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GOVERNOR

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
16 STATE HOUSE STATION  
AUGUSTA, MAINE 04333-0016

David Bernhardt  
COMMISSIONER

October 17, 2014  
Subject: **Bridge replacement**  
State WIN: 016755.00  
Location: **Thomaston**  
**Amendment No. 1**

Dear Sir/Ms:

The following questions have been received:

**Question:** We do not feel there is adequate time allowed to perform required work in stream with permit restrictions of November 9<sup>th</sup> to March 15<sup>th</sup> per SP 105 and open the bridge by October 17<sup>th</sup>, 2015 per SP 107.

Specifically, the project bids on October 29<sup>th</sup>, the department may take up to 30 calendar days to award the project, bringing the commencement of submittals to November 27<sup>th</sup>. The Coastguard notification requires all documents submitted 35 calendar days prior to construction, bringing commencement of construction to January 2<sup>nd</sup>. This leaves 51 working days, 62 if every Saturday is worked, not including inclement weather in the middle of the winter season, to construct access to three pier locations, install pile in three pier locations, remove access, construct abutment cofferdams and install riprap. It is not a feasible assumption to believe this can occur.

Can either the time allowed to work in stream be extended or the date in which the bridge is required to be open to traffic be extended, or both, to dates that are achievable?

**Response:** Getting an in-stream work window extension at this site would be difficult due to essential fish habitat and could not be accomplished prior to bid opening. The Coast Guard process typically is completed before the maximum time allowed, but this is not within the Department's control. MaineDOT is not willing to extend the Bridge open to traffic requirement such that the contractor would be able to construct the new bridge substructure into the 2015-2016 in-stream work window.

**Question:** SP 104 indicates the department's intent is to have 1 utility pole moved along the West approach. The existing bridge cannot be removed within OSHA regulations of power line clearances without the existing power lines being moved away from the existing



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bridge. Is it the department's intent to have these lines de-energized during bridge demolition to enable the contractor to perform the work safely and within OSHA regulations? If not, how does the department intend on the bridge demolition to occur within OSHA regulations?

**Response:** CMP plans to de-energize the lines over the river and back-feed power from both ends during construction.

**Question:** The following are Items with their respective listed quantities, are they correct, in my opinion they appear reversed?

- Item 203.2333, refers to contaminated soil between Sta. 27+50 and 29+50, this quantity is listed as 20 tons.
- Item 203.2312, refers to dredge material – which is the mud inside the pipe piles & riprap toe, this quantity is listed as 460 tons.

**Response:**

- The quantity for Item No. 203.2318 – Disposal of Special Waste (Incorrectly listed as Item No. 203.2312 in the RFI) is correct at 460 T. The quantity for dredge material is high do to the fact that there is a lot of riprap below the water line and there will be excavation associated with the placement of it.
- The bid quantity for Item No. 203.2333 – Disposal/Treatment of Special Excavation will be amended to 40 T. The station limits referenced in the RFI question identify areas of contamination and will be classified into Groups 1-3 per the Special Provision. Only Group 3 and any surplus of Group 2 that cannot be reused in accordance with the Special Provision will be paid for under this pay item.

**Question:**

- Section 504 Rock Anchors  
504.5 b) call for threadbar to be hot dipped galvanized.
- In the plans, Sheet 48, Rock Anchor Note # 8 “Rock Anchors shall have double corrosion protection.
- DCP is PVC sheathing & cement grout covering pre-coated threadbar.

If the threadbar is galvanized why have the PVC cap filled with “grease” and the steel duct filled with “grease” as shown in the plans, Sheet 48, Detail “A”?

**Response:** As a belt and suspenders approach we prefer to have the extra corrosion protection.

**Question:** Please confirm that the threadbar is HDG?

**Response:** The threadbar is galvanized.

**Question:** We cannot find any reference in either the plans or specifications of which buildings are to be removed under pay item 202.08. On pas projects we have seen a description of each building with a station and offset reference to be removed in SP 202 Removal of structures and Obstructions and the buildings required to be removed are shown on the plans as such. Please provide additional information for removal of buildings to be removed.

**Response:** One Building is about 52'x60' and is located at Station 28+50, 23' Lt. The other building is approximately 28' x 68' and is located at Station 28+50, 95' Lt.

**Question:** Additional information is required to be able to get pricing for transporting and disposal of underground storage tank Item 202.26. Qualified vendors require the quantity, size and contents of each tank to be transported and disposed of in order to provide pricing. Also, how is removal of underground storage tanks to be paid?

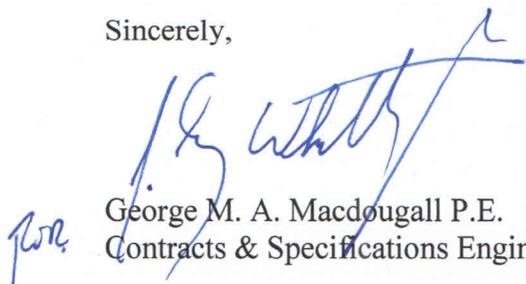
**Response:** MDEP records indicate two underground storage tanks (USTs) are present. One is 4000 gallon fiberglass UST for unleaded gasoline. The other is a 4000 gallon fiberglass UST with two chambers for regular and unleaded gasoline. It is assumed the USTs are mostly empty but there may be residual product that will need to be disposed of. Payment for removal is incidental to Item 202.26 L.S.

**Question:** Are there any analytical test results for the material to be disposed of as special waste? Disposal facilities require test results before they will provide pricing for disposal?

**Response:** No test results are available. MaineDOT was not able to get soil samples. The SP 203 for Contaminated Soil and Groundwater Management under Handling and Disposition of Soil materials states that the contractor is responsible for all additional testing and permits required for disposal of the contaminated soil

Consider these changes and information prior to submitting your bid on **October 29**, 2014.

Sincerely,

  
George M. A. Macdougall P.E.  
Contracts & Specifications Engineer