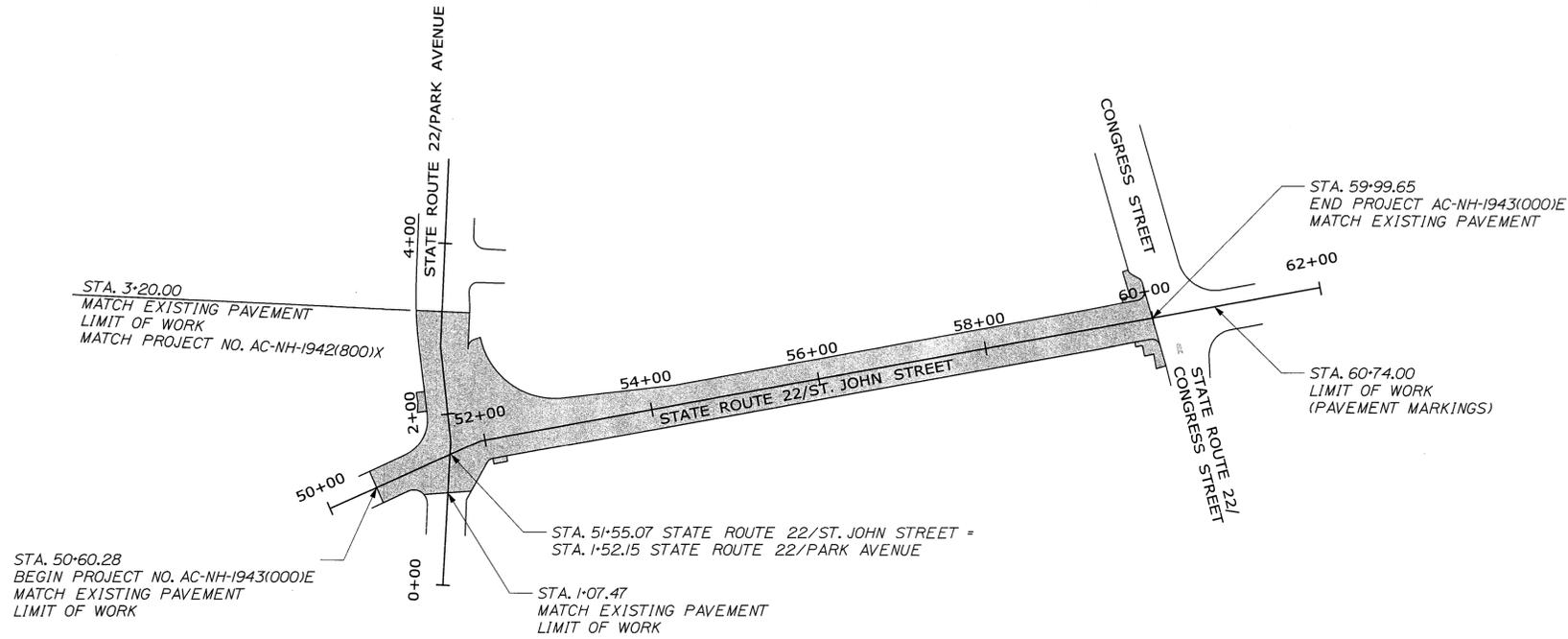


# STATE OF MAINE DEPARTMENT OF TRANSPORTATION



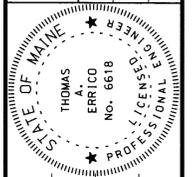
## PORTLAND CUMBERLAND ST. JOHN STREET AC-NH-1943(000)E

PROJECT LENGTH : 0.191 MILES



INDEX OF SHEETS	
Description	Sheet No.
Title Sheet .....	1
Detail Sheet .....	2
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Plans .....	4-5
Signal & Signing Plans .....	6-7

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	APPROVED	DATE
COMMISSIONER: <i>David R. Swaney</i>	<i>[Signature]</i>	8/3/12
CHIEF ENGINEER	<i>[Signature]</i>	8/2/12



SIGNATURE	P.E. NUMBER	DATE
<i>[Signature]</i>	6618	07/25/12

PROJECT INFORMATION
PROGRAM
PROJECT MANAGER
DESIGNER
CONSULTANT
PROJECT RESIDENT
CONTRACTOR
PROJECT COMPLETION DATE

PORTLAND ST. JOHN STREET
TITLE SHEET

SHEET NUMBER
1
OF 7

### TRAFFIC DATA

Current (2012) AADT .....	15020
Future (2032) AADT .....	16520
DHV - % of AADT .....	10%
Design Hour Volume .....	1652
% Heavy Trucks (AADT) .....	3%
% Heavy Trucks (DHV) .....	3%
Directional Distribution (DHV) ..	79%
18 kip Equivalent P 2.0 .....	193
18 kip Equivalent P 2.5 .....	184
Design Speed (mph) .....	30
Functional Class: .....	PRINCIPAL ARTERIAL

### LAYOUT SCALE



**TYLIN INTERNATIONAL**

**PROJECT LOCATION:** State Route 22

**PROGRAM AREA:** Traffic Program

**OUTLINE OF WORK:** Variable Depth Pavement Rehabilitation and Traffic and Pedestrian Improvements

WIN 19430.00 AC-NH-1943(000)E

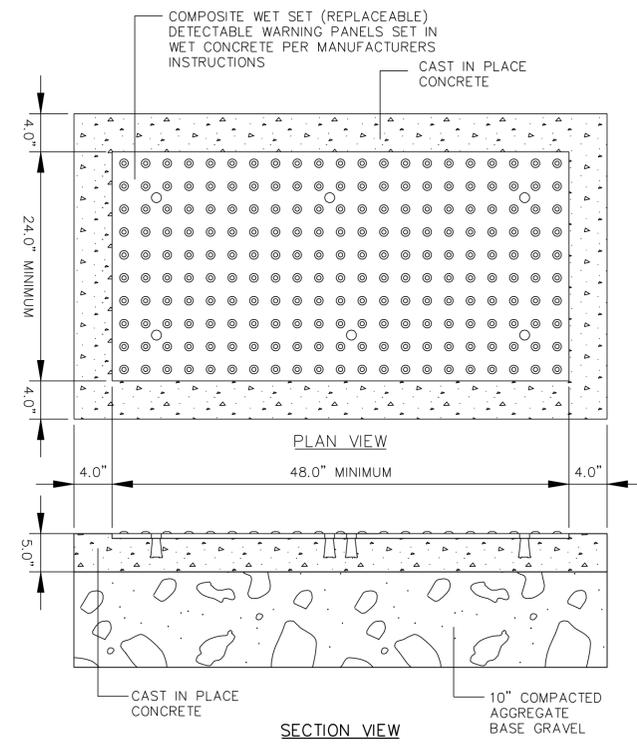
Date: 7/26/2012

Username: pmcclure

Division: HIGHWAY

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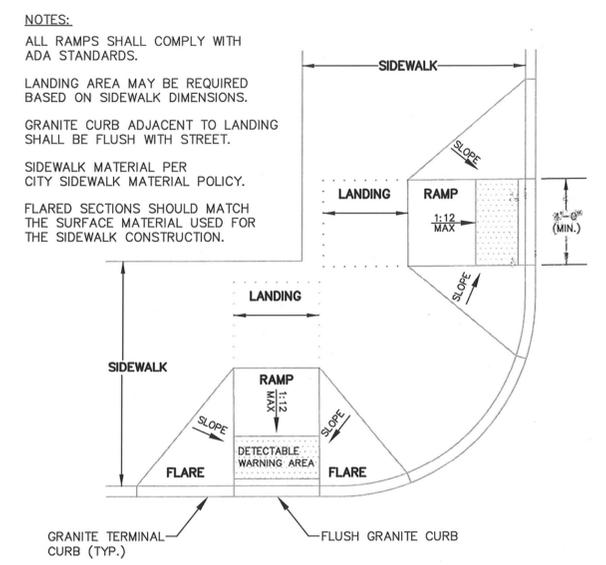
- NOTES:
1. COMPOSITE WET SET (REPLACEABLE) DETECTABLE WARNING PANELS SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC. (WWW.ADATILE.COM), OR APPROVED EQUAL.
  2. CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MAINE D.O.T. CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4,000 PSI. THE CONCRETE SHALL BE SEALED PRIOR TO SETTING PANELS. THE EXPOSED CONCRETE BORDER SHALL RECEIVE A GROOVED EDGE BETWEEN THE PANEL AND CONCRETE, ALONG WITH A UNIFORM BROOM FINISH PERPENDICULAR TO THE FLOW OF PEDESTRIAN TRAFFIC.
  3. TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. TRUNCATED DOME BRICKS AND GRANITE PAVERS ARE NOT ALLOWED.
  4. FOR ALL DETECTABLE WARNING PANELS (EXCEPT AS SPECIFIED IN FIGURE 1-7A AND TECHNICAL MANUAL SECTION 1.8.4.), FEDERAL YELLOW COLORED (#33538) PANELS SHALL BE USED. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
  5. SIZE: THE DETECTABLE WARNING PANEL(S) SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
  6. ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.



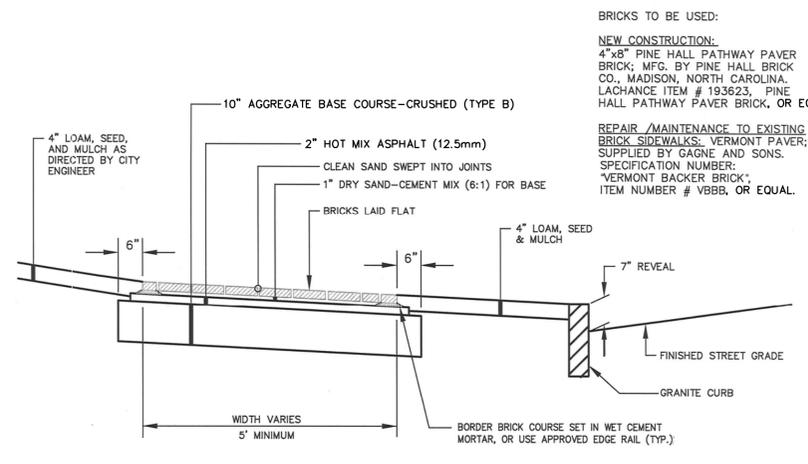
SIDEWALK RAMP DETECTABLE WARNING PANEL  
NOT TO SCALE



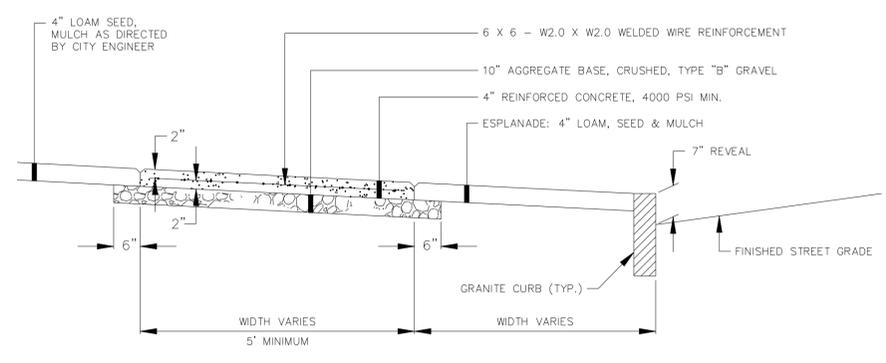
PHOTO OF DWT INSTALLATION  
ON BRICK SIDEWALK



PREFERRED SIDEWALK RAMP AT INTERSECTION  
NOT TO SCALE



BRICK SIDEWALK WITH BITUMINOUS BASE  
NOT TO SCALE

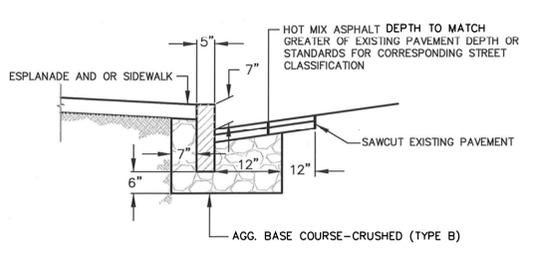
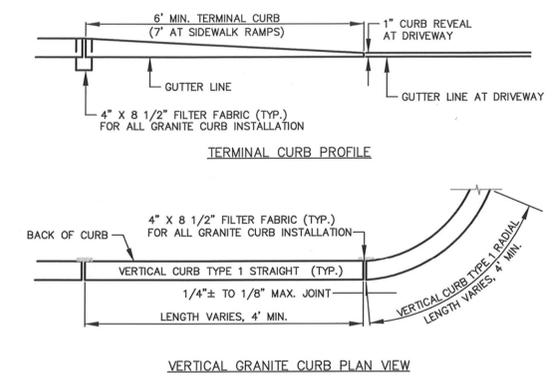


REINFORCED CONCRETE SIDEWALK  
NOT TO SCALE

BRICKS TO BE USED:

**NEW CONSTRUCTION:**  
4"x8" PINE HALL PATHWAY PAVER BRICK; MFG. BY PINE HALL BRICK CO., MADISON, NORTH CAROLINA. LACHANCE ITEM # 193623, PINE HALL PATHWAY PAVER BRICK, OR EQUAL.

**REPAIR /MAINTENANCE TO EXISTING BRICK SIDEWALKS:** VERMONT PAVER; SUPPLIED BY GAGNE AND SONS. SPECIFICATION NUMBER: "VERMONT BACKER BRICK"; ITEM NUMBER # VBBB, OR EQUAL.



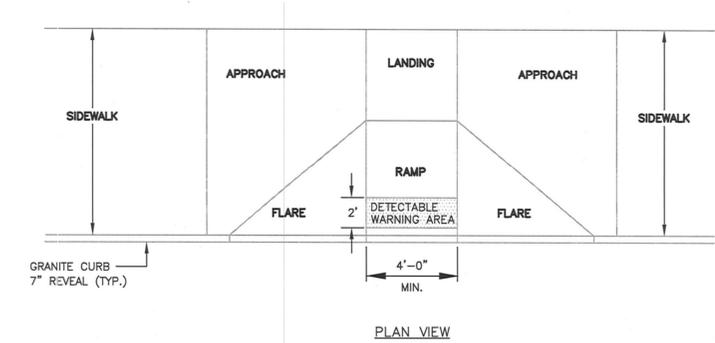
VERTICAL GRANITE CURB INSTALLATION IN EXISTING STREETS  
NOT TO SCALE

NOTES:

ALL RAMPS SHALL COMPLY WITH ADA STANDARDS.

GRANITE CURB ADJACENT TO RAMP SHALL BE FLUSH WITH STREET.

DESIGN ELEMENT	SLOPE IN DIRECTION OF TRAVEL	CROSS SLOPE
APPROACH	8.33% MAXIMUM	2%
LANDING	2%	2%
RAMP	8.33% MAXIMUM	MATCH STREET GRADE
FLARE	10% MAX. AT CURB FACE	-
SIDEWALK	MATCH STREET GRADE	2%



PERPENDICULAR ADA RAMP LAYOUT FOR WIDE SIDEWALK WITH NO ESPLANADE  
NOT TO SCALE

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
AC-NH-1943(00)E  
WIN 19430.00

THOMAS A. ERICO  
REGISTERED PROFESSIONAL ENGINEER  
NO. 6618  
02-06-2013  
DATE

PROJ. MANAGER T. Errico  
DESIGN-DETAILED TAE  
CHECKED-REVIEWED KEK  
DESIGN-DETAILED TAE  
REVISIONS 1  
REVISIONS 2  
REVISIONS 3  
REVISIONS 4  
FIELD CHANGES

PORTLAND  
ST. JOHN STREET  
DETAILS

SHEET NUMBER  
2  
OF 7

TYLIN INTERNATIONAL

Filename: ... \highway\msta\002\_Det\_001.dgn  
Division: HIGHWAY  
Username: pmcclure  
Date: 2/6/2013

## ESTIMATED QUANTITIES

ITEM NO.	ITEM	UNIT	QUANTITY
203.20	COMMON EXCAVATION	CY	1200
304.09	AGGREGATE BASE COURSE - CRUSHED	CY	30
403.210	HOT MIX ASPHALT - 9.5 MM NOMINAL MAX. SIZE, SURFACE	TON	520
403.213	HOT MIX ASPHALT - 12.5 MM NOMINAL MAX. SIZE, BASE	TON	1550
409.15	BITUMINOUS TACK COAT, APPLIED	G	160
411.09	UNTREATED AGGREGATE SURFACE COURSE	CY	420
603.179	18" CULVERT PIPE OPTION III	LF	47
604.071	CATCH BASIN TYPE A1-P	EA	3
604.16	ALTERING CATCH BASIN TO MANHOLE	EA	3
608.08	REINFORCED CONCRETE SIDEWALK	SY	41
608.15	BRICK SIDEWALK WITH BITUMINOUS BASE	SY	170
608.26	CURB RAMP DETECTABLE WARNING FIELD	SF	72
609.11	VERTICAL CURB TYPE 1	LF	200
609.12	VERTICAL CURB TYPE 1- CIRCULAR	LF	96
609.234	TERMINAL CURB TYPE 1- 4'	EA	9
609.2341	TERMINAL CURB TYPE 1- 4' CIRCULAR	EA	6
609.237	TERMINAL CURB TYPE 1- 7'	EA	1
609.400	RESET CURB TYPE 5	LF	10
609.441	CURBING REMOVED AND STACKED	LF	556
615.07	LOAM	CY	26
618.1301	SEEDING METHOD NUMBER 1, PLAN QUANTITY	Unit	3
619.1201	MULCH, PLAN QUANTITY	Unit	3
627.733	4" WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE	LF	3400
627.744	6" WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE	LF	200
627.75	WHITE OR YELLOW PAVEMENT AND CURB MARKING	SF	2100
627.76	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LS	1
627.77	REMOVE EXISTING PAVEMENT MARKING	SF	540
629.05	HAND LABOR - STRAIGHT TIME	HR	10
631.10	AIR COMPRESSOR (INCLUDING OPERATOR)	HR	10
631.11	AIR TOOL (INCLUDING OPERATOR)	HR	10
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	HR	10
631.14	GRADER (INC. OPERATOR)	HR	20
631.172	TRUCK, LARGE (INCLUDING OPERATOR)	HR	10
639.19	FIELD OFFICE TYPE B	EA	1
645.106	DEMOUNT REGULATORY, WARNING, CONFIRMATION, AND ROUTE MARKER ASSEMBLY SIGN	EA	10
645.1061	RELOCATE EXISTING SIGN ASSEMBLY AND POST	EA	2
645.108	DEMOUNT POLE	EA	5
645.292	REGULATORY, WARNING, CONFIRMATION AND ROUTE MARKER ASSEMBLY SIGNS TYPE II	SF	58
652.33	DRUM	EA	50
652.34	CONE	EA	50
652.35	CONSTRUCTION SIGNS	SF	600
652.36	MAINTENANCE OF TRAFFIC CONTROL DEVICES	CD	125
652.38	FLAGGERS	HR	2350
656.75	TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL	LS	1

**Bid Alternate 1: Loop Detection**

643.8	TRAFFIC SIGNAL AT: ST. JOHN AND PARK AVE	LS	1
643.8	TRAFFIC SIGNAL AT: ST. JOHN AND CONGRESS ST	LS	1
659.1	MOBILIZATION	LS	1

**Bid Alternate 2: Video Detection**

643.8	TRAFFIC SIGNAL AT: ST. JOHN AND PARK AVE	LS	1
643.8	TRAFFIC SIGNAL AT: ST. JOHN AND CONGRESS ST	LS	1
659.1	MOBILIZATION	LS	1

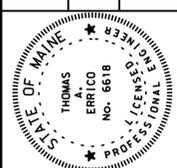
## GENERAL NOTES

- ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MAINEDOT) AND CITY OF PORTLAND REQUIREMENTS.
- THE BEGIN PROJECT LIMITS MATCH LIMITS OF WORK FOR WIN 19428.00 PARK AVENUE, WHICH IS EXPECTED TO BE CONSTRUCTED CONCURRENTLY.
- THE UTILITIES INVOLVED IN THIS CONTRACT ARE AS FOLLOWS: CENTRAL MAINE POWER COMPANY, FAIRPOINT COMMUNICATIONS, UNITIL, CITY OF PORTLAND (EMERGENCY COMMUNICATIONS, SEWER) AND PORTLAND WATER DISTRICT.
- MAINTENANCE OF TRAFFIC PLANS ARE TO BE APPROVED BY MAINEDOT WITH COOPERATION OF THE CITY PRIOR TO CONSTRUCTION. CITY OF PORTLAND DEPARTMENT OF FACILITIES HAS REQUIRED THAT CONSTRUCTION NOT BE PERFORMED DURING 2013 UNTIL THE SEA DOGS SEASON IS COMPLETED. CONTRACTOR SHALL ENSURE THAT SIDEWALK, CROSS WALKS, AND PARKING LANES ARE OPEN AND AVAILABLE FOR EVENTS. IF CONSTRUCTION CONTINUES INTO SPRING 2014, WORK CAN BE PERFORMED ONLY ON NON-GAME DAYS OR AWAY GAME DAYS, AND MUST BE FULLY COMPLETE ON OR BEFORE THURSDAY, MAY 22, 2014.
- ALL UTILITY FACILITIES SHALL BE ADJUSTED BY THE RESPECTIVE UTILITIES UNLESS OTHERWISE NOTED.
- THE WORK CONSISTS OF 6" HOT MIX ASPHALT OVER VARIABLE DEPTH UNTREATED AGGREGATE SURFACE COURSE GRAVEL FROM THE BEGINNING TO END OF PROJECT ALONG ST. JOHN STREET, AND WITHIN THE LIMITS OF WORK ALONG PARK AVENUE.
- WHERE PAVEMENT UNDER THIS CONTRACT JOINS AN EXISTING PAVEMENT, THE EXISTING PAVEMENT SHALL BE SAWCUT ALONG A SMOOTH LINE TO A NEAT, EVEN, VERTICAL JOINT AS DIRECTED BY THE RESIDENT. BROKEN OR RAVELED EDGES WILL NOT BE PERMITTED. ALL WORK NECESSARY FOR THE PREPARATION OF THIS JOINT WILL BE CONSIDERED INCIDENTAL TO THE RELATED CONTRACT ITEMS.
- PEDESTRIAN RAMPS AND CURB BUMPOUTS: RAMPS SHALL BE CONSTRUCTED CONSISTENT WITH EXISTING MATERIALS AND CITY OF PORTLAND SIDEWALK MATERIALS POLICY.
- THE AERIAL BASE MAP WAS PROVIDED BY THE CITY OF PORTLAND AND MAY NOT REFLECT CURRENT SIDEWALK MATERIALS IN SOME AREAS. SIDEWALK MATERIALS ARE AS FOLLOWS:  
CONCRETE: SE AND SW CORNERS OF PARK AVENUE INTERSECTION.  
BRICK: NE CORNER OF PARK AVENUE INTERSECTION, ALL RAMPS AT CONGRESS STREET INTERSECTION.
- ALL CURB SHALL BE VERTICAL TYPE 1 GRANITE, EXCEPT TYPE 5 CURB AT THE NORTHBOUND CONGRESS STREET CHANNELIZATION ISLAND MODIFICATION.
- GRANITE CURB THAT IS REMOVED AS A RESULT OF THIS PROJECT IS THE PROPERTY OF THE CITY. CURB SHALL BE REMOVED AND STACKED IN ACCORDANCE WITH SPECIAL PROVISION 609.
- PEDESTRIAN SIGNALS LOCATED WEST OF THE RAILROAD TRESTLE AT THE PARK AVENUE INTERSECTION WILL BE INSTALLED AS PART OF ANOTHER PROJECT.
- ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN WASTE AREAS APPROVED BY THE RESIDENT.
- THE FOLLOWING SHALL BE INCIDENTAL TO THE 603 ITEMS:  
- ALL PIPE EXCAVATION INCLUDING ANY CUTTING AND REMOVAL OF PAVEMENT  
- FURNISHING, PLACING, GRADING, AND COMPACTING OF ANY NEW GRAVEL AND/OR FILL MATERIAL INCLUDING GRANULAR BORROW USED UNDER PIPES AND FOR TEMPORARY DETOURS TO MAINTAIN TRAFFIC DURING PIPE INSTALLATION (EXCAVATION IS ALSO INCIDENTAL).  
- GRANULAR BORROW UNDER THE PIPE SHALL MEET THE REQUIREMENTS FOR UNDERWATER BACKFILL.  
NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED, OR PLUGGED WITHOUT PRIOR APPROVAL OF THE RESIDENT.
- ANY NECESSARY CUTTING OF EXISTING PIPES TO FIT IN AREAS OF PROPOSED CATCH BASINS WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO THE 604 ITEMS.
- LOAM HAS BEEN ESTIMATED FOR ALL DISTURBED AREAS. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS NOTED ON THE PLANS OR DESIGNATED BY THE RESIDENT.
- LOAM SHALL BE PLACED TO A NOMINAL DEPTH OF 4 INCHES IN LAWN AREAS UNLESS OTHERWISE NOTED OR DIRECTED.
- SEEDING METHOD NO. 1 SHALL BE UTILIZED ON ALL LAWNS AND DEVELOPED AREAS.
- PAVEMENT MARKINGS SHALL CONFORM TO STANDARD DETAILS AND LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND AS DIRECTED BY THE RESIDENT. THE ABBREVIATIONS SHOWN IN THE PLANS ARE AS FOLLOWS:  
  
BYLL: 4" BROKEN YELLOW LANE LINE (10' SEGMENT W / 30' GAP)  
BWLL: 4" BROKEN WHITE LANE LINE (10' SEGMENT W / 30' GAP)  
DWBL: 6" DOTTED WHITE BIKE LANE LINE (2' SEGMENT W / 4' GAP)  
DWEL: 4" DOTTED WHITE LANE LINE (2' SEGMENT W / 4' GAP)  
DYCL: 4" DOUBLE YELLOW CENTERLINE  
SL: 24" WHITE STOP LINE  
SWBL: 6" SOLID WHITE BIKE LANE LINE  
SWEL: 4" SOLID WHITE EDGE LINE  
SWLL: 4" SOLID WHITE LANE LINE  
SYCL: 4" SOLID YELLOW LANE LINE  
SYLL: 4" SOLID YELLOW LANE LINE  
CROSSWALK: 8" SOLID WHITE LINE (8' OUT TO OUT)
- IF CONSTRUCTION MUST CONTINUE INTO 2014, TEMPORARY STRIPING AND MARKINGS SHALL BE INSTALLED BEFORE WINTER SUSPENSION.
- ITEM 608.26 - CURB RAMP DETECTABLE WARNING FIELD SHALL BE INSTALLED AT EACH CURB RAMP ADJACENT TO A MARKED CROSSWALK. ACTUAL PLACEMENT SHALL BE AS REQUIRED BY THE SPECIAL PROVISIONS, DETAILS, AND AS DIRECTED BY THE RESIDENT. PAYMENT FOR THIS ITEM SHALL INCLUDE ALL CONCRETE AND WORK REQUIRED BY THE DETAILS.
- ALL CATCH BASIN RIM ELEVATIONS SHALL BE VERIFIED IN THE FIELD. NEW CATCH BASIN RIMS SHALL BE SET 1" BELOW EXISTING PAVEMENT ELEVATION AT THE PROPOSED GUTTER LINE. IF CONSTRUCTION IS CONTINUED INTO 2014, BEFORE WINTER SUSPENSION THE MANHOLE RIMS SHALL HAVE TEMPORARY SETS AND BE REDONE FOR 2014 FINAL PAVING. ALL TEMPORARY SET AND REDO WORK SHALL BE INCIDENTAL.
- NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION'S BEST MANAGEMENT PRACTICES FOR EROSION CONTROL & SEDIMENT CONTROL, FEBRUARY, 2008.
- TEMPORARY EROSION CONTROL MEASURES SHALL BE MAINTAINED AS SPECIFIED IN THE SOIL EROSION AND WATER POLLUTION CONTROL PLAN. PAYMENT WILL BE MADE UNDER 656.75 - TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL, LUMP SUM.

## GRANITE CURB TO BE REMOVED AS PART OF THIS PROJECT IS AT THE FOLLOWING

LOCATIONS AND OF THE FOLLOWING APPROXIMATE LENGTHS:

LOCATION	LENGTH, FT
STA 2+10 RT	16
STA 2+05 RT	13.8
STA 2+10 RT	124.3
SIDEWALK TRANSITION AT PARK, SE CORNER	6
PORK CHOP ISLAND	115
STA 52+05 RT	16.5
STA 52+50, CHANNELIZATION ISLAND	128.7
CONGRESS NE CORNER @ DOGFISH CAFE	49
CONGRESS NW CORNER	58.1
CONGRESS EAST CHANNELIZATION ISLAND	28.3
	555.7 LF

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
AC-NH-1943(00)E  
WIN  
19430.00THOMAS A. ERICO  
No. 6618  
LICENSED PROFESSIONAL ENGINEER  
SIGNATURE  
6618  
P.E. NUMBER  
02-06-2013  
DATE

PROJ. MANAGER	T. Errico	DATE	BY	DATE
DESIGN-DETAILED	TAE		PEM	
CHECKED-REVIEWED	KEK		TAE	
DESIGN-DETAILED				
DESIGN-DETAILED				
REVISIONS 1				
REVISIONS 2				
REVISIONS 3				
REVISIONS 4				
FIELD CHANGES				

PORTLAND  
ST. JOHN STREET  
ESTIMATED QUANTITIES  
AND GENERAL NOTES

SHEET NUMBER

3

OF 7

TYLIN INTERNATIONAL

Filename: ... \msto\004\_HDPlan\_001.dgn

Division: HIGHWAY

Username: pncclure

Date: 2/6/2013

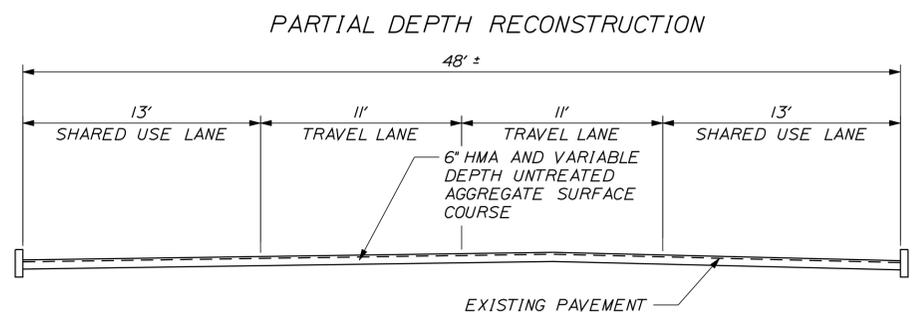


**LEGEND**

PROPOSED DETECTABLE WARNING PANEL	
PROPOSED CATCH BASIN	
PROPOSED LAWN	
PROPOSED CURB	
PROPOSED SIGNAL HEAD	
PROPOSED PEDESTRIAN HEAD	
PROPOSED JUNCTION BOX	
PROPOSED PEDESTAL POLE	
PROPOSED VEHICULAR HEAD	
PROPOSED SIGN ON POST	
PROPOSED CONDUIT	
EXISTING CATCH BASIN	
EXISTING HYDRANT	
EXISTING LIGHT POLE	
EXISTING JUNCTION BOX	
EXISTING MH COVER	
EXISTING TRAFFIC SIGNAL POLE	
EXISTING ORNAMENTAL LIGHT POLE	
EXISTING CMP POLE	
EXISTING SIGN ON POST	
EXISTING MAST ARM	
EXISTING HANGING SIGN	

**NOTE:**  
PEDESTRIAN IMPROVEMENTS WEST OF THE RAILROAD TRESTLE ARE PART OF ANOTHER PROJECT, BY OTHERS.

- BYLL - 4" BROKEN YELLOW LANE LINE (10' SEGMENT W/ 30' GAP)
- BWLL - 4" BROKEN WHITE LANE LINE (10' SEGMENT W/ 30' GAP)
- DWBL - 6" DOTTED WHITE BIKE LANE LINE (2' SEGMENT W/ 4' GAP)
- DWEL - 4" DOTTED WHITE LANE LINE (2' SEGMENT W / 4' GAP)
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- SYCL - 4" SOLID YELLOW LANE LINE
- SYLL - 4" SOLID YELLOW LANE LINE
- CROSSWALK - 8" SOLID WHITE LINE (8' OUT TO OUT)



**TYPICAL SECTION, ST. JOHN STREET**  
NOTE: PARK AVENUE SHALL ALSO HAVE 6" HMA AND VARIABLE DEPTH UNTREATED AGGREGATE SURFACE COURSE



STATE OF MAINE DEPARTMENT OF TRANSPORTATION		AC-NH-1943(000)E	
		WIN 19430.00	
DATE	BY	T. Errico	DATE
DESIGN DETAILED	TAE	PEM	
CHECKED/REVIEWED	KEK	TAE	
DESIGN DETAILED			
REVISIONS 1			
REVISIONS 2			
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			
PORTLAND ST. JOHN STREET		PLANS	
SHEET NUMBER		4	
		OF 7	

**TYLIN INTERNATIONAL**

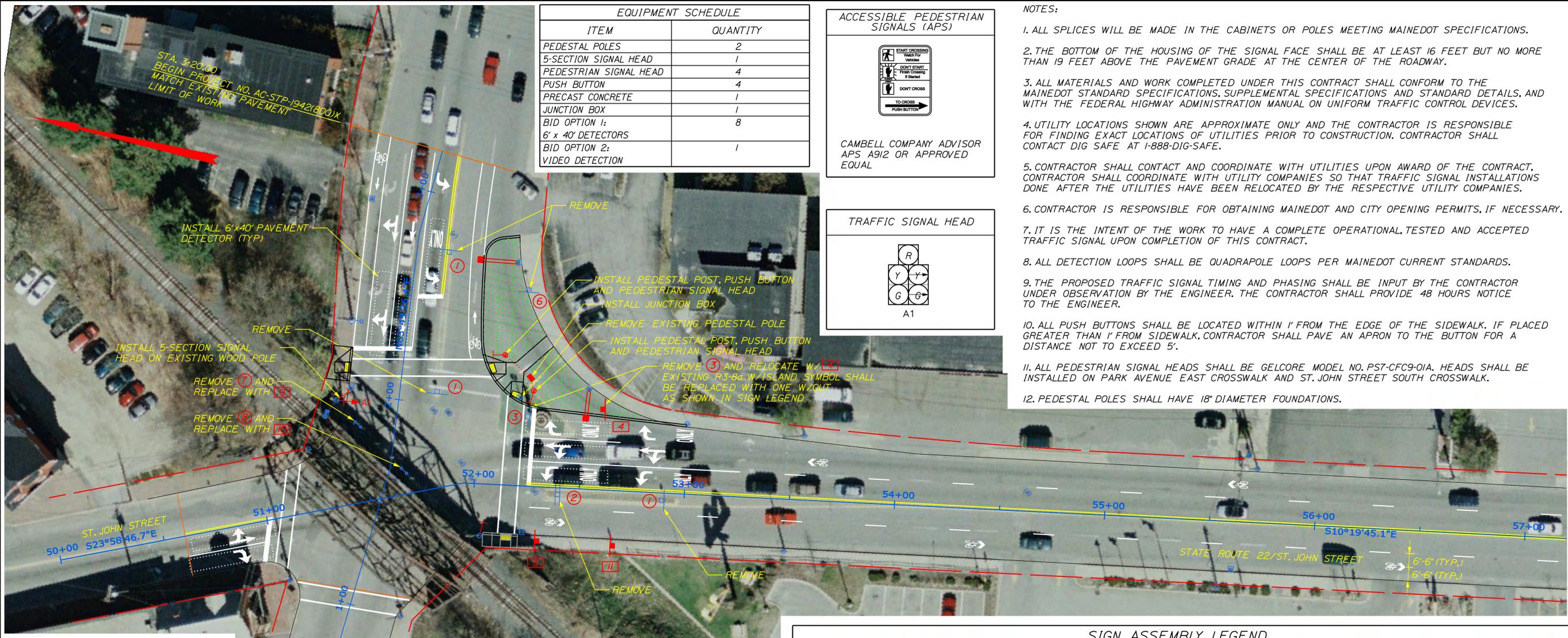


Date: 2/6/2013

Username: pmncclure

Division: HIGHWAY

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EQUIPMENT SCHEDULE	
ITEM	QUANTITY
PEDESTAL POLES	2
5-SECTION SIGNAL HEAD	1
PEDESTRIAN SIGNAL HEAD	4
PUSH BUTTON	4
PRECAST CONCRETE	1
JUNCTION BOX	1
BID OPTION 1: 6' x 40' DETECTORS	8
BID OPTION 2: VIDEO DETECTION	1

ACCESSIBLE PEDESTRIAN SIGNALS (APS)

CAMBELL COMPANY ADVISOR  
APS A912 OR APPROVED  
EQUAL

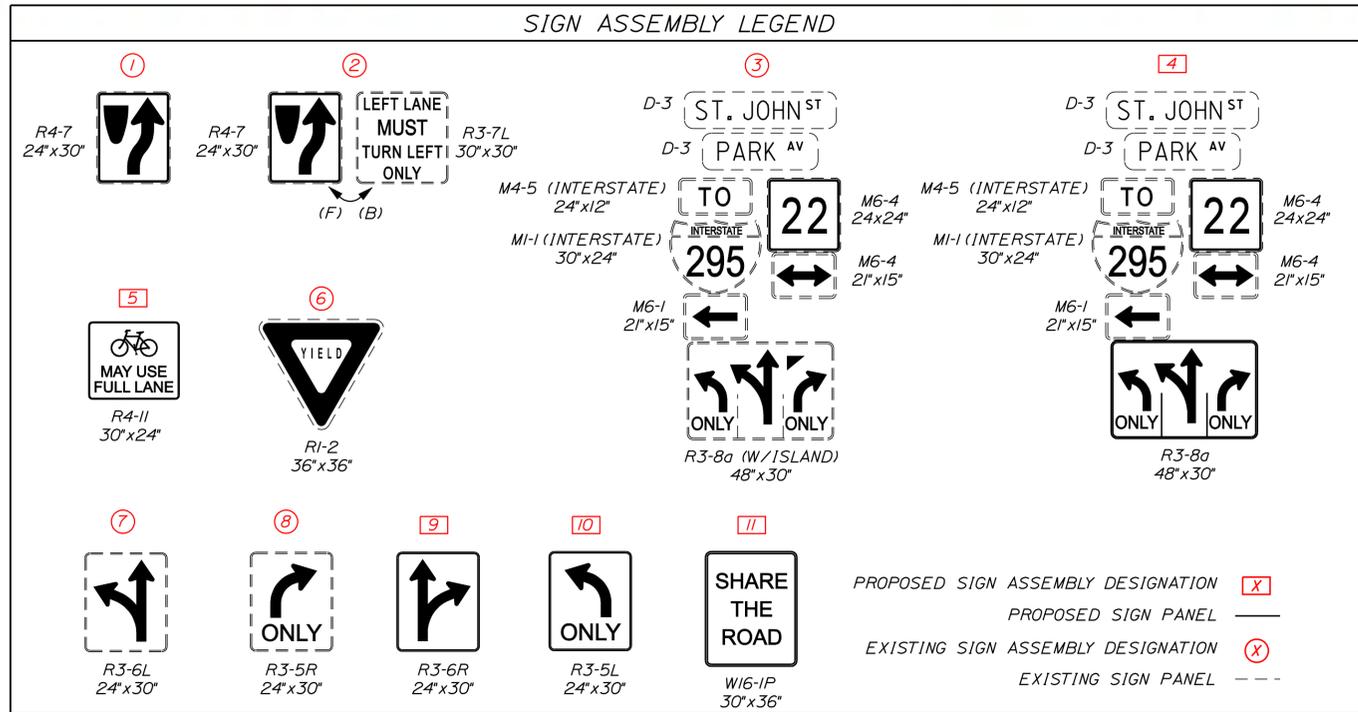
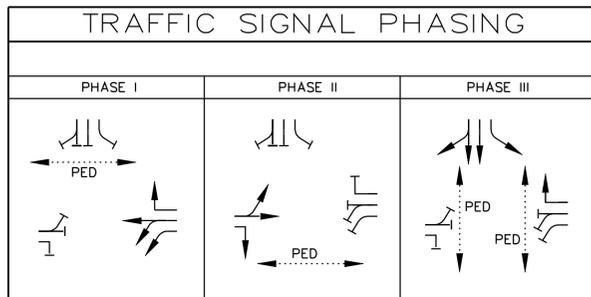
TRAFFIC SIGNAL HEAD

- NOTES:
- ALL SPLICES WILL BE MADE IN THE CABINETS OR POLES MEETING MAINEDOT SPECIFICATIONS.
  - THE BOTTOM OF THE HOUSING OF THE SIGNAL FACE SHALL BE AT LEAST 16 FEET BUT NO MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
  - ALL MATERIALS AND WORK COMPLETED UNDER THIS CONTRACT SHALL CONFORM TO THE MAINEDOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS, AND WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
  - UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY AND THE CONTRACTOR IS RESPONSIBLE FOR FINDING EXACT LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL CONTACT DIG SAFE AT 1-888-DIG-SAFE.
  - CONTRACTOR SHALL CONTACT AND COORDINATE WITH UTILITIES UPON AWARD OF THE CONTRACT. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES SO THAT TRAFFIC SIGNAL INSTALLATIONS DONE AFTER THE UTILITIES HAVE BEEN RELOCATED BY THE RESPECTIVE UTILITY COMPANIES.
  - CONTRACTOR IS RESPONSIBLE FOR OBTAINING MAINEDOT AND CITY OPENING PERMITS, IF NECESSARY.
  - IT IS THE INTENT OF THE WORK TO HAVE A COMPLETE OPERATIONAL, TESTED AND ACCEPTED TRAFFIC SIGNAL UPON COMPLETION OF THIS CONTRACT.
  - ALL DETECTION LOOPS SHALL BE QUADRAPOLE LOOPS PER MAINEDOT CURRENT STANDARDS.
  - THE PROPOSED TRAFFIC SIGNAL TIMING AND PHASING SHALL BE INPUT BY THE CONTRACTOR UNDER OBSERVATION BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE 48 HOURS NOTICE TO THE ENGINEER.
  - ALL PUSH BUTTONS SHALL BE LOCATED WITHIN 1' FROM THE EDGE OF THE SIDEWALK. IF PLACED GREATER THAN 1' FROM SIDEWALK, CONTRACTOR SHALL PAVE AN APRON TO THE BUTTON FOR A DISTANCE NOT TO EXCEED 5'.
  - ALL PEDESTRIAN SIGNAL HEADS SHALL BE GELCORE MODEL NO. PS7-CFC9-01A. HEADS SHALL BE INSTALLED ON PARK AVENUE EAST CROSSWALK AND ST. JOHN STREET SOUTH CROSSWALK.
  - PEDESTAL POLES SHALL HAVE 18" DIAMETER FOUNDATIONS.

- LEGEND
- PROPOSED DETECTABLE WARNING PANEL
  - PROPOSED CATCH BASIN
  - PROPOSED ESPLANADE
  - PROPOSED CURB
  - PROPOSED SIGNAL HEAD
  - PROPOSED PEDESTRIAN HEAD
  - PROPOSED JUNCTION BOX
  - PROPOSED PEDESTAL POLE
  - PROPOSED VEHICULAR HEAD
  - PROPOSED SIGN ON POST
  - PROPOSED CONDUIT
  - PROPOSED SIGN ASSEMBLY DESIGNATION
  - EXISTING SIGN ASSEMBLY DESIGNATION
  - EXISTING CATCH BASIN
  - EXISTING HYDRANT
  - EXISTING LIGHT POLE
  - EXISTING JUNCTION BOX
  - EXISTING MH COVER
  - EXISTING TRAFFIC SIGNAL POLE
  - EXISTING ORNAMENTAL LIGHT POLE
  - EXISTING CMP POLE
  - EXISTING SIGN ON POST
  - EXISTING MAST ARM
  - EXISTING HANGING SIGN

SIGNAL TIMING SCHEDULE			
ITEM/PHASE	1	2	3
MINIMUM INITIAL	3	3	3
MAXIMUM INITIAL	-	-	-
PASSAGE TIME	2	2	2
MAXIMUM 1	40	25	40
MAXIMUM 2	-	-	-
YELLOW	3	3	3
ALL RED	2	2	2
PEDESTRIAN WALK	4.0	4.0	4.0
PEDESTRIAN CLEARANCE	20	18	18
RECALL	0	S	0
DETECTION OPERATION	PR	PR	PR
PREEMPTION PRIORITY	-	-	-
FLASH	R	R	R
DUAL ENTRY	-	-	-

PR - PRESENCE  
R - RED  
Y - YELLOW  
S - SOFT RECALL  
O - RECALL OFF



STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
AC-NH-1943(00)E  
WIN  
19430.00



PROJ. MANAGER	DATE	BY	DATE
T. Errico		TAE	
DESIGN DETAILED		KEK	
CHECKED/REVIEWED		TAE	
DESIGN DETAILED			
DESIGN DETAILED			
REVISIONS 1			
REVISIONS 2			
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

PORTLAND  
ST. JOHN STREET  
SIGNAL & SIGNING PLANS

SHEET NUMBER

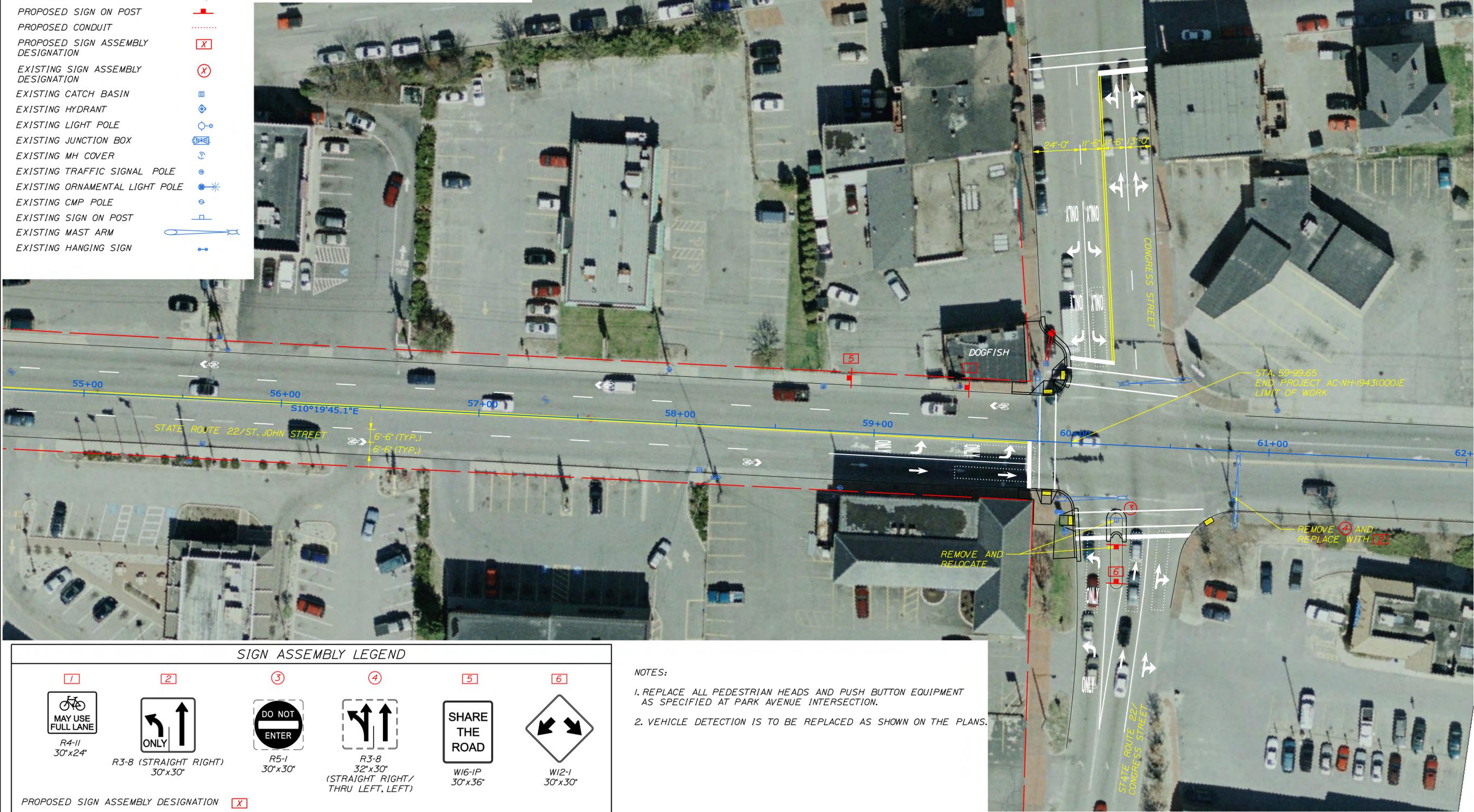
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OF 7



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 Division: HIGHWAY  
 Username: pncclure  
 Date: 2/6/2013

- LEGEND**
- PROPOSED DETECTABLE WARNING PANEL ■
  - PROPOSED CATCH BASIN ■
  - PROPOSED ESPLANADE ▬
  - PROPOSED CURB ▬
  - PROPOSED SIGNAL HEAD +
  - PROPOSED PEDESTRIAN HEAD T
  - PROPOSED JUNCTION BOX ■
  - PROPOSED PEDESTAL POLE ●
  - PROPOSED VEHICULAR HEAD ➔
  - PROPOSED SIGN ON POST T
  - PROPOSED CONDUIT ⋯
  - PROPOSED SIGN ASSEMBLY DESIGNATION X
  - EXISTING SIGN ASSEMBLY DESIGNATION X
  - EXISTING CATCH BASIN ■
  - EXISTING HYDRANT ●
  - EXISTING LIGHT POLE ○
  - EXISTING JUNCTION BOX ■
  - EXISTING MH COVER ○
  - EXISTING TRAFFIC SIGNAL POLE ●
  - EXISTING ORNAMENTAL LIGHT POLE \*
  - EXISTING CMP POLE ○
  - EXISTING SIGN ON POST T
  - EXISTING MAST ARM —
  - EXISTING HANGING SIGN —

EQUIPMENT SCHEDULE	
ITEM	QUANTITY
PEDESTRIAN SIGNAL HEAD	8
PUSH BUTTON	8
BID OPTION 1: 6' x 40' DETECTORS	
	7
BID OPTION 2: VIDEO DETECTORS	
	1



**SIGN ASSEMBLY LEGEND**

<span style="border: 1px solid red; padding: 2px;">1</span>  R4-11 30"x24"	<span style="border: 1px solid red; padding: 2px;">2</span>  R3-8 (STRAIGHT RIGHT) 30"x30"	<span style="border: 1px solid red; padding: 2px;">3</span>  R5-1 30"x30"	<span style="border: 1px solid red; padding: 2px;">4</span>  R3-8 32"x30" (STRAIGHT RIGHT/ THRU LEFT, LEFT)	<span style="border: 1px solid red; padding: 2px;">5</span>  W16-1P 30"x36"	<span style="border: 1px solid red; padding: 2px;">6</span>  W12-1 30"x30"
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PROPOSED SIGN ASSEMBLY DESIGNATION X  
 PROPOSED SIGN PANEL ▬  
 EXISTING SIGN ASSEMBLY DESIGNATION X  
 EXISTING SIGN PANEL ▬

**NOTES:**

1. REPLACE ALL PEDESTRIAN HEADS AND PUSH BUTTON EQUIPMENT AS SPECIFIED AT PARK AVENUE INTERSECTION.
2. VEHICLE DETECTION IS TO BE REPLACED AS SHOWN ON THE PLANS.



STATE OF MAINE DEPARTMENT OF TRANSPORTATION		AC-NH-1943(00)E	WIN 19430.00
		SIGNATURE THOMAS A. ERICO No. 6618	DATE 02-06-2013
DESIGN-DETAILED TAE	CHECKED-REVIEWED KEK	DESIGN-DETAILED TAE	REVISIONS 1 REVISIONS 2 REVISIONS 3 REVISIONS 4 FIELD CHANGES
PORTLAND ST. JOHN STREET		SIGNAL & SIGNING PLANS	
SHEET NUMBER		7	
OF 7			