



Paul R. LePage
GOVERNOR

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE 04333-0016

David Bernhardt
COMMISSIONER

February 7, 2014
Subject: **Fairfield - Waterville**
Federal Project No: STP-2031(700)
& STP-2032(000)
State WIN: 020317.00 & 020320.00
Amendment No. 2

Dear Sir/Ms:

Make the following change to the bid document:

In the Bid Book (page 120) "SPECIAL PROVISION, SECTION 403, HOT MIX ASPHALT" **ADD** the following note in pen and ink;
"20. The Contractor may place the specified HMA pavement course, not to exceed 2" inch compacted depth, over the full single travel lane width, for each production day. If this option is utilized the Contractor will be required to place a matching course of HMA over the adjacent section of travel lane before the end of the following calendar day. The Contractor will also be responsible for installing additional warning signage that clearly defines the centerline elevation differential hazard, as well as additional centerline delineation such as double RPM application, or temporary painted line. The Traffic Control Plan shall be amended to include this option and the additional requirements. All signs and traffic control devices will conform to Section 719.01, and Section 652, and will be installed prior to the work, at a maximum spacing of 0.50 mile for the entire length of effected roadway section. On roadways with two-way traffic, the Contractor will be required to place the specified course over the full width of the mainline traveled way being paved prior to opening the sections to weekend or holiday traffic. If this option is utilized, all additional signing, labor, traffic control devices, or incidentals will not be paid for directly, will be considered incidental to the appropriate 652 items."

The following questions have been received:

Question: Special provision Section 401 Plant Mix Pavements – general (Material Transfer vehicle – First paragraph states the mix for all leveling, base, binder & wearing courses shall be transferred to the paver by a Material Transfer Vehicle on main line paving. Special Provision Section 403 Hot Mix Asphalt – Note 19 requiring the use of a Material Transfer Vehicle is only required on the main line wearing course. Which special provision must be followed?

Response: The use of the MTV will be required on the main line wearing course. The shim, and shoulder wearing course paved in a separate operation from the main line will not require the MTV.



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Question: On page 120, Complementary Note #20 is not included in Special Provision Section 403 Hot Mix Asphalt. Is it the Departments intent to not allow the Contractor to pave over a single lane for each production day, and instead match all four adjacent lanes curb to curb for each day of paving?

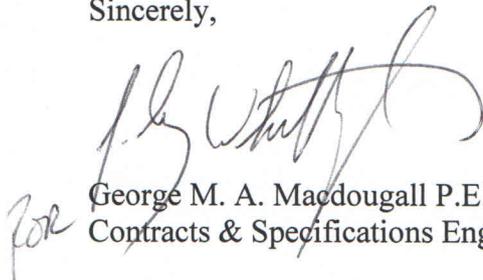
Response: Please see the above pen and ink change.

Question: Page 37 of the Spec book shows a typical detail for WIN 20317.00 from station 22+20 – 44+90, the detail shows removing 1.75” of Pavement Surface for the entire roadway width including the 4’ Center Island. Is it the Departments intent to remove the 4’ center island?

Response: No the Department’s intent is to keep the island as is, do not remove.

Consider this change and information prior to submitting your bid on February 12, 2014.

Sincerely,

A handwritten signature in black ink, appearing to read "G. Macdougall", written over a printed name and title.

George M. A. Macdougall P.E.
Contracts & Specifications Engineer