BEACON	MOUNTED ON 18" FOUNDATION	STA. 2+00.33, 16.25' LT
(B)	RECTANGULAR RAPID FLASHING BEACON	MOUNTED ON 18" FOUNDATION	STA. 2+00.33, 17.00' RT



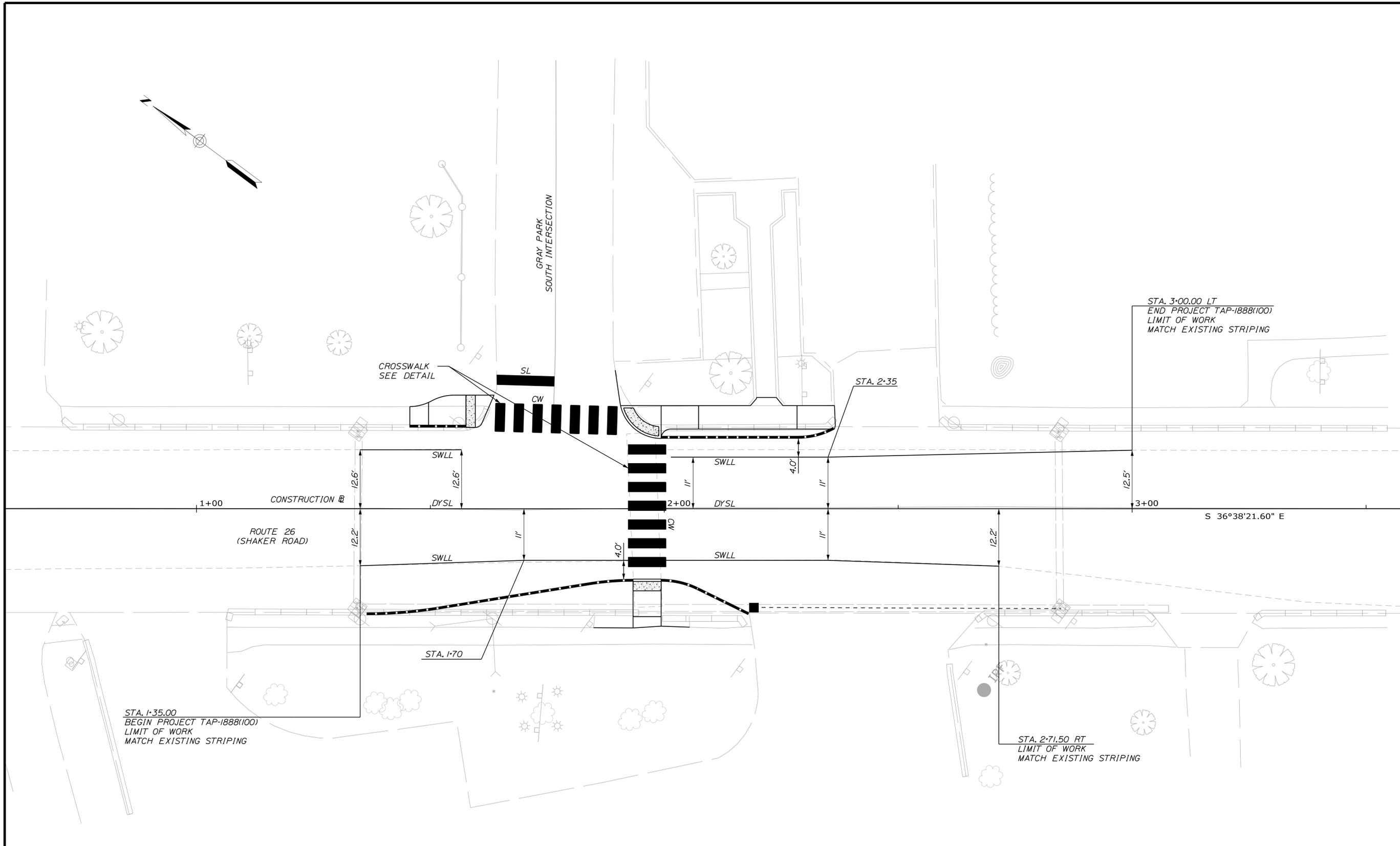
STATE OF MAINE DEPARTMENT OF TRANSPORTATION		TAP-1888(100)		WIN 018881.00	
PROJ. MANAGER	D. LETTINGER	BY	JSW	DATE	03/16
DESIGN DETAILED	JSW	CHECKED/REVIEWED	DGE	DATE	03/16
DESIGN DETAILED		DESIGN DETAILED		SIGNATURE	
REVISIONS 1		REVISIONS 1		P.E. NUMBER	
REVISIONS 2		REVISIONS 2		DATE	
REVISIONS 3		REVISIONS 3			
REVISIONS 4		REVISIONS 4			
FIELD CHANGES					
GRAY ROUTE 26 (SHAKER ROAD) GENERAL PLAN					
SHEET NUMBER					
4					
OF 6					



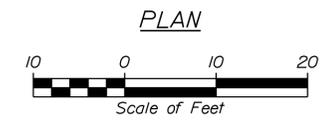
JASON CAMIRE
 KELLIE M. SCULLIN CAMIRE



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LEGEND	
DYSL	DOUBLE SOLID YELLOW LINE
SWLL	SOLID WHITE LANE LINE
CW	CROSSWALK STRIPE (SEE DETAILS)
SL	WHITE STOP LINE (24" WIDE)



PROJ. MANAGER	D. ETINGER	BY	DATE
DESIGN/DETAILED	JSW	03/16	SIGNATURE
CHECKED/REVIEWED	DGE	03/16	P.E. NUMBER
DESIGN/DETAILED			DATE
REVISIONS 1			
REVISIONS 2			
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

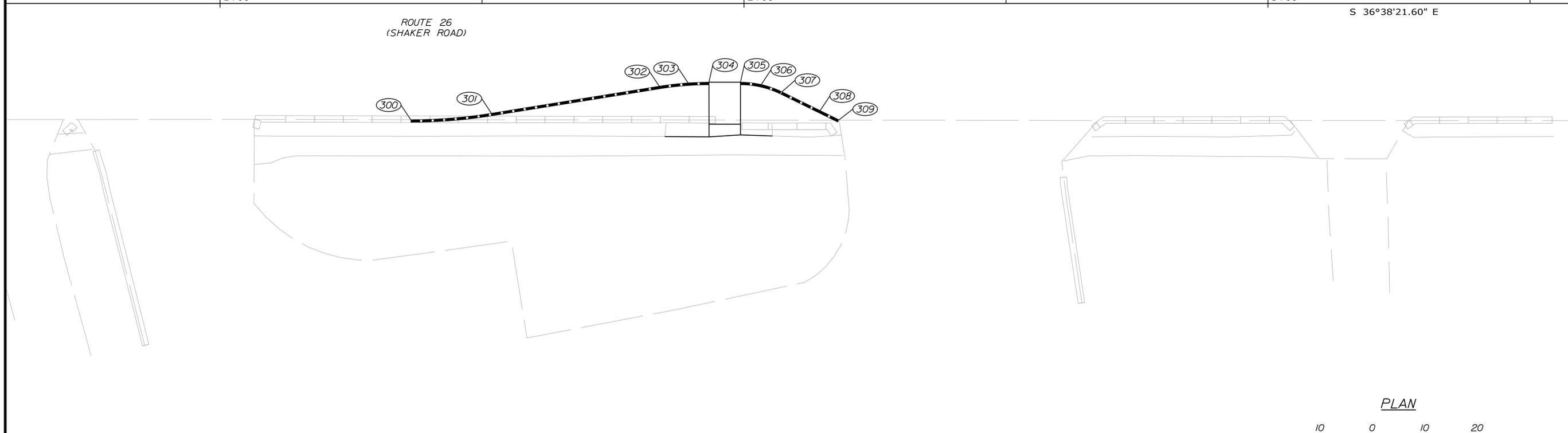
GRAY (SHAKER ROAD)
 STRIPING PLAN

Date: 3/31/2016

Username: jwinchenbach

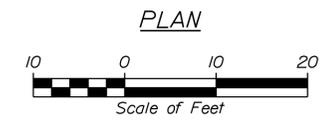
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CURB TABLE						
PT. NO.	STATION/OFFSET FROM	PT. NO.	STATION/OFFSET TO	LENGTH (FT)	RADIUS (FT)	CURB TYPE
100	1+45.61, 17.35' LT	101	1+49.61, 17.35' LT	4	-	I
101	1+49.61, 17.35' LT	102	1+57.61, 17.35' LT	8	-	TD
200	1+99.33, 15.00' LT	201	2+07.33, 15.00' LT	8	-	TD
201	2+07.33, 15.00' LT	202	2+28.67, 15.00' LT	21.34	-	I
202	2+28.67, 15.00' LT	203	2+36.34, 16.96' LT	8	16	TD
300	1+36.38, 22.13' RT	301	1+51.78, 20.91' RT	15.47	96	I
301	1+51.78, 20.91' RT	302	1+83.82, 15.76' RT	32.45	-	I
302	1+83.82, 15.76' RT	303	1+89.32, 15.14' RT	5.53	60	I
303	1+89.32, 15.14' RT	304	1+93.32, 15.00' RT	4	60	TD
305	1+99.33, 15.00' RT	306	2+03.30, 15.44' RT	4	18	TD
306	2+03.30, 15.44' RT	307	2+07.24, 16.83' RT	4.19	18	I
307	2+07.24, 16.83' RT	308	2+14.54, 20.40' RT	8.13	-	I
308	2+14.54, 20.40' RT	309	2+18.13, 22.16' RT	4	-	TD

TD = TIP DOWN GRANITE CURB
 I = VERTICAL GRANITE CURB



STATE OF MAINE
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 TAP-1888(100)
 WIN 018881.00

PROJ. MGR.	D. ETINGER	BY	DATE	SIGNATURE	P.E. NUMBER	DATE
DESIGN-DETAILED	JSW	03/16				
CHECKED-REVIEWED	DGE	03/16				
DESIGN-DETAILED						
REVISIONS 1						
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FIELD CHANGES						

GRAY
 ROUTE 26 (SHAKER ROAD)
 CURBING PLAN

SHEET NUMBER
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