

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



INDEX OF SHEETS

| Description | Sheet No. |
|---------------------------------------|-----------|
| Title Sheet | 1 |
| Construction Safety and Phasing Plans | 2-8 |



2014 Statewide Airport Crackseal Project

Various Airports:

Auburn-Lewiston, Belfast, Bethel,
Central Maine Airport of Norridgewock,
Eastern Slopes, Sanford Seacoast,
& Wiscasset

FAA AIP

SCOPE OF WORK includes:
Crackseal material delivery,
Crack cleaning, Grinding,
Crackseal application
& Crack Repair



| | | |
|--|--------------------|---------|
| STATE OF MAINE DEPARTMENT OF TRANSPORTATION | APPROVED | DATE |
| | <i>[Signature]</i> | 4/15/14 |
| COMMISSIONER: | | 4-7-14 |
| CHIEF ENGINEER: | <i>[Signature]</i> | |



| | | | |
|--------------------|-------------|-----------|----------------|
| Timothy E. LeSiege | SIGNATURE | PE # 7821 | DATE |
| <i>[Signature]</i> | | | March 21, 2014 |
| | P.E. NUMBER | | |

| PROJECT INFORMATION | |
|-------------------------|---------------------------|
| PROGRAM | AVIATION |
| PROJECT MANAGER | TIMOTHY E. LESIEGE |
| DESIGNER | TIMOTHY E. LESIEGE |
| CONSULTANT | FAY, SPOFFORD & THORNDIKE |
| PROJECT RESIDENT | FAY, SPOFFORD & THORNDIKE |
| CONTRACTOR | |
| PROJECT COMPLETION DATE | |

WIN 020771.00

| |
|--------------|
| TITLE SHEET |
| SHEET NUMBER |

1 OF 8

GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER AND RESIDENT ENGINEER.

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HAUL ROUTES SHOWN ON THE PLANS HAVE BEEN APPROVED BY THE AIRPORT MANAGER. ANY DEVIATION MUST BE APPROVED BY AIRPORT MANAGER OR RESIDENT ENGINEER PRIOR TO UTILIZATION BY THE CONTRACTOR. IT SHOULD BE NOTED THAT DEVIATIONS IN THE HAUL ROUTE MAY NOT BE APPROVED.

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THE CONTRACTOR SHALL HAVE A POWER SWEEPER ON THE SITE WHEN REQUIRED TO SWEEP PAVEMENTS IN WORK AREAS AND HAUL ROUTES AS DIRECTED BY THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.

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NOTAMS SHALL BE ISSUED AS NEEDED BY THE AIRPORT MANAGER AFTER COORDINATING WITH THE CONTRACTOR. NOTAMS SHALL BE COORDINATED A MINIMUM OF 48 HOURS IN ADVANCE UNLESS OTHERWISE SPECIFIED.

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7. RESTROOMS

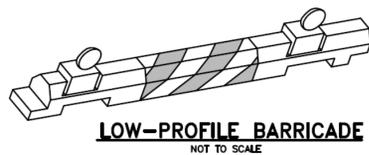
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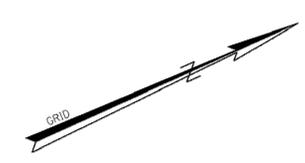
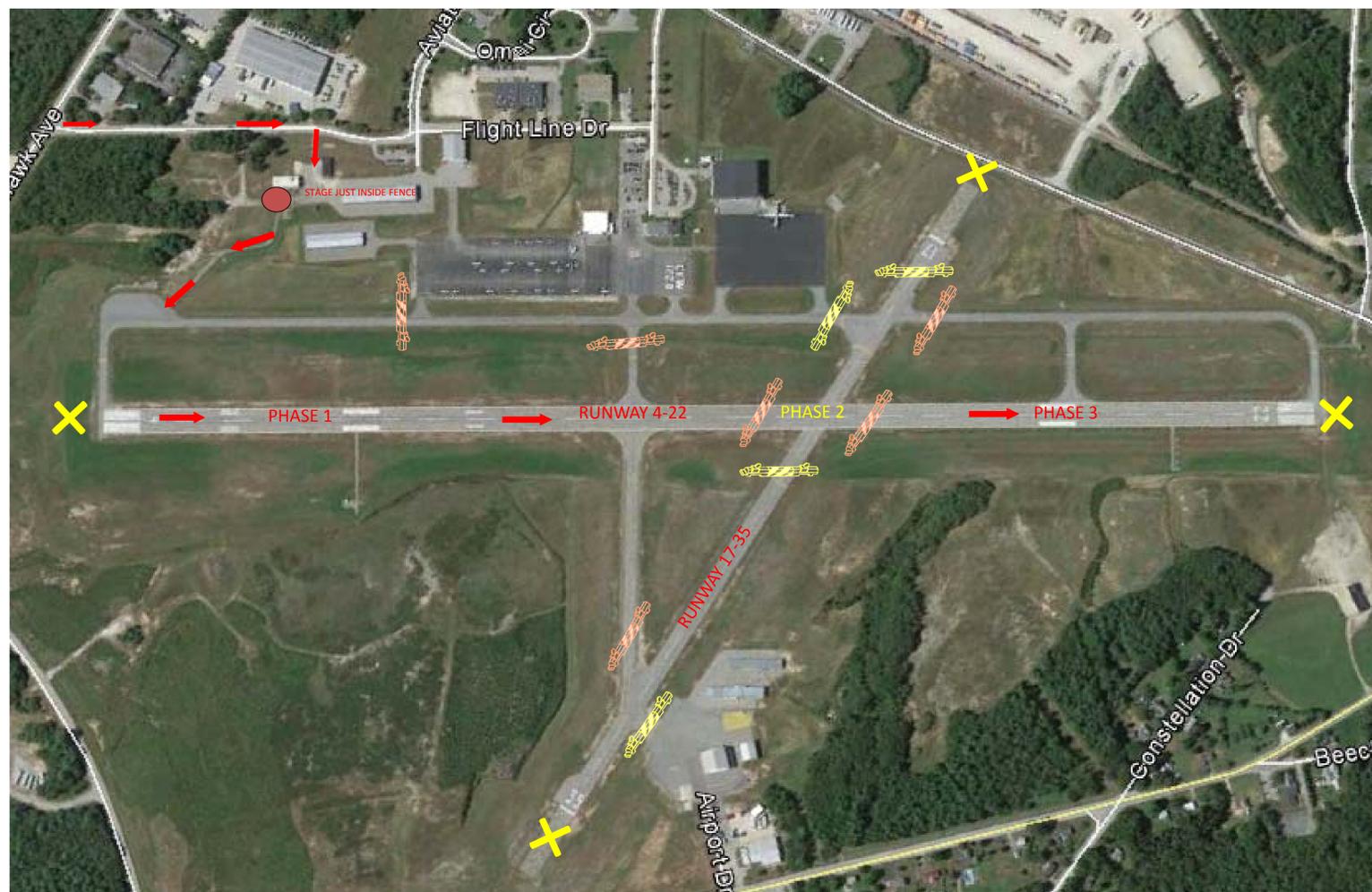
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THE FLAG SHOULD BE AT LEAST A 3-FOOT SQUARE HAVING A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. THE STAFF MAY BE A 48" WOODEN DOWEL.



BARRICADE NOTES

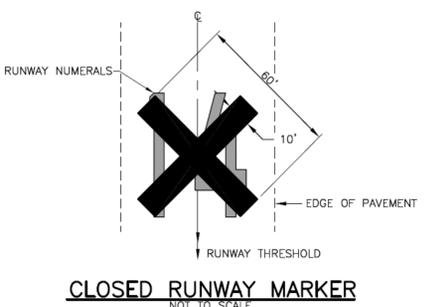
- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
- BARRICADES CAN BE FILLED WITH WATER OR SAND.
- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 6"(H) X 6"(W) X 6'(L)
- MAX BARRICADE SPACING 4' (END TO END)
- BARRICADES AND LIGHTS TO BE PROVIDED BY THE AIRPORT, PLACED BY THE CONTRACTOR AND WILL BE LOCATED AT MAINTENANCE FACILITY
- EACH RUNWAY SHALL BE BARRICADED USING 3 BARRICADES, EACH TAXIWAY SHALL BE BARRICADED USING 2 BARRICADES, THIS COULD CHANGE PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.



PHASES 1, 2 & 3

- PHASE 1 — CLOSE RUNWAY 4-22—BARRICADE AS NOTED—START ON R-4 END WORK EASTERLY TO AOA OF RUNWAY 17-35 ~ RUNWAY 4-22 MUST BE NOTAM CLOSED **
- PHASE 2 — CLOSE RUNWAY 17-35 AND KEEP RUNWAY 4-22 CLOSED — LEAVE PHASE 1 BARRICADES AS NOTED ADD PHASE 2 BARRICADES—COMPLETE CRACKSEALING BETWEEN R-17-35 AOA, R-4-22 AOA AND NORTHERLY ACROSS TAXIWAY TO COVER TAXIWAY OBJECT FREE AREA ~ BOTH RUNWAYS MUST BE NOTAMED CLOSED **
- PHASE 3 — REMOVE PHASE 2 BARRICADES ONLY—REOPEN RUNWAY 17-35—WORK FROM R-17-35 AOA EASTERLY TO END OF RUNWAY 22**

** ESCORT MANDATORY TO SET BARRICADES AND CLOSED RUNWAY MARKER EAST OF RUNWAY 17-35 AND FOR PHASE 3 AND ANY TIME CROSSING ACTIVE RUNWAYS

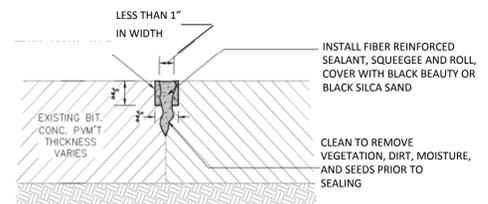


CLOSED RUNWAY MARKER NOTES

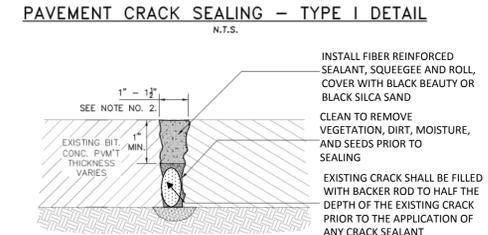
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- MARKERS SHALL BE HIGH STRENGTH, YELLOW VINYL, REUSABLE CLOSURE MARKERS WITH FACTORY INSTALLED BALLAST TUBES. MARKERS SHALL BE FAA APPROVED.
- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE CONTRACTOR, PLACED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.

HAUL ROAD NOTES

- THE CONTRACTOR SHALL ONLY BE PERMITTED TO WORK IN THE DESIGNATED WORK AREAS SHOWN ON THIS SHEET AND ON THE DESIGNATED HAUL ROADS AND STAGING AREAS AS SHOWN ON THIS SHEET. VEHICLES SHALL TRAVEL TO AND FROM WORK AREAS ON ESTABLISHED ROADS AND PATHS AND SHALL NOT TRAVEL IN GRASSLANDS, WETLANDS, OR SENSITIVE RESOURCE AREAS.
- THE CONTRACTOR WILL BE PROHIBITED FROM CROSSING RUNWAYS, TAXIWAYS, OR APRONS WITH CONSTRUCTION EQUIPMENT UNLESS PRIOR APPROVAL FROM THE AIRPORT IS RECEIVED. AT APPROVED CROSSING, THE CONTRACTOR WILL BE RESPONSIBLE TO PROTECT THE EXISTING PAVEMENT FROM DAMAGE. ALL DAMAGED PAVEMENT AREAS WILL BE REQUIRED TO BE REPAIRED TO FAA STANDARDS, AND TO THE SATISFACTION OF THE OWNER, AT THE EXPENSE OF THE CONTRACTOR. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM GENERATING THE OPERATION.
- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL AOA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXIWAYS OR RUNWAYS AND AS DIRECTED BY THE RESIDENT ENGINEER AND/OR AIRPORT MANAGER. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH, 2 FOOT HIGH (MAX) STOP SIGNS ON SITE FOR THIS PURPOSE.



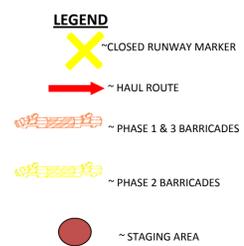
- NOTES:
- REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.



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- REHEATING OR REMOVAL OF EXISTING SEALANT, REPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.
 - CRACKS AND JOINTS THAT ARE IN EXCESS OF 1" IN WIDTH SHALL BE CLASSIFIED AS TYPE II CRACKS.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
- INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.



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|---|---------|---|------|
| STATE OF MAINE DEPARTMENT OF TRANSPORTATION | | 020771.00 WIN | |
| 2014 STATEWIDE AIRPORT CRACK SEAL PROJECT AUBURN—LEWISTON MUNICIPAL AIRPORT PHASES 1, 2 & 3 | | CONSTRUCTION SAFETY AND PHASING PLAN | |
| PROJ. NUMBER | DATE | BY | DATE |
| DESIGN-DRAWN | 2/12/14 | T.E.L. | |
| CHECKED-REVIEWED | 3/20/14 | D.A. | |
| DESIGN-DRAWN | 3/21/14 | T.E.L. | |
| DESIGN-DRAWN | | T.E.L. | |
| REVISION 1 | | | |
| REVISION 2 | | | |
| REVISION 3 | | | |
| REVISION 4 | | | |
| FIELD CHANGES | | | |
| SHEET NUMBER | | 2 OF 8 | |



TIMOTHY E. LESIEGE
REGISTERED PROFESSIONAL ENGINEER
LICENSE NO. 1621
P.E. # 7821
MARCH 21, 2014
DATE

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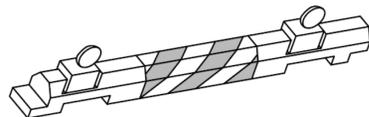
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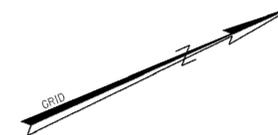
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LOW-PROFILE BARRICADE
NOT TO SCALE

BARRICADE NOTES

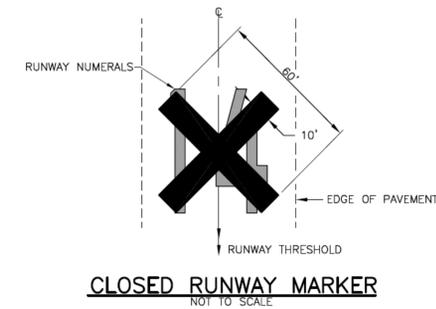
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PHASES 4 & 5

- PHASE 4 — CLOSE RUNWAY 17-35—BARRICADE AS NOTED—START ON R-35 END WORK NORTHERLY TO AOA OF RUNWAY 4-22 ** — RUNWAY 17-35 MUST BE NOTAMED CLOSED
- PHASE 5 — KEEP RUNWAY CLOSED — LEAVE BARRICADES AS NOTED—START AT TAXIWAY SAFETY AREA AND CONTINUE NORTHERLY TO END OF RUNWAY **

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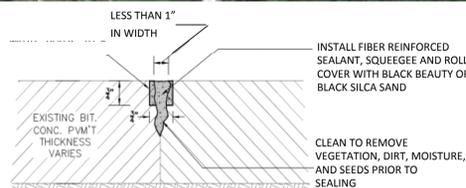
CLOSED RUNWAY MARKER
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CLOSED RUNWAY MARKER NOTES

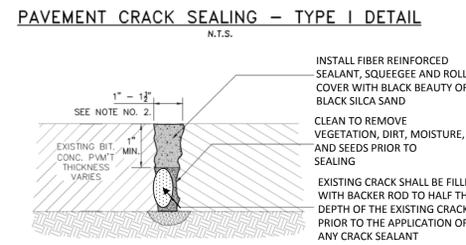
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PAVEMENT CRACK SEALING -- TYPE I DETAIL
N.T.S.



PAVEMENT CRACK SEALING -- TYPE II DETAIL
N.T.S.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
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LEGEND

- ~ CLOSED RUNWAY MARKER
- ~ HAUL ROUTE
- ~ PHASE 4 & 5 BARRICADES
- ~ STAGING AREA

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
020771.00
WIN

TIMOTHY E. LESIEGE
SIGNATURE
P.E. # 7821
MARCH 21, 2014
DATE

| PROJ. MANAGER | T.E.L. | BY | DATE |
|------------------|--------|--------|---------|
| DESIGN-DETAILED | T.E.L. | T.E.L. | 2/17/14 |
| CHECKED-REVIEWED | D.A. | D.A. | 3/20/14 |
| DESIGN-DETAILED | T.E.L. | T.E.L. | 3/21/14 |
| REVISIONS 1 | | | |
| REVISIONS 2 | | | |
| REVISIONS 3 | | | |
| REVISIONS 4 | | | |
| FIELD CHANGES | | | |

2014 STATEWIDE AIRPORT CRACKSEAL PROJECT
AUBURN—LEWISTON MUNICIPAL AIRPORT
PHASES 4 & 5
CONSTRUCTION SAFETY AND PHASING
PLAN

SHEET NUMBER

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THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGHWAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

HAUL ROUTES SHOWN ON THE PLANS HAVE BEEN APPROVED BY THE AIRPORT MANAGER. ANY DEVIATION MUST BE APPROVED BY AIRPORT MANAGER OR RESIDENT ENGINEER PRIOR TO UTILIZATION BY THE CONTRACTOR. IT SHOULD BE NOTED THAT DEVIATIONS IN THE HAUL ROUTE MAY NOT BE APPROVED.

4. AIRPORT OPERATIONS AND SAFETY REQUIREMENTS

RESTRICTED AIRPORT OPERATIONS WILL BE CONDUCTED DURING CONSTRUCTION. THE CONTRACTORS WORK SHALL BE CARRIED ON IN SUCH A MANNER AS NOT TO INTERFERE WITH THE AIRPORT OPERATIONS. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO INSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS THEIR OWN EQUIPMENT AND PERSONNEL. THE CONTRACTORS SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL HAVE A POWER SWEEPER ON THE SITE WHEN REQUIRED TO SWEEP PAVEMENTS IN WORK AREAS AND HAUL ROUTES AS DIRECTED BY THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.

5. NOTICES TO AIRMEN (NOTAMS)

NOTAMS SHALL BE ISSUED AS NEEDED BY THE AIRPORT MANAGER AFTER COORDINATING WITH THE CONTRACTOR. NOTAMS SHALL BE COORDINATED A MINIMUM OF 48 HOURS IN ADVANCE UNLESS OTHERWISE SPECIFIED.

6. AIRPORT RADIOS

THE CONTRACTORS SHALL PROVIDE A MINIMUM OF TWO AVIATION UNICOM RADIOS FOR USE DURING THE PROJECT. EACH RADIO SHALL BE CAPABLE OF TRANSMITTING ON FREQUENCY 122.80 MHZ AND BE EQUIPPED WITH A RECHARGING UNIT AND SPARE BATTERY PACK. RADIOS SHALL BE COM BRAND, MODEL LC-233 OR APPROVED EQUAL. THE CONTRACTOR SHALL CONTINUOUSLY MONITOR LOCAL AIR TRAFFIC AT ALL TIMES WHEN THEIR FORCES ARE LOCATED WITHIN ANY ADA. RADIO COSTS ARE CONSIDERED INCIDENTAL TO THE PROJECT.

7. RESTROOMS

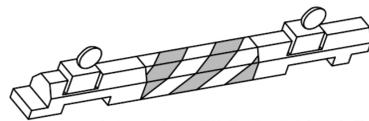
RESTROOM FACILITIES WILL BE PROVIDED AND MAINTAINED IN THE STAGING AREA BY THE CONTRACTOR. THE STAGING AREA WILL BE RESTORED BY THE CONTRACTOR, TO ORIGINAL CONDITION AS APPROVED BY THE OWNER.

8. CONTRACTOR VEHICLES

ALL CONTRACTORS VEHICLES SHALL HAVE THE COMPANY IDENTIFICATION PLAINLY VISIBLE ON BOTH SIDES OF THE VEHICLE IN ORDER TO IDENTIFY THE VEHICLE.

ALL VEHICLES THAT OPERATE DURING THE DAYLIGHT IN THE AIRCRAFT OPERATIONS AREA MUST BE PROVIDED WITH A FLASHING YELLOW BEACON OR FLAG ON A STAFF ATTACHED TO THE VEHICLE SO THAT THE FLAG CAN BE READILY VISIBLE. THIS INCLUDES PICKUP TRUCKS, BACKHOES, FRONT END LOADERS, SKID STEERS, PAVERS, GRADERS, ETC.

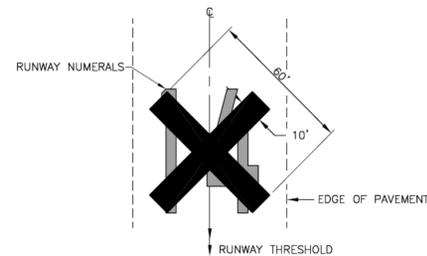
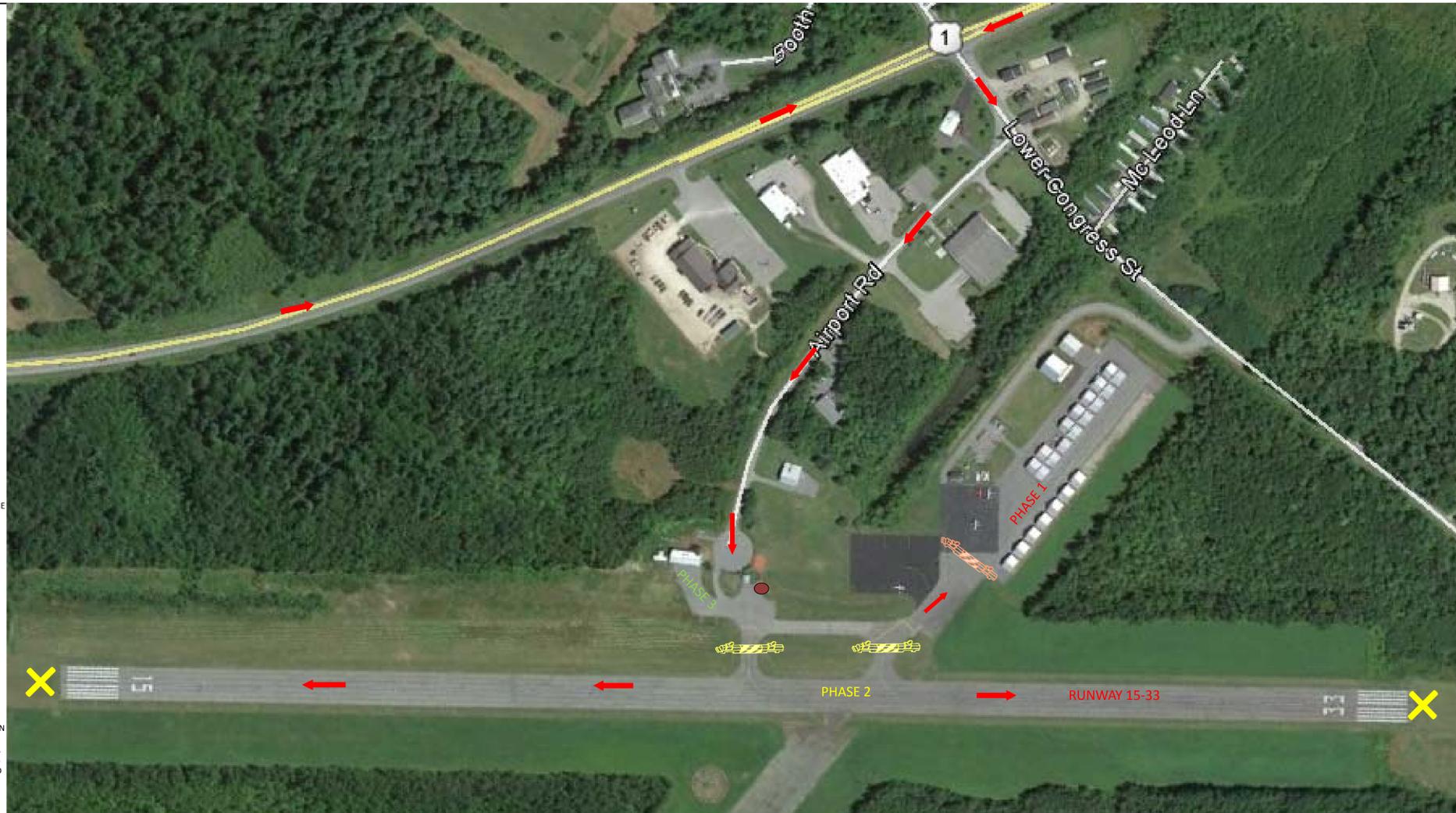
THE FLAG SHOULD BE AT LEAST A 3-FOOT SQUARE HAVING A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. THE STAFF MAY BE A 48" WOODEN DOWEL.



LOW-PROFILE BARRICADE
NOT TO SCALE

BARRICADE NOTES

- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
- BARRICADES CAN BE FILLED WITH WATER OR SAND.
- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 6"(H) X 6"(W) X 6"(L)
- MAX BARRICADE SPACING 4' (END TO END)
- FOUR (4) EACH LOW PROFILE BARRICADES ARE TO BE PROVIDED AND PLACED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE BARRICADES WILL BE MADE. THE BARRICADES WILL REMAIN THE PROPERTY OF THE CONTRACTOR UPON COMPLETION OF THE WORK OF THIS PROJECT AND ARE TO BE AVAILABLE FOR USE AT THE OTHER AIRPORTS ASSOCIATED WITH THIS CONTRACT.



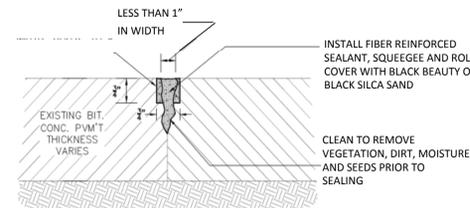
CLOSED RUNWAY MARKER
NOT TO SCALE

CLOSED RUNWAY MARKER NOTES

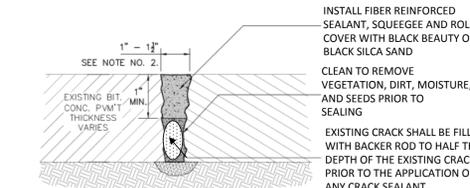
- CLOSED RUNWAY MARKERS (X'S) TO BE IN PLACE DURING CONSTRUCTION PERIOD AND/OR AS DIRECTED BY THE ENGINEER OR AIRPORT MANAGER. MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERAL OR ALONG THE EXTENDED RUNWAY CENTERLINE PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.
- MARKERS SHALL BE HIGH STRENGTH, YELLOW VINYL, REUSABLE CLOSURE MARKERS WITH FACTORY INSTALLED BALLAST TUBES. MARKERS SHALL BE FAA APPROVED.
- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.
- BELFAST WILL HAVE A 20 MINUTE PRIOR PERMISSION REQUIRED NEEDED TO LAND DURING THIS PROJECT. SHOULD AN AIRCRAFT REQUEST LANDING, THEY SHALL BE INFORMED OF THE PROJECT (HOT CRACK SEAL ON RUNWAY) AND AUTHORIZED TO LAND REGARDLESS OF MARKERS. CONTRACTOR IS TO CLEAR ALL PERSONNEL AND EQUIPMENT OFF OF THE RUNWAY UNTIL THE AIRCRAFT IS SAFELY ON THE APRON AND CLEAR OF THE WORK AREA. THE MARKERS SHALL ONLY BE REMOVED WHEN THE RUNWAY PORTION OF THE PROJECT IS COMPLETE.

HAUL ROAD NOTES

- THE CONTRACTOR SHALL ONLY BE PERMITTED TO WORK IN THE DESIGNATED WORK AREAS SHOWN ON THIS SHEET AND ON THE DESIGNATED HAUL ROADS AND STAGING AREAS AS SHOWN ON THIS SHEET. VEHICLES SHALL TRAVEL TO AND FROM WORK AREAS ON ESTABLISHED ROADS AND PATHS AND SHALL NOT TRAVEL IN GRASSLANDS, WETLANDS, OR SENSITIVE RESOURCE AREAS.
- THE CONTRACTOR WILL BE PROHIBITED FROM CROSSING RUNWAYS, TAXIWAYS, OR APRONS WITH CONSTRUCTION EQUIPMENT UNLESS PRIOR APPROVAL FROM THE AIRPORT IS RECEIVED. AT APPROVED CROSSING, THE CONTRACTOR WILL BE RESPONSIBLE TO PROTECT THE EXISTING PAVEMENT FROM DAMAGE. ALL DAMAGED PAVEMENT AREAS WILL BE REQUIRED TO BE REPAIRED TO FAA STANDARDS, AND TO THE SATISFACTION OF THE OWNER, AT THE EXPENSE OF THE CONTRACTOR. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM GENERATING THE OPERATION.
- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL ADA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXIWAYS AND AS DIRECTED BY THE ENGINEER AND/OR AIRPORT MANAGER. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH 2 FOOT HIGH (MAX) STOP SIGNS ON SITE FOR THIS PURPOSE.



PAVEMENT CRACK SEALING - TYPE I DETAIL
N.T.S.



PAVEMENT CRACK SEALING - TYPE II DETAIL
N.T.S.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
- INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

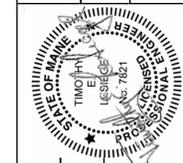
- ~CLOSED RUNWAY MARKER
- ~HAUL ROUTE
- ~PHASE 1 BARRICADE
- ~PHASE 2 BARRICADE
- ~STAGING AREA

PHASES 1, 2 & 3

- PHASE 1— BARRICADE AS NOTED—START ON OLDER PAVEMENT ON TAXIWAYS NEAR HANGARS AND FINISH TERMINAL APRON AREA
- PHASE 2 — CLOSE RUNWAY 15-33 — PLACE PHASE 2 BARRICADES AS NOTED— CRACKSEAL R-15-33 AND TAXIWAYS— RUNWAY 15-33 MUST BE NOTAMED CLOSED **
- PHASE 3— REMOVE BARRICADES— CRACK SEAL IN FRONT OF LARGE HANGAR ONLY— DO NOT CRACKSEAL ALLIGATOR CRACKED AREAS

** AIRPORT MAY REQUIRE THAT WORK ON THE RUNWAY BE DONE WITH A 20 MINUTE PRIOR PERMISSION REQUIRED NOTAM— THIS WOULD ALLOW NECESSARY JET TRAFFIC TO LAND WITH 20 MINUTES NOTICE GIVING CONTRACTOR TIME TO CLEAR THE RUNWAY

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
020771.00
WIN



TIMOTHY E. LESIEGE
ARCHITECTURE
PE # 7821
P.L. NUMBER
MARCH 21, 2014
DATE

| PROJ. NUMBER | DATE | BY | T.E.L. | REVISIONS |
|--|---------|--------|--------|---------------|
| 2014 STATEWIDE AIRPORT CRACKSEAL PROJECT | 2/12/14 | T.E.L. | | 1 |
| | 3/20/14 | D.A. | | 2 |
| | 3/21/14 | T.E.L. | | 3 |
| | | | | 4 |
| | | | | FIELD CHANGES |

2014 STATEWIDE AIRPORT CRACKSEAL PROJECT
BELFAST MUNICIPAL AIRPORT
CONSTRUCTION SAFETY AND PHASING PLAN

SHEET NUMBER

GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER AND RESIDENT ENGINEER.

3. CONTRACTORS STAGING AREAS AND HAUL ROUTE

THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLAN OR AS DIRECTED BY THE AIRPORT MANAGER OR THE RESIDENT ENGINEER FOR THEIR STAGING AREA. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL PROTECTION AND RESTORATION OF THE DESIGNATED AREA. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRASH PICK-UP GENERATED BY THE PROJECT. THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGH-WAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

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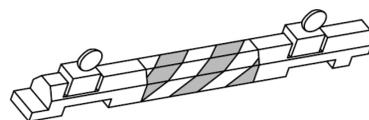
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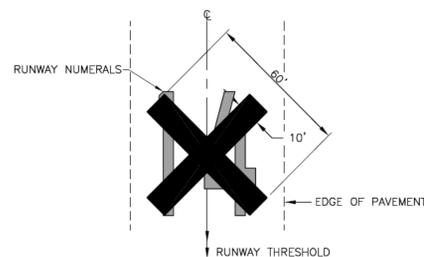
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LOW-PROFILE BARRICADE
NOT TO SCALE

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- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 6"(H) X 6"(W) X 6"(L)
- MAX BARRICADE SPACING 4' (END TO END)
- BARRICADES AND LIGHTS TO BE PROVIDED BY THE AIRPORT AND PLACED BY THE CONTRACTOR.
- THE APRON, TAXIWAY AND RUNWAY SHALL BE BARRICADED PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.



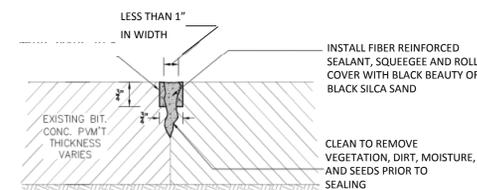
CLOSED RUNWAY MARKER
NOT TO SCALE

CLOSED RUNWAY MARKER NOTES

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- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE AIRPORT, PLACED BY THE CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.

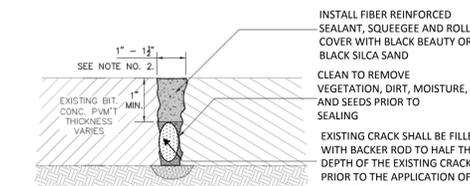
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- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL AOA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
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- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXILANES AND AS DIRECTED BY THE ENGINEER AND/OR AIRPORT MANAGER. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH 2 FOOT HIGH (MAX) STOP SIGNS FOR THIS PURPOSE.
- HAUL ROUTE MAY BE CHANGED BY RESIDENT ENGINEER ON SITE DUE TO ONGOING WORK AT THE TERMINAL BUILDING



- NOTE:
- REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.

PAVEMENT CRACK SEALING -- TYPE I DETAIL
N.T.S.



- NOTES:
- REHEATING OR REMOVAL OF EXISTING SEALANT, REPAIR OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.
 - CRACKS AND JOINTS THAT ARE IN EXCESS OF 1" IN WIDTH SHALL BE CLASSIFIED AS TYPE II CRACKS.

PAVEMENT CRACK SEALING -- TYPE II DETAIL
N.T.S.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
- INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

- ~CLOSED RUNWAY MARKER
- ~HAUL ROUTE
- ~PHASE 1 BARRICADES
- ~PHASE 2 BARRICADES
- ~PROPOSED STAGING AREA

PHASES 1 & 2

- PHASE 1 -- BARRICADE AS NOTED -- START ON NORTHERLY END OF TAXILANES BETWEEN HANGARS AND WORK OUTWARD TO FINISH TERMINAL APRON AREA
- PHASE 2 -- CLOSE RUNWAY 14-32 -- LEAVE PHASE 1 BARRICADES AS NOTED AND PLACE PHASE 2 BARRICADES AS NOTED BLOCKING TAXILANES BETWEEN HANGARS -- CRACKSEAL REMAINDER OF TAXILANES, APRON, TAXIWAYS AND RUNWAY R-14-32 -- RUNWAY 14-32 MUST BE NOTAMED CLOSED **

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| 3/27/14 | T.E.L. | | |

GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER/OPERATOR SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER/OPERATOR AND RESIDENT ENGINEER.

3. CONTRACTORS STAGING AREAS AND HAUL ROUTE

THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLAN FOR THEIR STAGING AREA. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL PROTECTION AND RESTORATION OF THE DESIGNATED AREA. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRASH PICK-UP GENERATED BY THE PROJECT. THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGHWAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

HAUL ROUTES SHOWN ON THE PLANS HAVE BEEN APPROVED BY THE AIRPORT MANAGER/OPERATOR. ANY DEVIATION MUST BE APPROVED BY AIRPORT MANAGER/OPERATOR OR RESIDENT ENGINEER PRIOR TO UTILIZATION BY THE CONTRACTOR. IT SHOULD BE NOTED THAT DEVIATIONS IN THE HAUL ROUTE MAY NOT BE APPROVED.

4. AIRPORT OPERATIONS AND SAFETY REQUIREMENTS

RESTRICTED AIRPORT OPERATIONS WILL BE CONDUCTED DURING CONSTRUCTION. THE CONTRACTORS WORK SHALL BE CARRIED ON IN SUCH A MANNER AS NOT TO INTERFERE WITH THE AIRPORT OPERATIONS. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO INSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS THEIR OWN EQUIPMENT AND PERSONNEL. THE CONTRACTORS SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL HAVE A POWER SWEEPER ON THE SITE WHEN REQUIRED TO SWEEP PAVEMENTS IN WORK AREAS AND HAUL ROUTES AS DIRECTED BY THE AIRPORT MANAGER/OPERATOR OR THE RESIDENT ENGINEER.

5. NOTICES TO AIRMEN (NOTAMS)

NOTAMS SHALL BE ISSUED AS NEEDED BY THE AIRPORT MANAGER/OPERATOR AFTER COORDINATING WITH THE CONTRACTOR. NOTAMS SHALL BE COORDINATED A MINIMUM OF 48 HOURS IN ADVANCE UNLESS OTHERWISE SPECIFIED.

6. AIRPORT RADIOS

THE CONTRACTORS SHALL PROVIDE A MINIMUM OF TWO AVIATION UNICOM RADIOS FOR USE DURING THE PROJECT. EACH RADIO SHALL BE CAPABLE OF TRANSMITTING ON FREQUENCY 122.80 MHZ AND BE EQUIPPED WITH A RECHARGING UNIT AND SPARE BATTERY PACK. RADIOS SHALL BE ICOM BRAND, MODEL IC-A23 OR APPROVED EQUAL. THE CONTRACTOR SHALL CONTINUOUSLY MONITOR LOCAL AIR TRAFFIC AT ALL TIMES WHEN THEIR FORCES ARE LOCATED WITHIN ANY AOA. RADIO COSTS ARE CONSIDERED INCIDENTAL TO THE PROJECT.

7. RESTROOMS

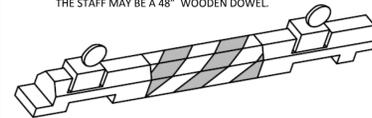
RESTROOM FACILITIES WILL BE PROVIDED AND MAINTAINED IN THE STAGING AREA BY THE CONTRACTOR. THE STAGING AREA WILL BE RESTORED BY THE CONTRACTOR, TO ORIGINAL CONDITION APPROVED BY THE OWNER.

8. CONTRACTOR VEHICLES

ALL CONTRACTORS VEHICLES SHALL HAVE THE COMPANY IDENTIFICATION PLAINLY VISIBLE ON BOTH SIDES OF THE VEHICLE IN ORDER TO IDENTIFY THE VEHICLE.

ALL VEHICLES THAT OPERATE DURING THE DAYLIGHT IN THE AIRCRAFT OPERATIONS AREA MUST BE PROVIDED WITH A FLASHING YELLOW BEACON OR FLAG ON A STAFF ATTACHED TO THE VEHICLE SO THAT THE FLAG CAN BE READILY VISIBLE. THIS INCLUDES PICKUP TRUCKS, BACKHOES, FRONT END LOADERS, SKID STEERS, PAVERS, GRADERS, ETC.

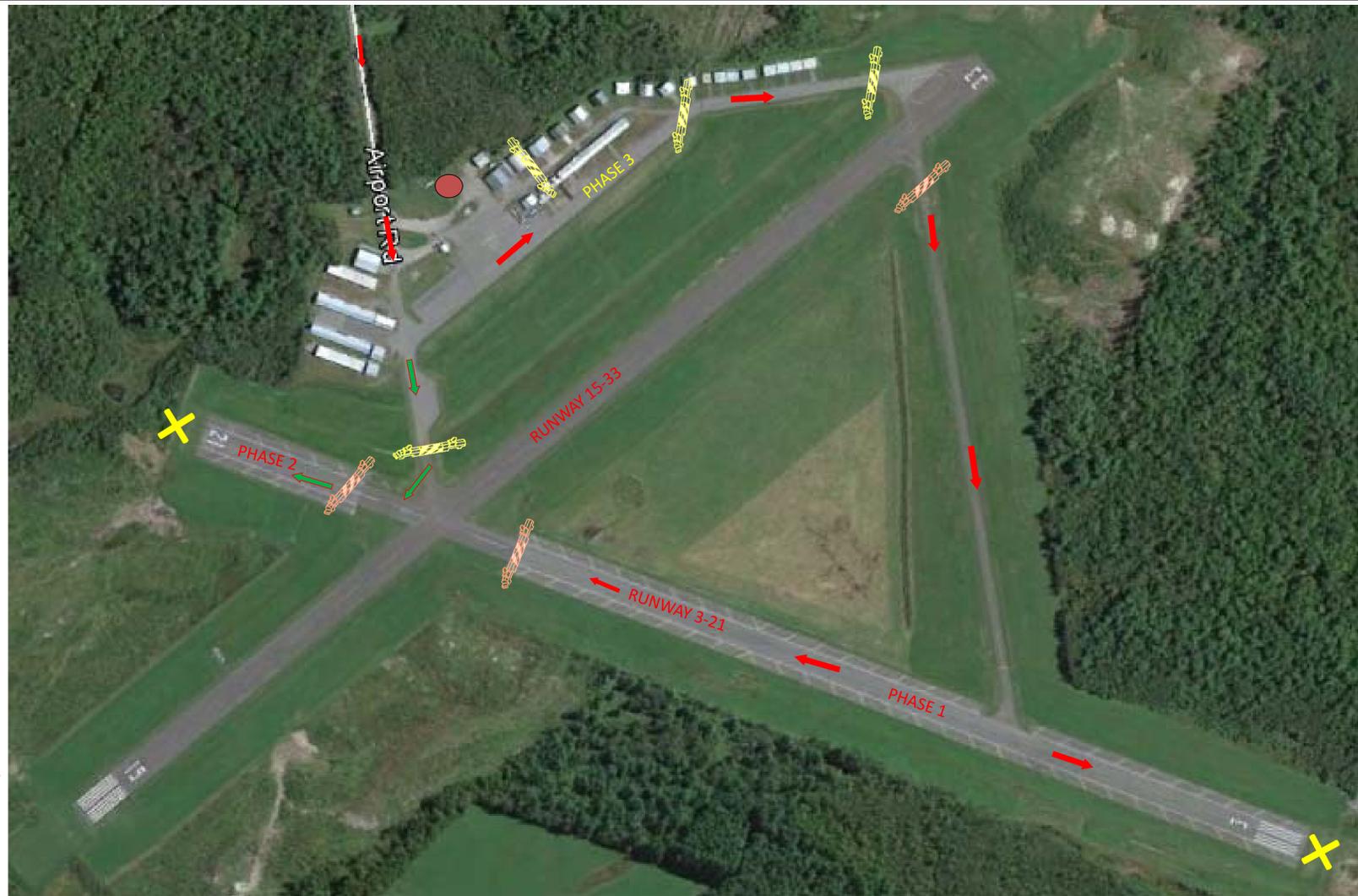
THE FLAG SHOULD BE AT LEAST A 3-FOOT SQUARE HAVING A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. THE STAFF MAY BE A 48" WOODEN DOWEL.



LOW-PROFILE BARRICADE
NOT TO SCALE

BARRICADE NOTES

- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
- BARRICADES CAN BE FILLED WITH WATER OR SAND.
- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 6"(H) X 6"(W) X 6'(L)
- MAX BARRICADE SPACING 4' (END TO END)
- BARRICADES AND LIGHTS TO BE PROVIDED BY THE AIRPORT AND PLACED BY CONTRACTOR.
- EACH RUNWAY AND TAXIWAY SHALL BE BARRICADED PER THE AIRPORT MANAGER/OPERATOR OR THE RESIDENT ENGINEER.



PHASES 1, 2 & 3

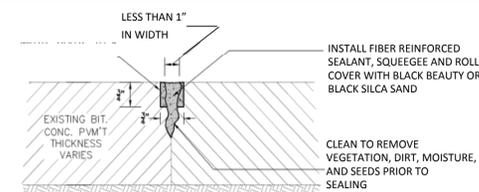
- PHASE 1— CLOSE RUNWAY 3-21—BARRICADE AS NOTED—START ON R-3 END WORK NORTHERLY TO AOA OF RUNWAY 15-33 ~ RUNWAY 3-21 MUST BE NOTAM CLOSED **
- PHASE 2 — KEEP RUNWAY 3-21 CLOSED — LEAVE PHASE 1 BARRICADES AS NOTED — COMPLETE CRACKSEALING BETWEEN R-15-33 AOA AND END OF RUNWAY 3-21 ~ RUNWAY 3-21 MUST REMAIN NOTAM CLOSED **
- PHASE 3— REMOVE PHASE 1 BARRICADES —REOPEN RUNWAY 3-21—PLACE BARRICADES AS DIRECTED BY AIRPORT MANAGER/OPERATOR OR THE RESIDENT ENGINEER -- START WORK AT RUNWAY 33 END AND CONTINUE NORTHWESTERLY ALONG TAXIWAY THROUGH TERMINAL APRON TO THE OTHER END OF THE TAXIWAY INCLUDING ALL TAXIWAYS ~ AVAILABILITY OF ALLOTTED PRODUCT PERMITTING**

** ESCORT MANDATORY TO SET BARRICADES AND CLOSED RUNWAY MARKERS FOR PHASES 1 & 2 AND WHEN WORKING IN AOA OF RUNWAY 33 END AND ANY TIME CROSSING ACTIVE RUNWAYS

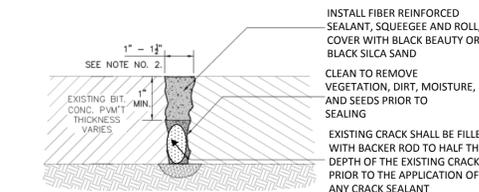
RUNWAY 15-33 IS TO REMAIN OPEN AT ALL TIMES

HAUL ROAD NOTES

- THE CONTRACTOR SHALL ONLY BE PERMITTED TO WORK IN THE DESIGNATED WORK AREAS SHOWN ON THIS SHEET AND ON THE DESIGNATED HAUL ROADS AND STAGING AREAS AS SHOWN ON THIS SHEET. VEHICLES SHALL TRAVEL TO AND FROM WORK AREAS ON ESTABLISHED ROADS AND PATHS AND SHALL NOT TRAVEL IN GRASSLANDS, WETLANDS, OR SENSITIVE RESOURCE AREAS.
- THE CONTRACTOR WILL BE PROHIBITED FROM CROSSING RUNWAYS, TAXIWAYS, OR APRONS WITH CONSTRUCTION EQUIPMENT UNLESS PRIOR APPROVAL FROM THE AIRPORT IS RECEIVED. AT APPROVED CROSSING, THE CONTRACTOR WILL BE RESPONSIBLE TO PROTECT THE EXISTING PAVEMENT FROM DAMAGE. ALL DAMAGED PAVEMENT AREAS WILL BE REQUIRED TO BE REPAIRED TO FAA STANDARDS, AND TO THE SATISFACTION OF THE OWNER, AT THE EXPENSE OF THE CONTRACTOR. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM GENERATING THE OPERATION.
- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL AOA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXIWAYS AND AS DIRECTED BY THE ENGINEER AND/OR AIRPORT MANAGER/OPERATOR. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH 2 FOOT HIGH (MAX) STOP SIGNS ON SITE FOR THIS PURPOSE.



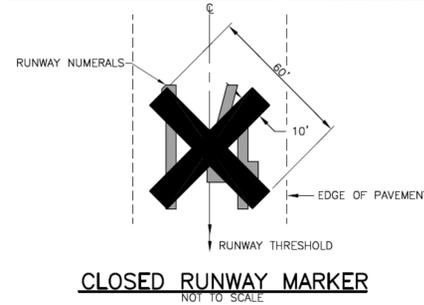
PAVEMENT CRACK SEALING – TYPE I DETAIL
N.T.S.



PAVEMENT CRACK SEALING – TYPE II DETAIL
N.T.S.

CRACKSEAL NOTE

- FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
- INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.



CLOSED RUNWAY MARKER
NOT TO SCALE

CLOSED RUNWAY MARKER NOTES

- CLOSED RUNWAY MARKERS (X'S) TO BE IN PLACE DURING CONSTRUCTION PERIOD AND/OR AS DIRECTED BY THE ENGINEER OR AIRPORT MANAGER/OPERATOR. MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERAL OR ALONG THE EXTENDED RUNWAY CENTERLINE PER THE AIRPORT MANAGER/OPERATOR OR THE RESIDENT ENGINEER.
- MARKERS SHALL BE HIGH STRENGTH, YELLOW VINYL, REUSABLE CLOSURE MARKERS WITH FACTORY INSTALLED BALLAST TUBES. MARKERS SHALL BE FAA APPROVED.
- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE AIRPORT, PLACED BY CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.

LEGEND

- ~CLOSED RUNWAY MARKER
- ~ HAUL ROUTE
- ~ PHASE 2 HAUL ROUTE
- ~ BARRICADES
- ~ PHASE 3 BARRICADES
- ~ STAGING AREA

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| STATE OF MAINE DEPARTMENT OF TRANSPORTATION | | 020771.00 WIN | |
| | | P.R. NUMBER: MARCH 21, 2014 DATE: | |
| PROJ. NUMBER DESIGNED BY CHECKED BY DESIGNED BY REVISION 1 REVISION 2 REVISION 3 REVISION 4 FIELD CHANGES | DATE 2/12/14 3/20/14 3/21/14 | BY T.E.L. D.A. T.E.L. | TIMOTHY E. LESIGE REGISTERED PROFESSIONAL ENGINEER PE # 7821 |
| 2014 STATEWIDE AIRPORT CRACKSEAL PROJECT CENTRAL MAINE AIRPORT OF NORRIDGEWOCK | | CONSTRUCTION SAFETY AND PHASING PLAN | |
| SHEET NUMBER | | 6 of 8 | |

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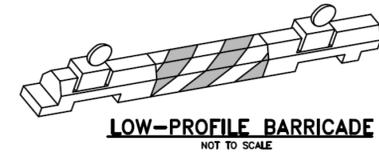
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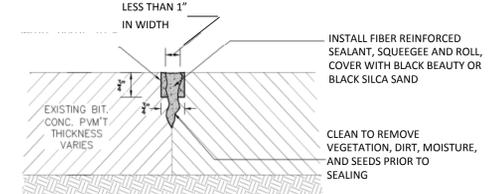


BARRICADE NOTES

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2. BARRICADES CAN BE FILLED WITH WATER OR SAND.
3. DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
4. MADE OF DURABLE POLYETHYLENE PLASTIC.
5. DIMENSIONS: 6"(H) X 6"(W) X 6"(L)
6. MAX BARRICADE SPACING 4' (END TO END)
7. BARRICADES AND LIGHTS TO BE PROVIDED BY THE AIRPORT AND PLACED BY THE CONTRACTOR.
8. EACH TAXIWAY AND THE APRON SHALL BE BARRICADED PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.

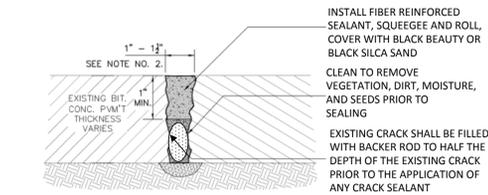
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- NOTE:
1. REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.

PAVEMENT CRACK SEALING – TYPE I DETAIL
N.T.S.



- NOTE:
1. REHEATING OR REMOVAL OF EXISTING SEALANT, REPAIRATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.
 2. CRACKS AND JOINTS THAT ARE IN EXCESS OF 1" IN WIDTH SHALL BE CLASSIFIED AS TYPE II CRACKS.

PAVEMENT CRACK SEALING – TYPE II DETAIL
N.T.S.

CRACKSEAL NOTE

1. FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
2. INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

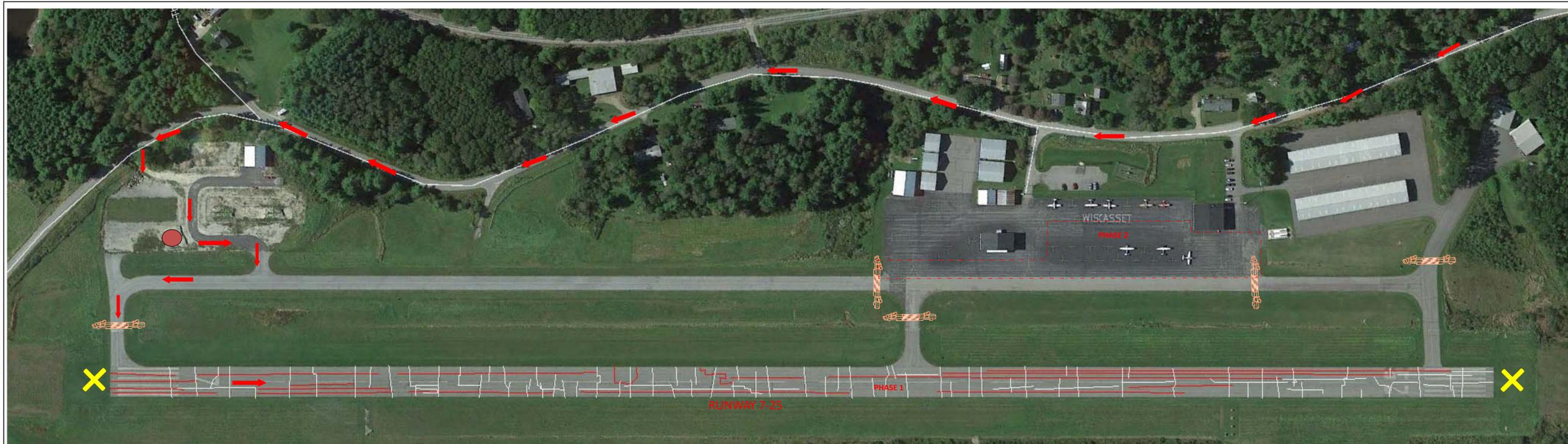
- ~ HAUL ROUTE (Red arrow)
- ~ PHASE 1 BARRICADES (Orange hatched)
- ~ PHASE 2 BARRICADES (Yellow hatched)
- ~ PHASE 3 BARRICADES (Green hatched)
- ~ PHASE 1 AREA (Hatched)
- ~ STAGING AREA (Red circle)
- ~ DIRECTION OF WORK PROGRESS (Green arrow)

PHASES 1, 2 & 3

1. PHASE 1 — BARRICADE AS NOTED—CRACKSEAL TAXILANES NEAR HANGARS AS NOTED
2. PHASE 2 — CLOSE SOUTHEASTERLY END OF APRON — SET PHASE 2 BARRICADES TO DETOUR AIRCRAFT TO NORTHWESTLY END OF APRON— CRACKSEAL SOUTHEASTERLY END OF APRON AND ADJACENT TAXIWAY
3. PHASE 3 — REOPEN SOUTHEASTERLY END OF APRON BY REMOVING SELECT BARRICADES AS DIRECTED BY THE RESIDENT ENGINEER— CLOSE NORTHWESTERLY END OF APRON— SET PHASE 3 BARRICADES TO DETOUR AIRCRAFT TO SOUTHEASTERLY END OF APRON— CRACKSEAL NORTHWESTERLY END OF APRON AND ADJACENT TAXIWAY ~ AVAILABILITY OF ALLOTTED PRODUCT PERMITTING

RUNWAY 14-32 IS TO REMAIN OPEN AT ALL TIMES

| | | | |
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| STATE OF MAINE DEPARTMENT OF TRANSPORTATION 020771.00 WIN | | TIMOTHY E. LESIEGE SUPERVISOR PE # 7821 MARCH 21, 2014 DATE | |
| | | PREL. NUMBER DESIGN-DETAIL NO. CHECKED-REV. WED. D. A. DESIGN-DETAIL NO. REVISIONS 1 REVISIONS 2 REVISIONS 3 FIELD CHANGES | DATE 2/27/14 3/20/14 3/23/14 |
| 2014 STATEWIDE AIRPORTCRACKSEAL PROJECT EASTERN SLOPES REGIONAL AIRPORT | | CONSTRUCTION SAFETY AND PHASING PLAN | |
| SHEET NUMBER | | 7 OF 8 | |



GENERAL NOTES

1. AIRPORT OPERATIONS COORDINATOR

THE AIRPORT MANAGER SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS, AND TO COORDINATE WITH AIRPORT USERS.

2. AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000.00 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND THEIR PERSONNEL, SUBCONTRACTORS AND VENDORS.

CONTRACTOR SHALL INSTRUCT SUPPLIERS, ON ACCESS PROCEDURES TO BE FOLLOWED.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER AND RESIDENT ENGINEER.

3. CONTRACTORS STAGING AREAS AND HAUL ROUTE

THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLAN FOR THEIR STAGING AREA. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL PROTECTION AND RESTORATION OF THE DESIGNATED AREA. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRASH PICK-UP GENERATED BY THE PROJECT. THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREA AND HAUL ROUTES SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT. STATE AND LOCAL HIGHWAY LOAD LIMITS APPLY ON ALL HAUL ROUTES. ALL HAUL ROUTES SHALL BE RESTORED TO THE ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.

HAUL ROUTES SHOWN ON THE PLANS HAVE BEEN APPROVED BY THE AIRPORT MANAGER. ANY DEVIATION MUST BE APPROVED BY AIRPORT MANAGER OR RESIDENT ENGINEER PRIOR TO UTILIZATION BY THE CONTRACTOR. IT SHOULD BE NOTED THAT DEVIATIONS IN THE HAUL ROUTE MAY NOT BE APPROVED.

4. AIRPORT OPERATIONS AND SAFETY REQUIREMENTS

NORMAL AIRPORT OPERATIONS WILL NOT BE CONDUCTED DURING CONSTRUCTION. THE AIRPORT WILL BE CLOSED TO ALL AIR AND GROUND MOVEMENT. THE CONTRACTORS WORK SHALL BE CARRIED ON IN SUCH A MANNER AS NOT TO INTERFERE WITH THE AIRCRAFT ON SITE. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO INSURE THE SAFETY OF THEIR OWN EQUIPMENT AND PERSONNEL. THE CONTRACTORS SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN.

THE CONTRACTOR SHALL HAVE A POWER SWEEPER ON THE SITE WHEN REQUIRED TO SWEEP PAVEMENTS IN WORK AREAS AND HAUL ROUTES AS DIRECTED BY THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.

5. NOTICES TO AIRMEN (NOTAMS)

NOTAMS SHALL BE ISSUED AS NEEDED BY THE AIRPORT MANAGER AFTER COORDINATING WITH THE CONTRACTOR. NOTAMS SHALL BE COORDINATED A MINIMUM OF 48 HOURS IN ADVANCE UNLESS OTHERWISE SPECIFIED.

6. AIRPORT RADIOS

THE CONTRACTORS SHALL PROVIDE A MINIMUM OF TWO AVIATION UNICOM RADIOS FOR USE DURING THE PROJECT. EACH RADIO SHALL BE CAPABLE OF TRANSMITTING ON FREQUENCY 122.80 MHZ AND BE EQUIPPED WITH A RECHARGING UNIT AND SPARE BATTERY PACK. RADIOS SHALL BE ICOM BRAND, MODEL IC-A23 OR APPROVED EQUAL. THE CONTRACTOR SHALL CONTINUOUSLY MONITOR LOCAL AIR TRAFFIC AT ALL TIMES WHEN THEIR FORCES ARE LOCATED WITHIN ANY AOA. RADIO COSTS ARE CONSIDERED INCIDENTAL TO THE PROJECT.

7. RESTROOMS

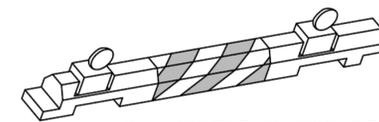
RESTROOM FACILITIES WILL BE PROVIDED AND MAINTAINED IN THE STAGING AREA BY THE CONTRACTOR. THE STAGING AREA WILL BE RESTORED BY THE CONTRACTOR, TO ORIGINAL CONDITION APPROVED BY THE OWNER.

8. CONTRACTOR VEHICLES

ALL CONTRACTORS VEHICLES SHALL HAVE THE COMPANY IDENTIFICATION PLAINLY VISIBLE ON BOTH SIDES OF THE VEHICLE IN ORDER TO IDENTIFY THE VEHICLE.

ALL VEHICLES THAT OPERATE DURING THE DAYLIGHT IN THE AIRCRAFT OPERATIONS AREA MUST BE PROVIDED WITH A FLASHING YELLOW BEACON OR FLAG ON A STAFF ATTACHED TO THE VEHICLE SO THAT THE FLAG CAN BE READILY VISIBLE. THIS INCLUDES PICKUP TRUCKS, BACKHOES, FRONT END LOADERS, SKID STEERS, PAVERS, GRADERS, ETC.

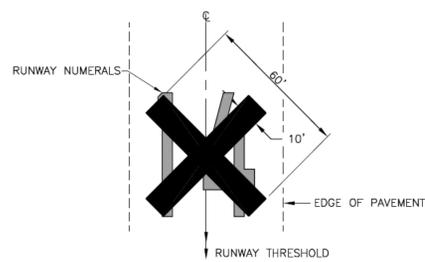
THE FLAG SHOULD BE AT LEAST A 3-FOOT SQUARE HAVING A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. THE STAFF MAY BE A 48" WOODEN DOWEL.



LOW-PROFILE BARRICADE
NOT TO SCALE

BARRICADE NOTES

- EACH BARRICADE IS MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE REFLECTIVE GRADE TAPE 6" WIDE. ALSO EACH WILL HAVE TWO 3-WAY APPROVED "SOLAR" RED LIGHTS TO MEET FAA STANDARDS.
- BARRICADES CAN BE FILLED WITH WATER OR SAND.
- DESIGNED TO FAA AC 150/5340-1H & 150/5370-2E SPECIFICATIONS AND PERFORMANCE STANDARDS
- MADE OF DURABLE POLYETHYLENE PLASTIC.
- DIMENSIONS: 6"(H) X 6"(W) X 6'(L)
- MAX BARRICADE SPACING 4' (END TO END)
- BARRICADES AND LIGHTS TO BE PROVIDED BY THE AIRPORT AND PLACED BY CONTRACTOR.
- EACH RUNWAY AND TAXIWAY SHALL BE BARRICADED PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.
- TEN (10) EACH LOW PROFILE BARRICADES ARE TO BE PROVIDED AND PLACED BY THE CONTRACTOR AND NO SEPARATE PAY ITEM FOR FURNISHING OR PLACING THE BARRICADES WILL BE MADE. THE BARRICADES WILL REMAIN THE PROPERTY OF THE AIRPORT UPON COMPLETION OF THE WORK AT THIS AIRPORT.



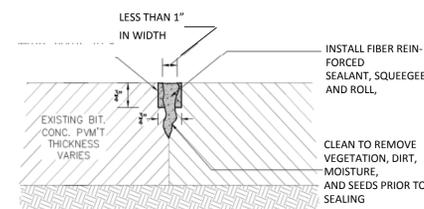
CLOSED RUNWAY MARKER
NOT TO SCALE

CLOSED RUNWAY MARKER NOTES

- CLOSED RUNWAY MARKERS (X'S) TO BE IN PLACE DURING CONSTRUCTION PERIOD AND/OR AS DIRECTED BY THE ENGINEER OR AIRPORT MANAGER. MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERAL OR ALONG THE EXTENDED RUNWAY CENTERLINE PER THE AIRPORT MANAGER OR THE RESIDENT ENGINEER.
- MARKERS SHALL BE HIGH STRENGTH, YELLOW VINYL, REUSABLE CLOSURE MARKERS WITH FACTORY INSTALLED BALLAST TUBES. MARKERS SHALL BE FAA APPROVED.
- CLOSED RUNWAY MARKERS ARE TO BE PROVIDED BY THE AIRPORT, PLACED BY CONTRACTOR AND NO SEPARATE PAYMENT FOR FURNISHING OR PLACING THE MARKERS WILL BE MADE.

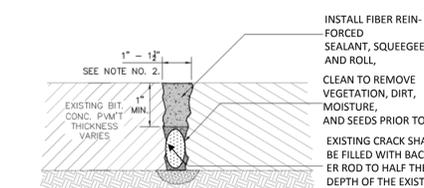
HAUL ROAD NOTES

- THE CONTRACTOR SHALL ONLY BE PERMITTED TO WORK IN THE DESIGNATED WORK AREAS SHOWN ON THIS SHEET AND ON THE DESIGNATED HAUL ROADS AND STAGING AREAS AS SHOWN ON THIS SHEET. VEHICLES SHALL TRAVEL TO AND FROM WORK AREAS ON ESTABLISHED ROADS AND PATHS AND SHALL NOT TRAVEL IN GRASSLANDS, WETLANDS, OR SENSITIVE RESOURCE AREAS.
- THE CONTRACTOR WILL BE PROHIBITED FROM CROSSING RUNWAYS, TAXIWAYS, OR APRONS WITH CONSTRUCTION EQUIPMENT UNLESS PRIOR APPROVAL FROM THE AIRPORT IS RECEIVED. AT APPROVED CROSSING, THE CONTRACTOR WILL BE RESPONSIBLE TO PROTECT THE EXISTING PAVEMENT FROM DAMAGE. ALL DAMAGED PAVEMENT AREAS WILL BE REQUIRED TO BE REPAIRED TO FAA STANDARDS, AND TO THE SATISFACTION OF THE OWNER, AT THE EXPENSE OF THE CONTRACTOR. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM GENERATING THE OPERATION.
- THE HAULING OF MATERIAL FROM STAGING AREAS TO THE WORK AREA(S) MUST BE ON THE DESIGNATED HAUL ROADS AT A SAFE DISTANCE FROM ALL AOA'S AS NOTED.
- THE CONTRACTOR MUST OBEY ALL POSTED SPEED LIMITS ON THE AIRFIELD. IF NO SPEED LIMIT IS POSTED, THE SPEED LIMIT SHALL BE 25 MPH.
- THE CONTRACTOR MUST ALWAYS MAINTAIN SECURITY ON THE AIRFIELD BY ENSURING THAT ALL GATES, IN USE BY THE CONTRACTOR, REMAIN LOCKED OR MONITORED AT ALL TIMES.
- AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY.
- STOP SIGNS (INCIDENTAL TO ITEMS GENERATING MATERIAL BEING HAULED) SHALL BE INSTALLED WHERE VEHICLES CROSS ACTIVE TAXIWAYS AND AS DIRECTED BY THE ENGINEER AND/OR AIRPORT MANAGER. CONTRACTOR IS REQUIRED TO HAVE A QUANTITY OF TWO (2) EACH, 2 FOOT HIGH (MAX) STOP SIGNS ON SITE FOR THIS PURPOSE.



NOTE:
1. REHEATING OR REMOVAL OF EXISTING SEALANT, PREPARATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.

PAVEMENT CRACK SEALING - TYPE I DETAIL
N.T.S.



NOTE:
1. REHEATING OR REMOVAL OF EXISTING SEALANT, REPAIRATION OF CRACK, AND CLEANING OF CRACK SHALL BE INCIDENTAL TO THE CRACK SEALING ITEM.
2. CRACKS AND JOINTS THAT ARE IN EXCESS OF 1" IN WIDTH SHALL BE CLASSIFIED AS TYPE II CRACKS.

PAVEMENT CRACK SEALING - TYPE II DETAIL
N.T.S.

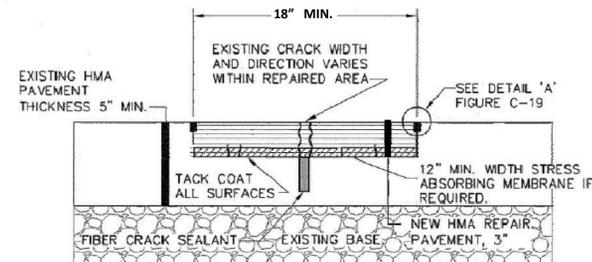
CRACKSEAL NOTE
1. FILL FLUSH OR 1/8" BELOW SURFACE. DO NOT OVERBAND.
2. INSTALL COMPRESSIBLE BACKER ROD IN CRACKS GREATER THAN 1" WIDE.

LEGEND

- X ~ CLOSED RUNWAY MARKER
- ~ HAUL ROUTE
- ~ BARRICADE
- ~ STAGING AREA

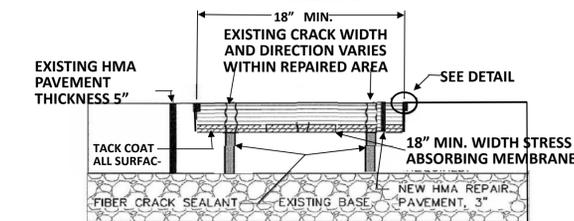
PHASE 1 & 2

- PHASE 1 — BARRICADE AND CLOSE RUNWAY 7-25 AS NOTED—START CRACK REPAIR ON RUNWAY 25 APPROACH END, WORK WESTERLY
 - EXPECTED CRACK REPAIRS DENOTED AS WHITE, EXPECTED CRACKSEAL DENOTED AS RED
 - PHASE 2 — PERFORM CRACK REPAIRS AND CRACKSEAL ON APRON AS NOTED BY RESIDENT ENGINEER
- NOTE: PHASE 1 & 2 CAN BE DONE CONCURRENTLY—BARRICADE PER RESIDENT ENGINEER



NOTE:
1. THIS REPAIR MAY BE USED WITHOUT AN OVERLAY AS SHOWN.
2. WHEN AN OVERLAY IS SCHEDULED DETAIL 'A' MAY BE ELIMINATED.

TYPE 2A CRACK REPAIR DETAIL - CRACKS GREATER THAN 1" WIDE IN PAVEMENTS 5" OR GREATER IN THICKNESS
NOT TO SCALE



NOTE:
1. THIS REPAIR MAY BE USED WITHOUT AN OVERLAY AS SHOWN.

TYPE 2B CRACK REPAIR DETAIL - CRACKS GREATER THAN 1" WIDE IN PAVEMENTS 5" OR GREATER IN THICKNESS AND PART OF A PRIOR CRACK REPAIR.
NOT TO SCALE

| | | | |
|---|--------|------------------|---------------|
| STATE OF MAINE DEPARTMENT OF TRANSPORTATION | | 020771.00 WIN | |
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| DATE | BY | DATE | DATE |
| 2/27/14 | T.E.L. | 3/20/14 | 3/21/14 |
| DESIGN-DRAWN | T.E.L. | D.A. | T.E.L. |
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| DESIGN-DETAILED | T.E.L. | D.A. | T.E.L. |
| DESIGN-DETAILED | T.E.L. | D.A. | T.E.L. |
| REVISION 1 | | | |
| REVISION 2 | | | |
| REVISION 3 | | | |
| REVISION 4 | | | |
| | | | FIELD CHANGES |
| 2014 STATEWIDE AIRPORT CRACKSEAL PROJECT WISCASSET AIRPORT CONSTRUCTION SAFETY AND PHASING PLAN | | | |
| SHEET NUMBER | | 8 OF 8 | |