

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



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Construction Safety and Phasing Plans	2-6
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2015 Statewide Airport Crackseal Project

Various Airports:

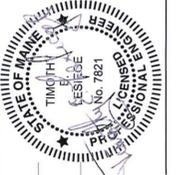
Lincoln Regional, Millinocket Municipal,
Newton Field (Jackman), Pittsfield Municipal,
& Robert LaFleur - Waterville

FAA AIP
3-23-2300-07-2015

SCOPE OF WORK includes:
Crack cleaning, Grinding,
Crackseal application
Paint Removal & Paint Application



STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION
 APPROVED: *[Signature]*
 COMMISSIONER: *[Signature]*
 DATE: 4-14-15
 CHIEF ENGINEER: *[Signature]*



Timothy E. LeSiege
 SIGNATURE
 PE # 7821
 P.E. NUMBER
 March 6, 2015
 DATE

PROGRAM	AVIATION
PROJECT MANAGER	TIMOTHY E. LESIEGE
DESIGNER	TIMOTHY E. LESIEGE
CONSULTANT	FAY, SPOFFORD & THORNDIKE
PROJECT RESIDENT	FAY, SPOFFORD & THORNDIKE
CONTRACTOR	FAY, SPOFFORD & THORNDIKE
PROJECT COMPLETION DATE	

WIN 018717.00

TITLE SHEET

SHEET NUMBER
 1 OF 7

GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER AND RESIDENT ENGINEER.

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THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLAN FOR THEIR STAGING AREA. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL PROTECTION AND RESTORATION OF THE DESIGNATED AREA. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRASH PICK-UP GENERATED BY THE PROJECT.

THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGHWAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

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RESTRICTED AIRPORT OPERATIONS WILL BE CONDUCTED DURING CONSTRUCTION. THE CONTRACTORS WORK SHALL BE CARRIED ON IN SUCH A MANNER AS NOT TO INTERFERE WITH THE AIRPORT OPERATIONS. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO INSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS THEIR OWN EQUIPMENT AND PERSONNEL. THE CONTRACTORS SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

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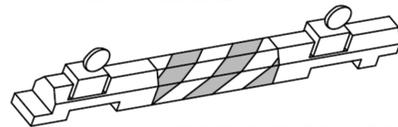
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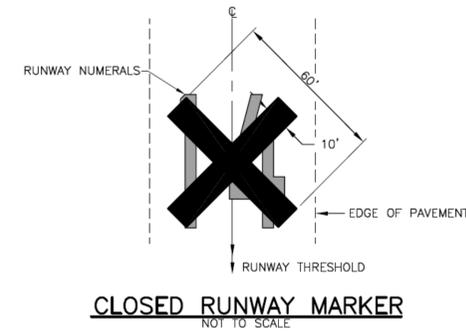
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LOW-PROFILE BARRICADE
NOT TO SCALE

BARRICADE NOTES

- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
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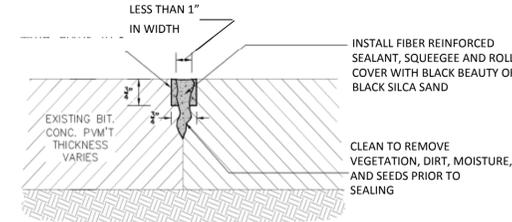
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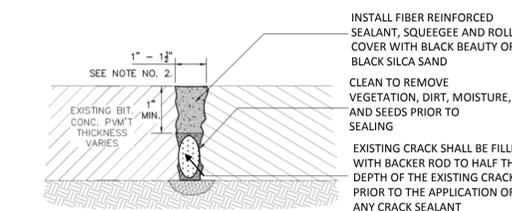
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- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.



- NOTE:
- REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.

PAVEMENT CRACK SEALING – TYPE I DETAIL
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CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
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LEGEND

- ~CLOSED RUNWAY MARKER
- ~HAUL ROUTE
- ~PHASE 1 BARRICADE
- ~PHASE 2 BARRICADE
- ~STAGING AREA

PHASES 1 & 2

- PHASE 1— CLOSE RUNWAY 13-31 BARRICADE AS NOTED—CRACKSEAL RUNWAY 13-31 AND TAXIWAY— RUNWAY 13-31 MUST BE NOTAMED CLOSED
- PHASE 2 — PLACE PHASE 2 BARRICADES AS NOTED— STAY ON OLDER PAVEMENT ON TAXILANES NEAR HANGARS AND FINISH TERMINAL APRON AREA LAST

PAINT REMOVAL AND AIRFIELD MARKING TO FOLLOW THE SAME PHASES



STATE OF MAINE DEPARTMENT OF TRANSPORTATION		018717.00 WIN	
PROJECT MANAGER	TIMOTHY E. LESIEGE	DATE	APRIL 14, 2015
DESIGN-DETAILED	2/09/15	BY	
CHECKED-REVIEWED	4/13/15	T.E.L.	
DESIGN-DETAILED	DA-FST	T.E.L.	
DESIGN-DETAILED	DA-FST	T.E.L.	
REVISIONS 1		P.E. NUMBER	7821
REVISIONS 2		DATE	APRIL 14, 2015
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

2015 STATEWIDE AIRPORT CRACKSEAL PROJECT
NEWTON FIELD

CONSTRUCTION SAFETY AND PHASING PLAN

SHEET NUMBER
2 OF 7

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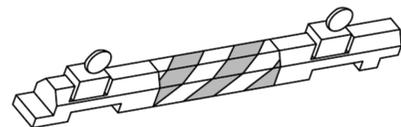
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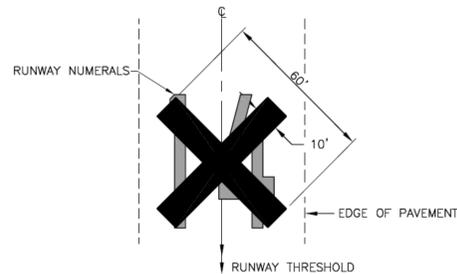
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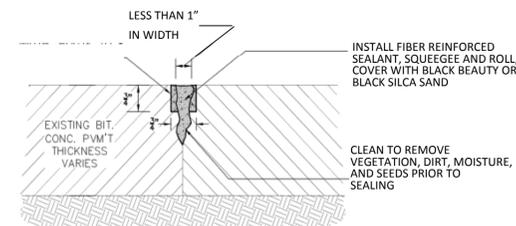
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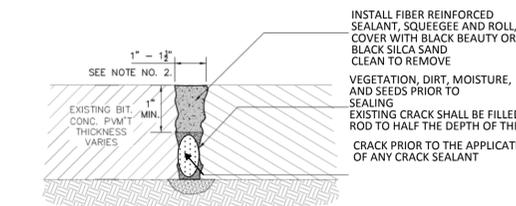
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- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXILANES AND AS DIRECTED BY THE ENGINEER AND/OR AIRPORT MANAGER. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH 2 FOOT HIGH (MAX) STOP SIGNS ON SITE FOR THIS PURPOSE.



NOTE:

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PAVEMENT CRACK SEALING – TYPE I DETAIL
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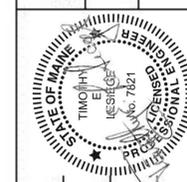
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STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
018717.00
WIN



TIMOTHY E. LESIEGE
SIGNATURE
PE # 7821
P.R. NUMBER
APRIL 14, 2015
DATE

PROJ. MANAGER	DATE	BY	T.E.L.	DATE	BY	T.E.L.
DESIGN-DETAILED	2/12/15	T.E.L.	DA-FST	4/13/15	T.E.L.	DA-FST
CHECKED-REVIEWED	4/13/15	T.E.L.	DA-FST	4/13/15	T.E.L.	DA-FST
DESIGN-DETAILED						
REVISIONS 1						
REVISIONS 2						
REVISIONS 3						
REVISIONS 4						
FIELD CHANGES						

2015 STATEWIDE AIRPORT CRACKSEAL PROJECT
LINCOLN REGIONAL AIRPORT
CONSTRUCTION SAFETY AND PHASING
PLAN

SHEET NUMBER

3 OF 7

LEGEND

- X ~CLOSED RUNWAY MARKER
- ~HAUL ROUTE
- ~ PHASE 1 BARRICADE
- ~ PHASE 2 BARRICADE
- ~STAGING AREA

PHASES 1 & 2

- PHASE 1— CLOSE RUNWAY 17-35 BARRICADE AS NOTED—CRACKSEAL R-17-35 AND NORTHERN TAXIWAY AND APRON— RUNWAY 17-35 MUST BE NOTAMED CLOSED **
- PHASE 2 — REMOVE NORTHERN SET OF PHASE 1 BARRICADES, OPEN RUNWAY 17-35, PLACE PHASE 2 BARRICADES AS NOTED— CRACKSEAL SOUTHERN APRON AND TAXILANES

PAINT REMOVAL AND AIRFIELD MARKING TO FOLLOW THE SAME PHASES

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THE CONTRACTOR SHALL HAVE A POWER SWEEPER ON THE SITE WHEN REQUIRED TO SWEEP PAVEMENTS IN WORK AREAS AND HAUL ROUTES AS DIRECTED BY THE AIRPORT MANAGER OR THE RESIDENT ENGINEER .

5. NOTICES TO AIRMEN (NOTAMS)

NOTAMS SHALL BE ISSUED AS NEEDED BY THE AIRPORT MANAGER AFTER COORDINATING WITH THE CONTRACTOR. NOTAMS SHALL BE COORDINATED A MINIMUM OF 48 HOURS IN ADVANCE UNLESS OTHERWISE SPECIFIED.

6. AIRPORT RADIOS

THE CONTRACTORS SHALL PROVIDE A MINIMUM OF TWO AVIATION UNICOM RADIOS FOR USE DURING THE PROJECT. EACH RADIO SHALL BE CAPABLE OF TRANSMITTING ON FREQUENCY 122.85 MHZ AND BE EQUIPPED WITH A RECHARGING UNIT AND SPARE BATTERY PACK. RADIOS SHALL BE ICOM BRAND, MODEL IC-A23 OR APPROVED EQUAL. THE CONTRACTOR SHALL CONTINUOUSLY MONITOR LOCAL AIR TRAFFIC AT ALL TIMES WHEN THEIR FORCES ARE LOCATED WITHIN ANY AOA. RADIO COSTS ARE CONSIDERED INCIDENTAL TO THE PROJECT.

7. RESTROOMS

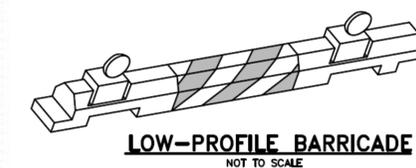
RESTROOM FACILITIES SHALL BE PROVIDED AND MAINTAINED IN THE STAGING AREA BY THE CONTRACTOR. RESTROOM FACILITY COSTS ARE CONSIDERED INCIDENTAL TO THE PROJECT. THE STAGING AREA WILL BE RESTORED BY THE CONTRACTOR, TO ORIGINAL CONDITION AS APPROVED BY THE OWNER.

8. CONTRACTOR VEHICLES

ALL CONTRACTORS VEHICLES SHALL HAVE THE COMPANY IDENTIFICATION PLAINLY VISIBLE ON BOTH SIDES OF THE VEHICLE IN ORDER TO IDENTIFY THE VEHICLE.

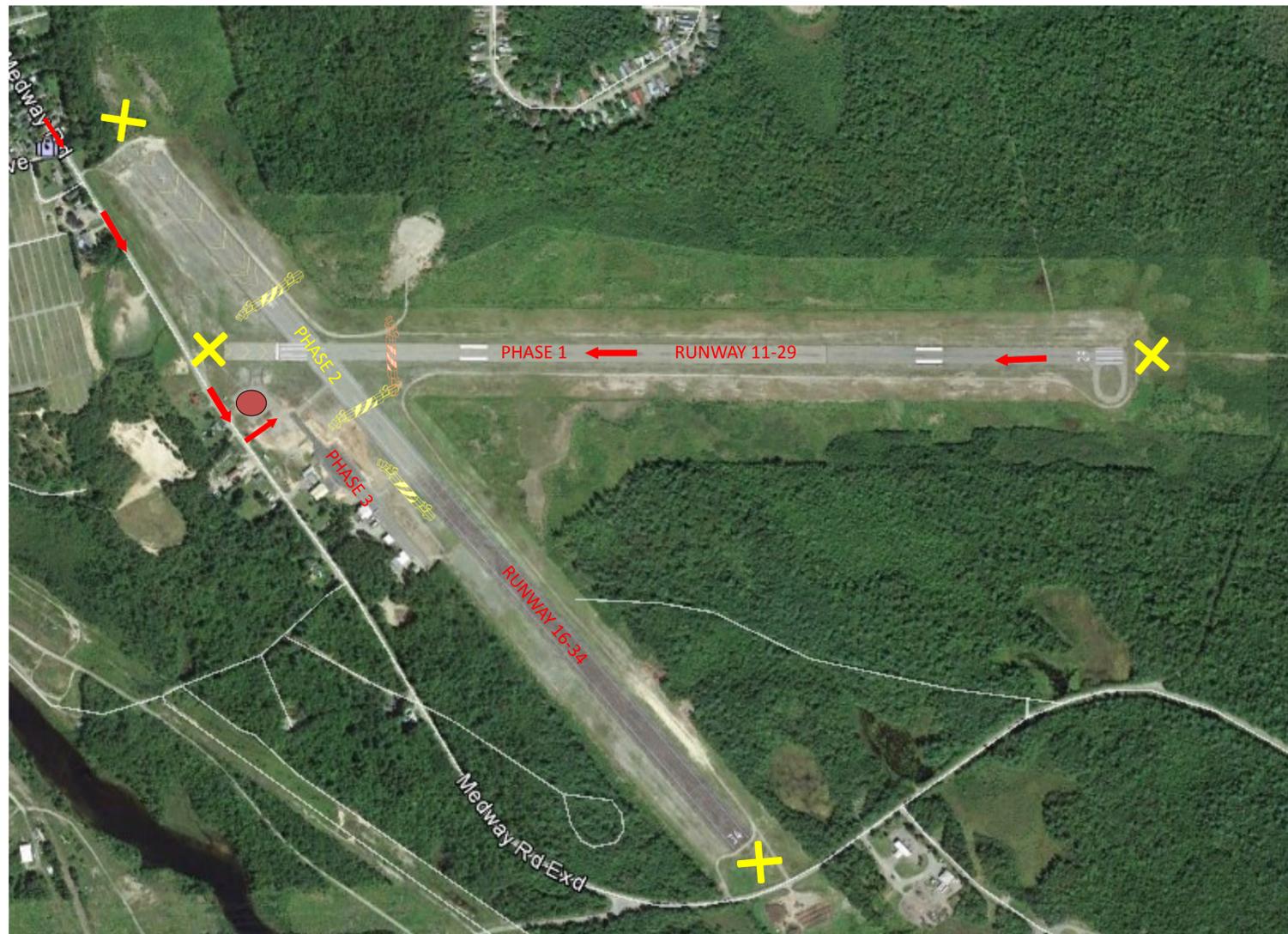
ALL VEHICLES THAT OPERATE DURING THE DAYLIGHT IN THE AIRCRAFT OPERATIONS AREA MUST BE PROVIDED WITH A FLASHING YELLOW BEACON OR FLAG ON A STAFF ATTACHED TO THE VEHICLE SO THAT THE FLAG CAN BE READILY VISIBLE. THIS INCLUDES PICKUP TRUCKS, BACKHOES, FRONT END LOADERS, SKID STEERS, PAVERS, GRADERS, ETC.

THE FLAG SHOULD BE AT LEAST A 3-FOOT SQUARE HAVING A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. THE STAFF MAY BE A 48" WOODEN DOWEL.



BARRICADE NOTES

- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
- BARRICADES CAN BE FILLED WITH WATER OR SAND.
- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 5.5"-10"(H) X 6"-10"(W) X 96"(L)
- MAX BARRICADE SPACING 4' (END TO END)
- LOW PROFILE BARRICADES ARE TO BE PROVIDED AND PLACED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE BARRICADES WILL BE MADE. THE BARRICADES WILL REMAIN THE PROPERTY OF THE CONTRACTOR UPON COMPLETION OF THE WORK OF THIS PROJECT AND ARE TO BE AVAILABLE FOR USE AT THE OTHER AIRPORTS ASSOCIATED WITH THIS CONTRACT.

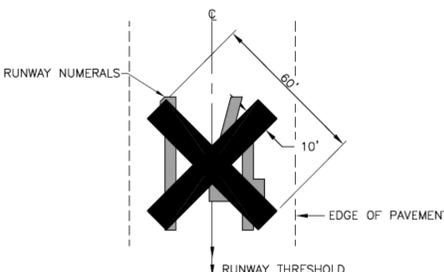


PHASES 1,2 & 3

- PHASE 1 — CLOSE RUNWAY 11-29—BARRICADE AS NOTED—START ON RUNWAY 29 END WORK WESTERLY TO AOA OF RUNWAY 16-34 ~ RUNWAY 11-29 MUST BE NOTAM CLOSED **
- PHASE 2 — CLOSE RUNWAY 16-34 AND KEEP RUNWAY 11-29 CLOSED — LEAVE PHASE 1 BARRICADES AS NOTED ADD PHASE 2 BARRICADES—COMPLETE CRACKSEALING BETWEEN R-16-34 AOA, R-11-29 AOA AND WEST END OF RUNWAY 11-29 ~ BOTH RUNWAYS MUST BE NOTAMED CLOSED **
- PHASE 3 — REMOVE PHASE 1 & PHASE 2 BARRICADES —REOPEN BOTH RUNWAYS—SET BARRICADES PER AIRPORT MANAGER AND CRACK SEAL APRON

** ESCORT MANDATORY TO SET BARRICADES AND CLOSED RUNWAY MARKER EAST OF RUNWAY 16-34 AND ANY TIME CROSSING ACTIVE RUNWAYS

PAINT REMOVAL AND AIRFIELD MARKING TO FOLLOW THE SAME PHASES



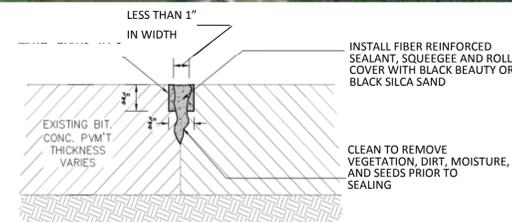
CLOSED RUNWAY MARKER
NOT TO SCALE

CLOSED RUNWAY MARKER NOTES

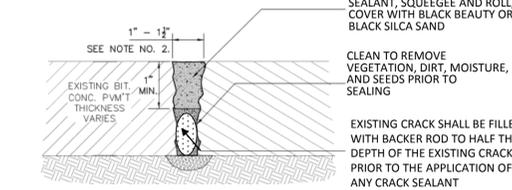
- CLOSED RUNWAY MARKERS (X's) TO BE IN PLACE DURING CONSTRUCTION PERIOD AND/OR AS DIRECTED BY THE ENGINEER OR AIRPORT MANAGER. MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERAL OR ALONG THE EXTENDED RUNWAY CENTERLINE PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.
- MARKERS SHALL BE HIGH STRENGTH, YELLOW VINYL, REUSABLE CLOSURE MARKERS WITH FACTORY INSTALLED BALLAST TUBES. MARKERS SHALL BE FAA APPROVED.
- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE CONTRACTOR, PLACED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.

HAUL ROAD NOTES

- THE CONTRACTOR SHALL ONLY BE PERMITTED TO WORK IN THE DESIGNATED WORK AREAS SHOWN ON THIS SHEET AND ON THE DESIGNATED HAUL ROADS AND STAGING AREAS AS SHOWN ON THIS SHEET. VEHICLES SHALL TRAVEL TO AND FROM WORK AREAS ON ESTABLISHED ROADS AND PATHS AND SHALL NOT TRAVEL IN GRASSLANDS, WETLANDS, OR SENSITIVE RESOURCE AREAS.
- THE CONTRACTOR WILL BE PROHIBITED FROM CROSSING RUNWAYS, TAXIWAYS, OR APRONS WITH CONSTRUCTION EQUIPMENT UNLESS PRIOR APPROVAL FROM THE AIRPORT IS RECEIVED. AT APPROVED CROSSING, THE CONTRACTOR WILL BE RESPONSIBLE TO PROTECT THE EXISTING PAVEMENT FROM DAMAGE. ALL DAMAGED PAVEMENT AREAS WILL BE REQUIRED TO BE REPAIRED TO FAA STANDARDS, AND TO THE SATISFACTION OF THE OWNER, AT THE EXPENSE OF THE CONTRACTOR. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM GENERATING THE OPERATION.
- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL AOA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.



PAVEMENT CRACK SEALING — TYPE I DETAIL
N.T.S.

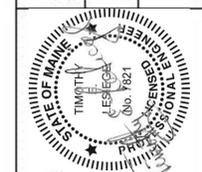


PAVEMENT CRACK SEALING — TYPE II DETAIL
N.T.S.

- CRACKSEAL NOTE**
- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
 - INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

- ~CLOSED RUNWAY MARKER
- ~ HAUL ROUTE
- ~ PHASE 1 BARRICADES
- ~ PHASE 2 & 3 BARRICADES
- ~ STAGING AREA



TIMOTHY E. LESIEGE
REGISTERED PROFESSIONAL ENGINEER
PE # 7821
APRIL 14, 2015
DATE

PROJ. NUMBER	DATE	BY	T.E.L.
DESIGN-DETAILED	2/12/15	T.E.L.	
CHECKED-REVISED	4/13/15	DA-FST	
DESIGN-DETAILED	9/14/15	T.E.L.	
REVISIONS 1			
REVISIONS 2			
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER AND RESIDENT ENGINEER.

3. CONTRACTORS STAGING AREAS AND HAUL ROUTE

THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLAN FOR THEIR STAGING AREA. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL PROTECTION AND RESTORATION OF THE DESIGNATED AREA. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRASH PICK-UP GENERATED BY THE PROJECT.

THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGHWAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

HAUL ROUTES SHOWN ON THE PLANS HAVE BEEN APPROVED BY THE AIRPORT MANAGER. ANY DEVIATION MUST BE APPROVED BY AIRPORT MANAGER OR RESIDENT ENGINEER PRIOR TO UTILIZATION BY THE CONTRACTOR. IT SHOULD BE NOTED THAT DEVIATIONS IN THE HAUL ROUTE MAY NOT BE APPROVED.

4. AIRPORT OPERATIONS AND SAFETY REQUIREMENTS

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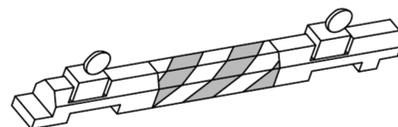
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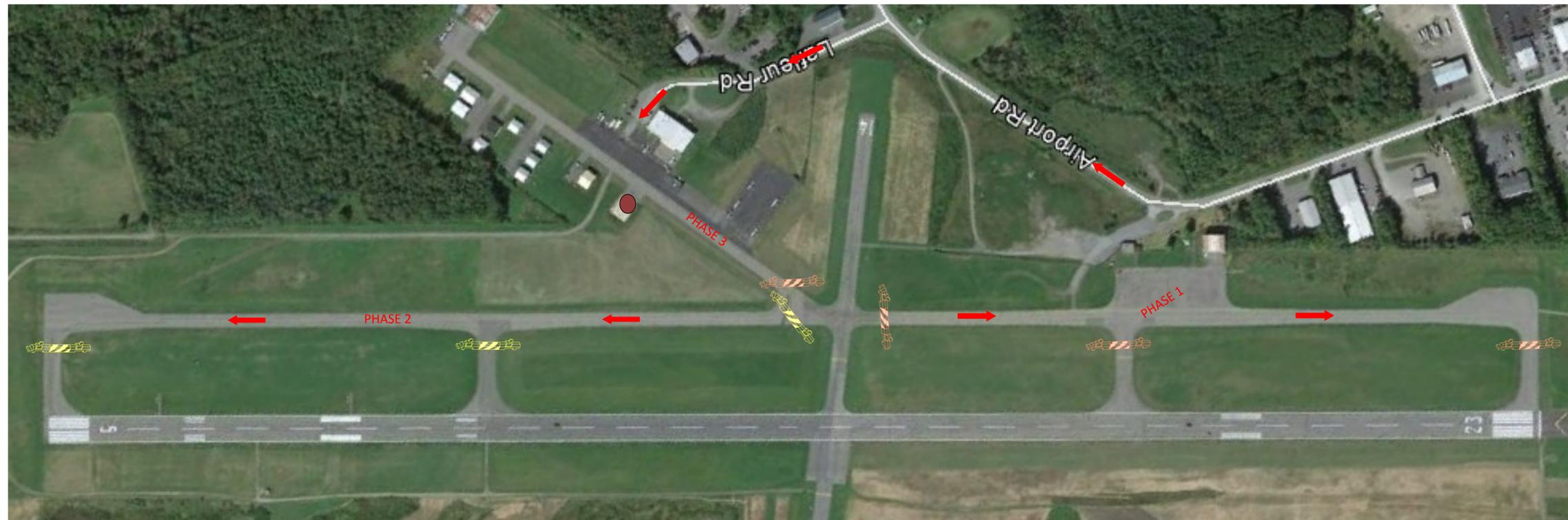
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LOW-PROFILE BARRICADE
NOT TO SCALE

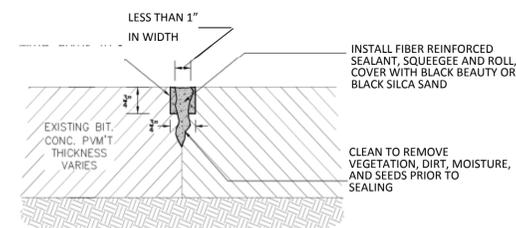
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- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 5.5'-10"(H) X 6'-10"(W) X 96"(L)
- MAX BARRICADE SPACING 4' (END TO END)
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HAUL ROAD NOTES

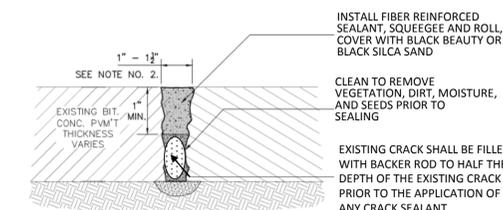
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- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.



NOTE: 1. REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.

PAVEMENT CRACK SEALING - TYPE I DETAIL

N.T.S.



NOTES:

- REHEATING OR REMOVAL OF EXISTING SEALANT, REPAIRATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.
- CRACKS AND JOINTS THAT ARE IN EXCESS OF 1" IN WIDTH SHALL BE CLASSIFIED AS TYPE II CRACKS.

PAVEMENT CRACK SEALING - TYPE II DETAIL

N.T.S.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
- INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

- ~ HAUL ROUTE
- ~ PHASE 1 & 3 BARRICADES
- ~ PHASE 2 BARRICADE
- ~ STAGING AREA

PHASES 1 & 2

- PHASE 1 - NO RUNWAY CLOSURE NEEDED - BARRICADE AS NOTED - CRACKSEAL TAXIWAY AND APRON NORTH OF RUNWAY 14-32
- PHASE 2 - REMOVE PHASE 1 BARRICADES AND PLACE PHASE 2 BARRICADES AS NOTED - CRACKSEAL TAXIWAY SOUTH OF RUNWAY 14-32
- PHASE 3 - REMOVE PHASE 2 BARRICADES AND PLACE PHASE 3 BARRICADES - CRACKSEAL TAXIWAY DELTA AND TERMINAL APRONS AS NEEDED

PAINT REMOVAL ON ALL PHASES AND AIRFIELD MARKING TO BE DONE ONLY FOR PHASE 3

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
018717.00
WIN



TIMOTHY E. LESIEGE
SIGNATURE
PE # 7821
P.E. NUMBER
APRIL 14, 2015
DATE

PROJ. MANAGER	DATE	BY	T.E.L.	DA - FST	T.E.L.
DESIGN-DETAILED	2/22/15	T.E.L.			
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REVISIONS 4					
FIELD CHANGES					

2015 STATEWIDE AIRPORT CRACKSEAL PROJECT
WATERVILLE- ROBERT LaFLEUR AIRPORT
CONSTRUCTION SAFETY AND PHASING
PLAN

SHEET NUMBER
6 OF 7

