



Paul R. LePage
GOVERNOR

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE 04333-0016

August 23, 2013
Subject: **Puddle Dock Bridge**
State WIN: 20914.00
Location: Albion
Amendment No. 1

David Bernhardt
COMMISSIONER

Dear Sir/Ms:

Make the following changes to the Bid Document:

In the Bid Book (page 9), "NOTICE TO CONTRACTORS" **CHANGE** the bid opening date in the first sentence from "August 28, 2013" to read "**September 4, 2013**". Make this change in pen and ink.

In the Bid Book, (pages 25-26), **REMOVE** "NOTES", (2 pages total) and **REPLACE** with the attached new "NOTES" (2 pages total).

In the Bid Book, before page 27 **ADD** the attached "Lumber Schedule and Hardware Schedule" 1 page.

In the Bid Book, (page 34), "SPECIAL PROVISION SECTION 528, STRUCTURAL TIMBER DECK" **ADD** the following under "Preservative Treatment." "**All timber shall be treated with Chromated Copper Arsenate, Type C conforming to AWP Standard P 5 to a minimum retention of 0.60 PCF.**" Make this change in pen and ink.

The following questions have been received:

Question: Clarify the lumber surfacing requirements for the floor beams on the lumber schedule table located on the drawings with in the bid book? Is it S1S1E?

Response: The lumber surfacing requirements for the floor beams is S1S1E.

Question: How many TimberLOK screws will be required for each running board and at what spacing?

Response: TimberLOK screws are to be set in pairs with 2ft. spacing in each running board.

Question: Please confirm treatment requirements. Spec indicates AASHTO M 133 but does not indicate treatment level (i.e. 0.4 #CCA or 2.5@CCA)?



PRINTED ON RECYCLED PAPER

Response: All timber shall be treated with Chromated Copper Arsenate, Type C conforming to AWPA Standard P 5 to a minimum retention of 0.60 PCF in accordance with AASHTO M 133.

Question: Please confirm no MDOT facilities will be required?

Response: No field trailer will be required.

Question: Please confirm spacing on the 5/8" curb to deck bolts. Additionally there is a conflict between the hardware schedule and the transverse section re: bolt lengths. I assume the hardware schedule to be correct on this?

Response: There shall be one curb to deck bolt at every 1ft scupper block and one curb to deck bolt at every curb end (i.e. two bolts at 2ft scupper blocks). The Hardware Schedule is correct.

Question: Please provide a nailing detail for the Floorbeam to the Nailing strip. I assume two nails but not sure?

Response: There shall be two nails per Floorbeam per Nailing Strip intersection.

Question: Please provide a screw pattern/spacing detail for the TimberLOK fasteners at the Running Boards. Additionally, the Lumber Schedule calls out 1.5x11.25 for the Running Boards but note 3 of the materials section of the cover page indicates Running Boards to be rough sawn? This would appear to be in conflict?

Response: The TimberLOK fasteners are to be set in pairs with 2ft spacing on each running board. The Running Boards are to be S2E.

Question: On plan sheet 2 of 2 or solicitation pdf page 26 of 68, the lumber schedule is not clear. Can you provide a clear schedule?

Response: A clear Lumber Schedule has been provided.

Question: The net sizes shown on the table are not standard S4S sizes. Are you looking for standard S4S sizes or custom surfacing to the net size as shown on the Lumber Schedule?

Response: Timber dimensions are intended to be descriptive, but not restrictive, and are used to indicate the characteristics of products that are satisfactory. The Department will

consider all alternate timber dimensions submitted by the Contractor, but is not bound to accept any which, in its opinion, is not in the Department's best interest. The Department will accept alternate timber dimensions which are determined by the Department to be of equal value in all material respects to the those specified. The evaluation of and determination as to equality of the product offered shall be the responsibility of the Department and will be based on information furnished by the Contractor, as well as information reasonably available to the purchasing activity.

Question: Are any of the existing timbers treated? If so with what products?

Response: Yes, there are timbers treated with creosoted timbers and/or CCA

Question: If the timbers are treated are there any disposal requirements?

Response: The Contractor shall contain all demolition debris and shall not allow it to discharge to any resource. All materials removed shall become the property of the Contractor. The Contractor shall dispose of debris in accordance with the Maine Solid Waste Law, Title 38 M.R.S.A., Section 1301 et. seq. Containment and disposal of demolition debris shall be addressed in the Contractor's SEWPCP. The Contractor shall comply with all federal, State, and local laws concerning the handling, storage, treatment, and disposal of uncontrolled petroleum or hazardous material. The existing wearing surface is expected to include timbers treated with creosote and/or CCA.

Question: Are there any requirements for the lead paint during demolition. The existing bolts need to be removed thru the top flange of the bridge beams.

Response: No, there are no lead paints requirements.

Question: Does any of the lumber need to be pressure treated? The special provisions are not clear as to what is required.

Response: All timber shall be treated with Chromated Copper Arsenate, Type C conforming to AWWA Standard P 5 to a minimum retention of 0.60 PCF in accordance with AASHTO M 133.

Question: In large letters and clear handwriting please provide the entire lumber schedule information; neither the download nor page 26 is clear.

Response: A clear Lumber Schedule has been provided. Consider these changes and information prior to submitting your bid on September 4, 2013.

Sincerely,

George M. A. Macdougall, P.E.

GM
George M. A. Macdougall P.E.
Contracts & Specifications Engineer

SPECIFICATIONS

DESIGN: Load and Resistance Factor Design Per AASHTO LRFD Bridge Design Specifications, Fifth Edition 2010.

TRAFFIC DATA

Current (2012) AADT	604
Future (2032) AADT	906
% Heavy Trucks (AADT)	8
% Heavy Trucks (DHY)	8

MAINTENANCE OF TRAFFIC

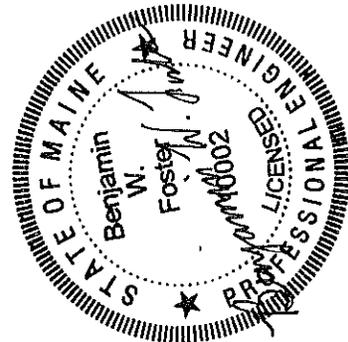
Bridge will be closed during construction.

SCOPE OF WORK

1. Remove existing timber deck, running boards, and curbs.
2. Install new timber deck, running boards, and curbs.

MATERIALS

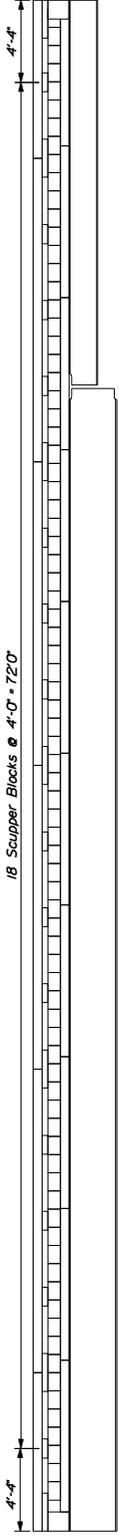
1. All timber to be construction grade Douglas Fir or Southern Yellow Pine
2. Timber to meet required dimensions according to plan
3. All hardware to meet specifications defined in The Contract



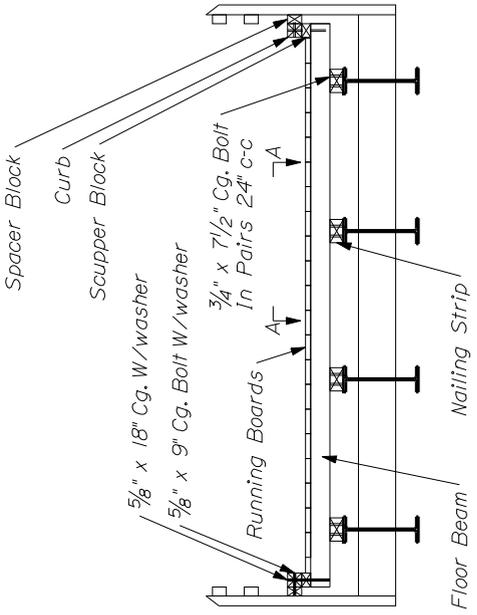
GENERAL CONSTRUCTION NOTES

1. Project information referred to below may be accessed at the following MaineDOT web address: <http://www.maine.gov/mdot/contractors/>.
2. Running boards to be attached to deck with TimberLOK Heavy Duty Wood Screw, placed parallel with traffic, and beveled ends on bridge approaches. TimberLOK Heavy Duty Wood Screws to be countersunk to provide a flush surface on running boards. TimberLOK screws to be set in pairs with 2ft spacing in each Running Board.
3. 2ft long scupper blocks to be placed at end of curbs and at curb splices. There shall be one Curb to Deck bolt at each curb piece end and each 1ft scupper block.
4. New nailing strips to be connected using existing holes in top flange of girder. Two nails to be used for every Floorbeam at each Nailing Strip intersection.
5. 2 layers of 30' roofing felt to be placed between nailing strip and floor beam with 2" overhang.
6. Spacer Blocks to be placed at every rail post.
7. Timber dimensions are intended to be descriptive, but not restrictive, and are used to indicate the characteristics of products that are satisfactory. The Department will consider all alternate timber dimensions submitted by the Contractor, but is not bound to accept any which, in its opinion, is not in the Department's best interest. The Department will accept alternate timber dimensions which are determined by the Department to be of equal value in all material respects to the those specified. The evaluation of and determination as to equality of the product offered shall be the responsibility of the Department and will be based on information furnished by the Contractor, as well as information reasonably available to the purchasing activity.
8. The Contractor shall contain all demolition debris and shall not allow it to discharge to any resource. All materials removed shall become the property of the Contractor. The Contractor shall dispose of debris in accordance with the Maine Solid Waste Law, Title 38 M.R.S.A., Section 1301 et. seq. Containment and disposal of demolition debris shall be addressed in the Contractor's SEM/PCP. The Contractor shall comply with all Federal, State, and local laws concerning the handling, storage, treatment, and disposal of uncontrolled petroleum or hazardous material. The existing deck and wearing surface is expected to include timbers treated with creosote and/or CCA.
9. No field trailer will be required.

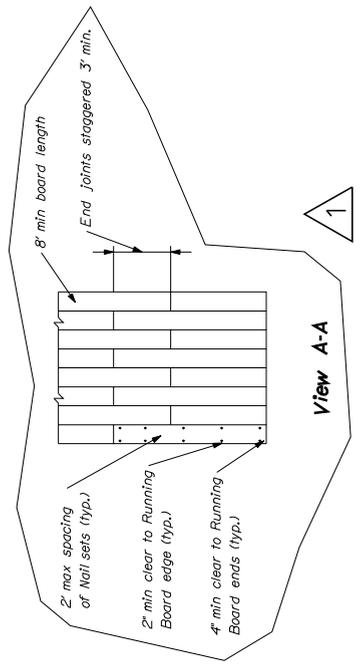
STATE OF MAINE DEPARTMENT OF TRANSPORTATION	ALBION KENNEBEC COUNTY	SHEET NUMBER 1
PUDDLE DOCK BRIDGE	NOTES	8/22/13 PK OF 2



Elevation



Transverse Section



Lumber Schedule		
Dress	Req'd Dim	Location
S1E	7 3/4" x 1 1/2"	Floor Beam
S4S	5 1/2" x 9 1/2"	Nailing Strip
S4S	5 3/4" x 5 3/4"	Curbs Interior
S4S	5 3/4" x 5 3/4"	Curbs Ends
S4S	3 5/8" x 5 3/4"	Scupper Blocks
S4S	3 5/8" x 5 3/4"	Scupper Blocks
S4S	3 5/8" x 5 3/4"	Curb to Rail Spacers
S4S	5 1/2" x 9 1/2"	End Nailing Strip
S2E	1 1/2" x 1 1/4"	Running Boards

Hardware Schedule		
Description	Size	Location
Nails	12"	Floor to Nailing Strip
Carriage Bolt	3/4" φ	Nailer to Flange
Carriage Bolt	5/8" φ	Curb to Deck
Carriage Bolt	5/8" φ	Curb to Post
TimberLOK	3/8" φ	Running Board to Floor
Flat Washer	5/8" φ	5/8" x 12" Bolt
Lock Washer	5/8" ID φ	All 5/8" Bolts
Lock Washer	3/4" ID φ	All 3/4" Bolt

<i>Lumber Schedule</i>			
<i>Dress</i>	<i>Req'd Dim</i>	<i>Length</i>	<i>Location</i>
<i>SISIE</i>	<i>7³/₄" x 11¹/₂"</i>	<i>19'-0"</i>	<i>Floor Beam</i>
<i>S4S</i>	<i>5¹/₂" x 9¹/₂"</i>	<i>8'-0"</i>	<i>Nailing Strip</i>
<i>S4S</i>	<i>5⁵/₈" x 5⁵/₈"</i>	<i>16'-0"</i>	<i>Curbs Interior</i>
<i>S4S</i>	<i>5⁵/₈" x 5⁵/₈"</i>	<i>8'-4"</i>	<i>Curbs Ends</i>
<i>S4S</i>	<i>3⁵/₈" x 5⁵/₈"</i>	<i>2'-0"</i>	<i>Scupper Blocks</i>
<i>S4S</i>	<i>3⁵/₈" x 5⁵/₈"</i>	<i>1'-0"</i>	<i>Scupper Blocks</i>
<i>S4S</i>	<i>3⁵/₈" x 5⁵/₈"</i>	<i>0'-6"</i>	<i>Curb to Rail Spacers</i>
<i>S4S</i>	<i>5¹/₂" x 9¹/₂"</i>	<i>10'-0"</i>	<i>End Nailing Strip</i>
<i>S2E</i>	<i>1¹/₂" x 11¹/₄"</i>	<i>8'-0"</i>	<i>Running Boards</i>

<i>Hardware Schedule</i>			
<i>Discription</i>	<i>Size</i>	<i>Length</i>	<i>Location</i>
<i>Nails</i>	<i>Br. Spikes</i>	<i>12"</i>	<i>Floor to Nailing Strip</i>
<i>Carriage Bolt</i>	<i>3⁴/₄" ϕ</i>	<i>7¹/₂"</i>	<i>Nailer to Flange</i>
<i>Carriage Bolt</i>	<i>5⁸/₈" ϕ</i>	<i>18"</i>	<i>Curb to Deck</i>
<i>Carriage Bolt</i>	<i>5⁸/₈" ϕ</i>	<i>10¹/₂"</i>	<i>Curb to Post</i>
<i>TimberLOK</i>	<i>3⁸/₈" ϕ</i>	<i>6"</i>	<i>Running Board to Floor</i>
<i>Flat Washer</i>	<i>5⁸/₈" ϕ</i>	<i>N/A</i>	<i>5⁸/₈" x 19" Bolt</i>
<i>Lock Washer</i>	<i>5⁸/₈" ID ϕ</i>	<i>N/A</i>	<i>All 5⁸/₈" Bolts</i>
<i>Lock Washer</i>	<i>3⁴/₄" ID ϕ</i>	<i>N/A</i>	<i>All 3⁴/₄" Bolt</i>