



Paul R. LePage
GOVERNOR

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE 04333-0016

David Bernhardt
COMMISSIONER

August 24, 2015
Subject: Bridge Replacement
State WIN: 018229.00
Location: **Scarborough**
Amendment No. 1

Dear Sir/Ms:

Make the following changes to the Bid Documents:

In the Bid Book **ADD** "SPECIAL PROVISION, PROTECTION OF RAILROAD TRAFIC AND STRUCTURES Revised August 24, 2015 (one page)

In the Bid Book (page 72) **REMOVE** "SPECIAL PROVISION, SECTION 105 General Scope of Work (Environmental Requirements) dated July 17, 2015 and **REPLACE** with the attached new SPECIAL PROVISION, SECTION 105 General Scope of Work (Environmental Requirements) dated August 6, 2015.

In the Bid Book (page 76) **REMOVE** "SPECIAL PROVISION, SECTION 107 TIME (Supplemental Liquidated Damaged for Limitations of Operations).

In the Bid Book (page 78) **REMOVE** "SPECIAL PROVISION, SECTION 202 REMOVING STRUCTURES AND OBSTRUCTIONS (Removing Existing Bridge) and **REPLACE** with the attached new SPECIAL PROVISION, SECTION 202 REMOVING STRUCTURES AND OBSTRUCTIONS (Removing Existing Bridge) dated August 19, 2015. (one page)

The following question has been received:

Question: Is the pipe pile reinforcing steel epoxy coated or uncoated?

Response: The pipe pile reinforcing steel is uncoated (i.e. black bar).

Question: Can #4 ties @ 8" o.c. be substituted for the #4 spirals with a pitch of 8" in the pipe piles

Response: A spiral is required.



PRINTED ON RECYCLED PAPER

Question: The Contract Plan set is numbered sheets 1 through 57 on the List of Drawings. However the set of plans on your website ends at sheet 56 of 57. Is there a sheet 57?

Response: There is no sheet 57. Consider sheet 57 to be blank.

Question: Please clarify if the Railroad is being paid by the DOT or the Contractor for the installation & removal of the temporary crossing described on page 70 of 231 of the Contract Bid Book.

Response: The Contractor is paying the Railroad for the installation and removal of the temporary crossing described on page 70 of 231 of the Contract Bid Book.

Question:5 Page 75 of the specification states that the bridge and road shall be reopened to 2 lane traffic by 10/31/16. Page 76 of the specification states that the bridge and road shall be reopened to 2 lane traffic by 11/20/16. Which is correct?

Response: Page 75 is correct. Page 76 shall be removed from the contract book.

Question:6 Page 59 of the specification calls for a daily rate for (4) consecutive hours of work for rail road flagging. In other parts of the specification for railroad flagging (8) hour rates are noted. Is the contractor supposed to add the hourly rate stated for work beyond the (4) hours?

Response:6 Please refer to "SPECIAL PROVISION, PROTECTION OF RAILROAD TRAFIC AND STRUCTURES Revised August 24, 2015 that updates section 14, subsection c.

Question: Please clarify if the existing abutments must be removed in their entirety or just the existing backwalls must be removed. Plan sheet 9 would indicate backwalls only, while the Special Provision Section 202 would suggest the entire abutment.

Response: Please refer to the updated SPECIAL PROVISION, SECTION 202 REMOVING STRUCTURES AND OBSTRUCTIONS (Removing Existing Bridge) dated August 19, 2015

Question: In order to remove existing Pier 1 to footing elevation, it appears shoring will be needed between the footing face and the track rail. The clearness from rail to existing column is field measured at 14'0". Taking away the 3'6" to footing face leaves 6" to install shoring for an E80 Design load. Would this pier column be better left 1'0" below existing grade?

Response: Please refer to the updated SPECIAL PROVISION, SECTION 202 REMOVING STRUCTURES AND OBSTRUCTIONS (Removing Existing Bridge) dated August 19, 2015

Question: MSE wall note 4 on contract drawing sheet 46 states the bearing resistance at the service limit state should be evaluated using a factored bearing resistance of 1.7 KSF to limit

settlement to 1 inch. This bearing resistance cannot be met based on the height of the proposed MSE walls. The applied service limit state bearing will be as high as 5.3 KSF. The Geotechnical report section 6.4.1 estimates 3 inches of settlement for a service limit state bearing of 5.2 KSF. The Geotechnical report values appear reasonable for the height of the structures. Likewise for the strength limit state, we calculate 7.5 KSF, the soil report estimates 7.5 KSF, however Note 4 on sheet 46 limits this value to 5.0 KSF. This value cannot be met without use of lightweight fill. Please check the values listed in Note 4 on sheet 46

Response: Note #4 on sheet 46 is replaced with the following new note #4:

Using pen and ink, the Contractor shall cross out and delete note #4 on sheet 46 of the plans.

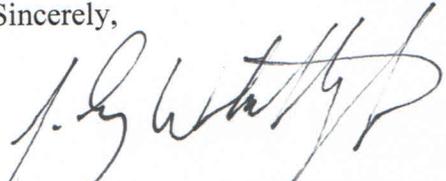
The bearing resistance for the reinforced soil volume and leveling pads founded on 12 inches of crushed stone shall be evaluated at the strength limit state using factored loads and a factored bearing resistance of 7.5 KSF using an effective footing width of 12 feet and a Resistance Factor of 0.65. The bearing resistance shall be evaluated at the service limit state using a factored bearing resistance up to 5.2 KSF to limit settlement to 3 inches. The service limit state settlement will occur during construction of the MSE wall fill. The settlement shall be accounted for in the design of the wall and facing panels.

Question: The MSE Coping detail in the lower left hand corner of Sheet 46 of the contract drawings shows limits of architectural finish, and item 677.06 F 8 of the Special Provision (page 6-208) requires details of the architectural treatment of facing panels. We could not find a description or specification for the architectural finish. Is an architectural finish required?

Response: No architectural treatment is required. Rectangular facing panels shall be used to accommodate anticipated settlement.

Consider this change and information prior to submitting your bid on **August 26, 2015.**

Sincerely,


for George M. A. Macdougall P.E.
Contracts & Specifications Engineer

Town: Scarborough
WIN #: 18229.00
Date: 8/6/2015

SPECIAL PROVISION
SECTION 105
General Scope of Work
(Environmental Requirements)

I. No in-water work is permitted for this project.

II. Special Conditions:

1. Special Conditions of Army Corps of Engineers (ACOE) Category I permit apply (see permit and conditions in contract documents).
2. Recent occurrences of New England Cottontail, a State-listed Endangered Species, have been documented within the project footprint. **The following Special Conditions of the Incidental Take Plan for New England Cottontail (NEC) with Maine Department of Inland Fisheries & Wildlife apply :**
 - a. Clearing shall not occur prior to September 15, 2015. The contractor shall notify MaineDOT Environmental Office Biologist Sarah Boyden (sarah.boyden@maine.gov, (207) 441-0561) at least two weeks prior to site preparation and clearing to coordinate New England Cottontail survey. NEC survey will take up to 1 full day to complete.
 - b. MaineDOT staff shall be on-site during clearing operations to visually inspect for sign of rabbits. If (NEC) are observed during clearing operations, MaineDOT staff will notify the contractor and the contractor shall stop immediately until the observed rabbits leave the area. NEC presence in the project footprint may result in up to a 1-day delay.
 - c. The contractor shall install an exclusionary barrier (e.g. silt fence) serving as a rabbit-proof fence around the perimeter of the construction area to reduce the potential for rabbits encountering construction equipment during bridge replacement operations. The contractor shall bury the bottom edge of the exclusionary barrier. The barrier shall be installed immediately after the pre-construction New England Cottontail survey described in II.2.a. of this Special Provision and prior to clearing. Throughout the construction process, the contractor shall inspect the fence for integrity and repair damaged areas.
 - d. It is unlikely that rabbits will be encountered in the construction area after the exclusionary barrier is installed. However, if the contractor or the resident observes a rabbit within the exclusionary barrier during construction activities, the contractor shall stop work and contact MaineDOT Environmental Office Biologist. MaineDOT staff will work to evacuation the rabbit from the work area. Rabbit evacuation could cause up to a 1-day delay.
 - e. Special Provision 618 and Special Provision 621 outline additional requirements to avoid and minimize post-construction impacts to NEC.

III. Approvals:

1. Temporary Soil Erosion and Water Pollution Control Plan
2. Permitted Resource Impacts (square feet):

Wetland:

Permanent: PSS-750

SPECIAL PROVISION
SECTION 202
REMOVING STRUCTURES AND OBSTRUCTIONS
(Removing Existing Bridge)

202.03 Removing Existing Superstructure, Structural Concrete, Railing, Curbs, Sidewalks and Bridges. This section has been amended to include:

The bridge shall be removed in its entirety with the following exceptions for the substructures:

- The Abutment backwalls and wingwalls shall be completely removed down to the adjacent top of bridge seat elevation. Remaining portions of the abutment that are 3 feet below the finished grade may remain.
- Pier 1 shall be removed to the top of footing or the top of railroad support zone as shown in the plans, whichever is higher.
- Pier 3 shall be removed to the top of footing.

202.08 Basis of Payment This section has been amended to include:

Payment will be made under:

<u>Pay Item</u>		<u>Pay Unit</u>
202.19	Removing Existing Bridge	Lump Sum

Town: Scarborough
WIN 18229.00
Federal Project: BH-1822(900)X
Date: June 12, 2015
Revised: August 24, 2015

**SPECIAL PROVISION
PROTECTION OF RAILROAD TRAFFIC AND STRUCTURES**

Section 14, subsection C is replaced in its entirety with the following:

(c) Estimated Cost. The following is an estimate of the cost per day of inspection/flagging necessary for this project. The rates shown include all overhead charges, travel time, deadheading and personal expenses.

Date of estimate **04/29/2015**.

Estimated daily rate for eight (8) consecutive hours Monday-Friday (straight time): **\$336.00**

Estimated daily rate for eight (8) consecutive hours Saturday, Sunday, Holiday (overtime):
\$513.00

Estimated rate for hours worked in excess of eight (8) hours in any one day: **\$65.00**

Rates charged will be those in effect at the time of the performing the inspection/ flagging which may be different than the rates used at the date of the Estimate. The Railroad agrees to notify the Department if rates used to calculate the above estimates change before the date of bids are received for this Contract.

(d) Definitions.

Man day (M.D.) -eight (8) consecutive hours or any portion thereof.

Overtime -Each additional hour or fraction thereof consecutive to and beyond the standard man day will count as 3/16 of a man day.

Standard Man day -Eight (8) consecutive hour, Monday -Friday between the hours of **7:00** a.m. to **3:30** p.m., minus lunch period, unless otherwise noted and agreed to by all parties.*

*Does not include lunch period from 12:00 – 12:30 p.m.

Travel Time -Time required by flagger and/or inspector to commute between his or her point of headquarters to the project site. This time shall not be charged used in determining available man days.