

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



PORTLAND CUMBERLAND

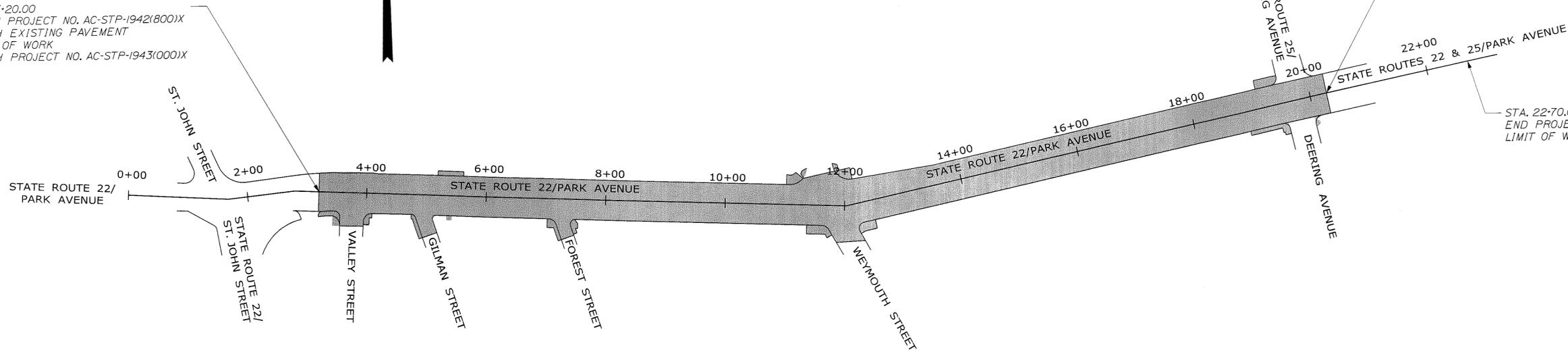
PARK AVENUE
AC-STP-1942(800)X
PROJECT LENGTH : 0.369 MILES

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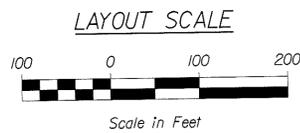
STA. 3+20.00
BEGIN PROJECT NO. AC-STP-1942(800)X
MATCH EXISTING PAVEMENT
LIMIT OF WORK
MATCH PROJECT NO. AC-STP-1943(000)X

STA. 20+27.73
END OVERLAY
MATCH EXISTING PAVEMENT

STA. 22+70.00
END PROJECT NO. AC-STP-1942(800)X
LIMIT OF WORK



TRAFFIC DATA	
Current 2012) AADT	10040
Future (2032) AADT	11040
DHV - % of AADT	11%
Design Hour Volume	1214
% Heavy Trucks (AADT)	4%
% Heavy Trucks (DHV)	4%
Directional Distribution (DHV)	72%
18 kip Equivalent P 2.0	134
18 kip Equivalent P 2.5	127
Design Speed (mph)	30
Functional Class:	PRINCIPAL ARTERIAL



<u>PROJECT LOCATION:</u>	State Route 22
<u>PROGRAM AREA:</u>	Traffic Program
<u>OUTLINE OF WORK:</u>	Partial Mill and 3/4" Overlay to Include Traffic and Pedestrian Improvements

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	APPROVED	DATE
COMMISSIONER:	[Signature]	7/25/12
CHIEF ENGINEER:	[Signature]	7/25/12

STATE OF MAINE THOMAS A. ERICO No. 6618 PROFESSIONAL ENGINEER	[Signature]	DATE
SIGNATURE	6618	07/20/12
P.E. NUMBER	07/20/12	DATE

PROJECT INFORMATION	
PROGRAM	TRAFFIC
PROJECT MANAGER	BRIAN KEETZER
DESIGNER	TOM ERICO
CONSULTANT	T.Y. LIN INTERNATIONAL
PROJECT RESIDENT	
CONTRACTOR	
PROJECT COMPLETION DATE	

PORTLAND
PARK AVENUE
TITLE SHEET

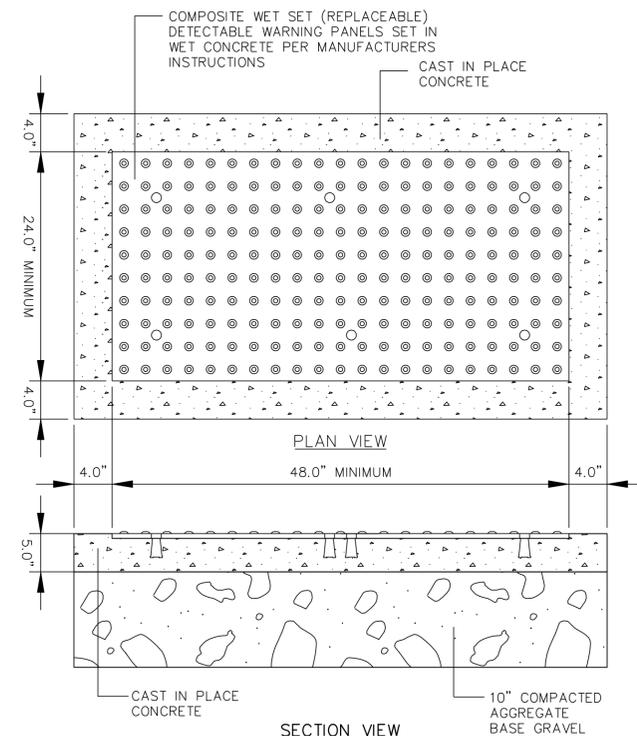
SHEET NUMBER
1
OF 9

Date: 7/23/2012
 Username: pmccclure
 Division: HIGHWAY
 Filename: ... \HIGHWAY\WSTA\001_title.dgn

WIN 19428.00 AC-STP-1942(800)X

NOTES:

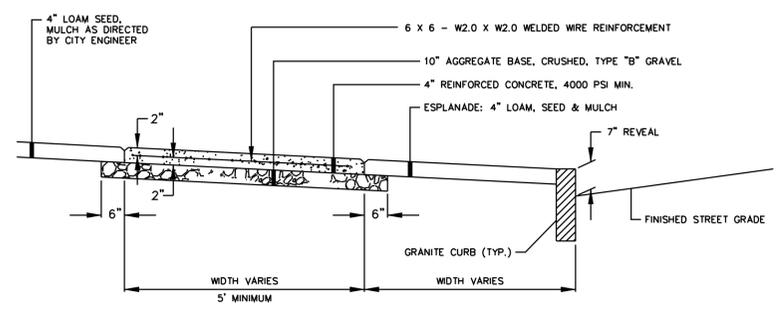
1. COMPOSITE WET SET (REPLACEABLE) DETECTABLE WARNING PANELS SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC. (WWW.ADATILE.COM), OR APPROVED EQUAL.
2. CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MAINE D.O.T. CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4,000 PSI. THE CONCRETE SHALL BE SEALED PRIOR TO SETTING PANELS. THE EXPOSED CONCRETE BORDER SHALL RECEIVE A GROOVED EDGE BETWEEN THE PANEL AND CONCRETE, ALONG WITH A UNIFORM BROOM FINISH PERPENDICULAR TO THE FLOW OF PEDESTRIAN TRAFFIC.
3. TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. TRUNCATED DOME BRICKS AND GRANITE PAVERS ARE NOT ALLOWED.
4. FOR ALL DETECTABLE WARNING PANELS (EXCEPT AS SPECIFIED IN FIGURE 1-7A AND TECHNICAL MANUAL SECTION 1.8.4.), FEDERAL YELLOW COLORED (#33538) PANELS SHALL BE USED. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
5. SIZE: THE DETECTABLE WARNING PANEL(S) SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
6. ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.



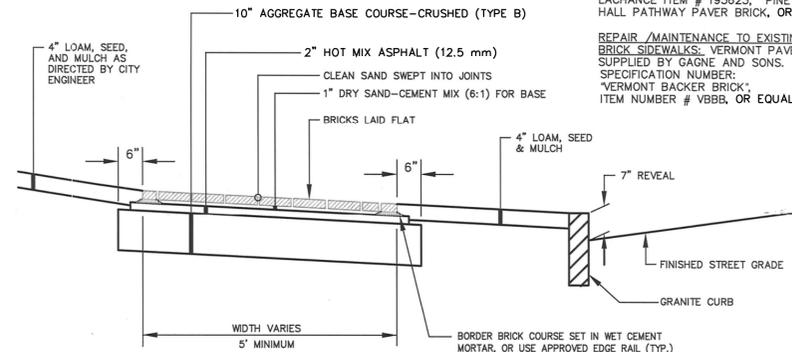
SIDEWALK RAMP DETECTABLE WARNING PANEL
NOT TO SCALE



PHOTO OF DWT INSTALLATION ON BRICK SIDEWALK



REINFORCED CONCRETE SIDEWALK
NOT TO SCALE



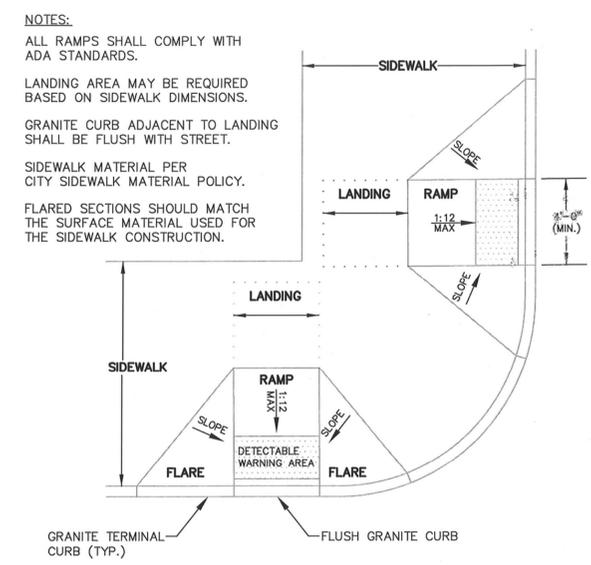
BRICK SIDEWALK WITH BITUMINOUS BASE
NOT TO SCALE

NOTE: HMA BASE IS 12.5mm

BRICKS TO BE USED:

NEW CONSTRUCTION:
4"x8" PINE HALL PATHWAY PAVER BRICK; MFG. BY PINE HALL BRICK CO., MADISON, NORTH CAROLINA. LACHANCE ITEM # 193623, PINE HALL PATHWAY PAVER BRICK, OR EQUAL.

REPAIR /MAINTENANCE TO EXISTING BRICK SIDEWALKS: VERMONT PAVER; SUPPLIED BY GAGNE AND SONS. SPECIFICATION NUMBER: "VERMONT BACKER BRICK"; ITEM NUMBER # VBBB, OR EQUAL.

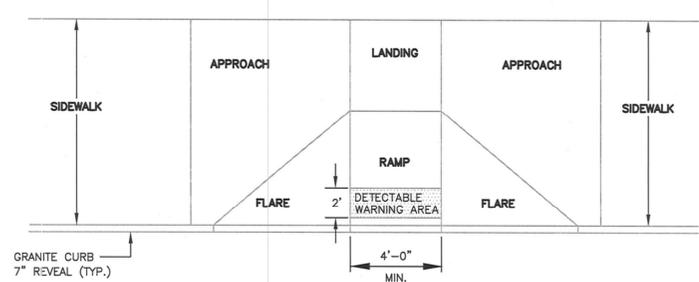


PREFERRED SIDEWALK RAMP AT INTERSECTION
NOT TO SCALE

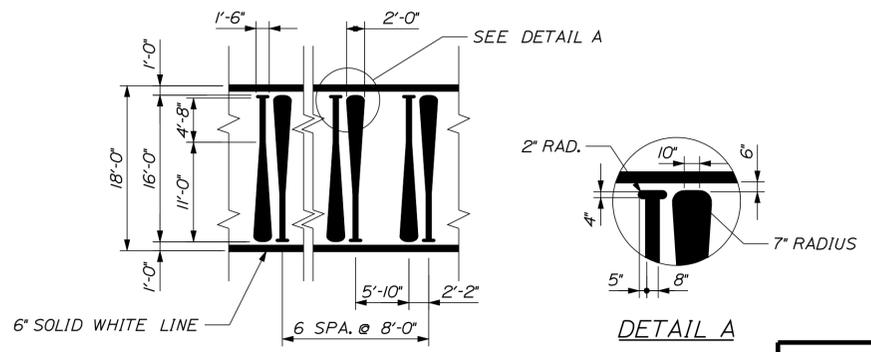
NOTES:

ALL RAMPS SHALL COMPLY WITH ADA STANDARDS.

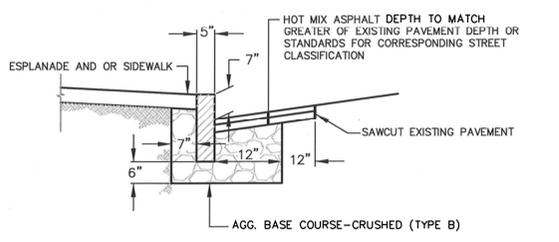
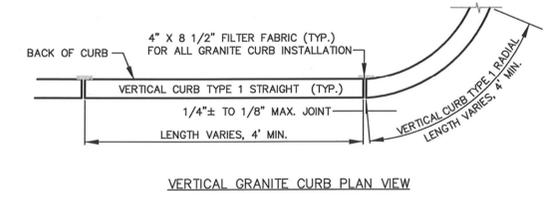
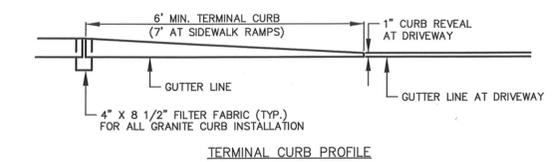
GRANITE CURB ADJACENT TO RAMP SHALL BE FLUSH WITH STREET.



PERPENDICULAR ADA RAMP LAYOUT FOR WIDE SIDEWALK WITH NO ESPLANADE
NOT TO SCALE



HADLOCK CROSSWALK DETAIL



VERTICAL GRANITE CURB INSTALLATION IN EXISTING STREETS
NOT TO SCALE

DESIGN ELEMENT	SLOPE IN DIRECTION OF TRAVEL	CROSS SLOPE
APPROACH	8.33% MAXIMUM	2%
LANDING	2%	2%
RAMP	8.33% MAXIMUM	MATCH STREET GRADE
FLARE	10% MAX. AT CURB FACE	-
SIDEWALK	MATCH STREET GRADE	2%

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
AC-STP-1942(800)X
WIN 19428.00

THOMAS A. ERICO
REGISTERED PROFESSIONAL ENGINEER
No. 6618
02-06-2013

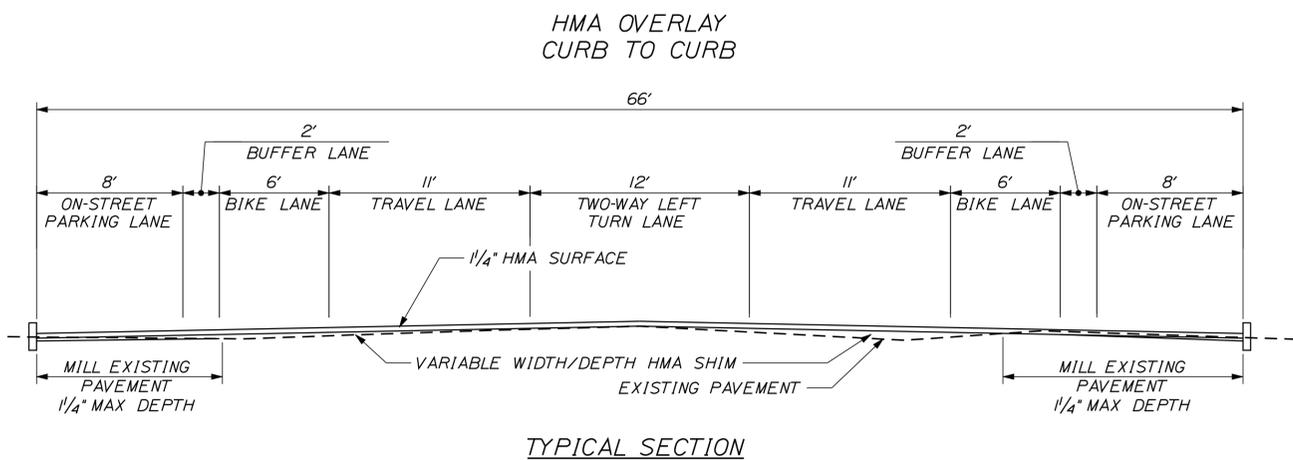
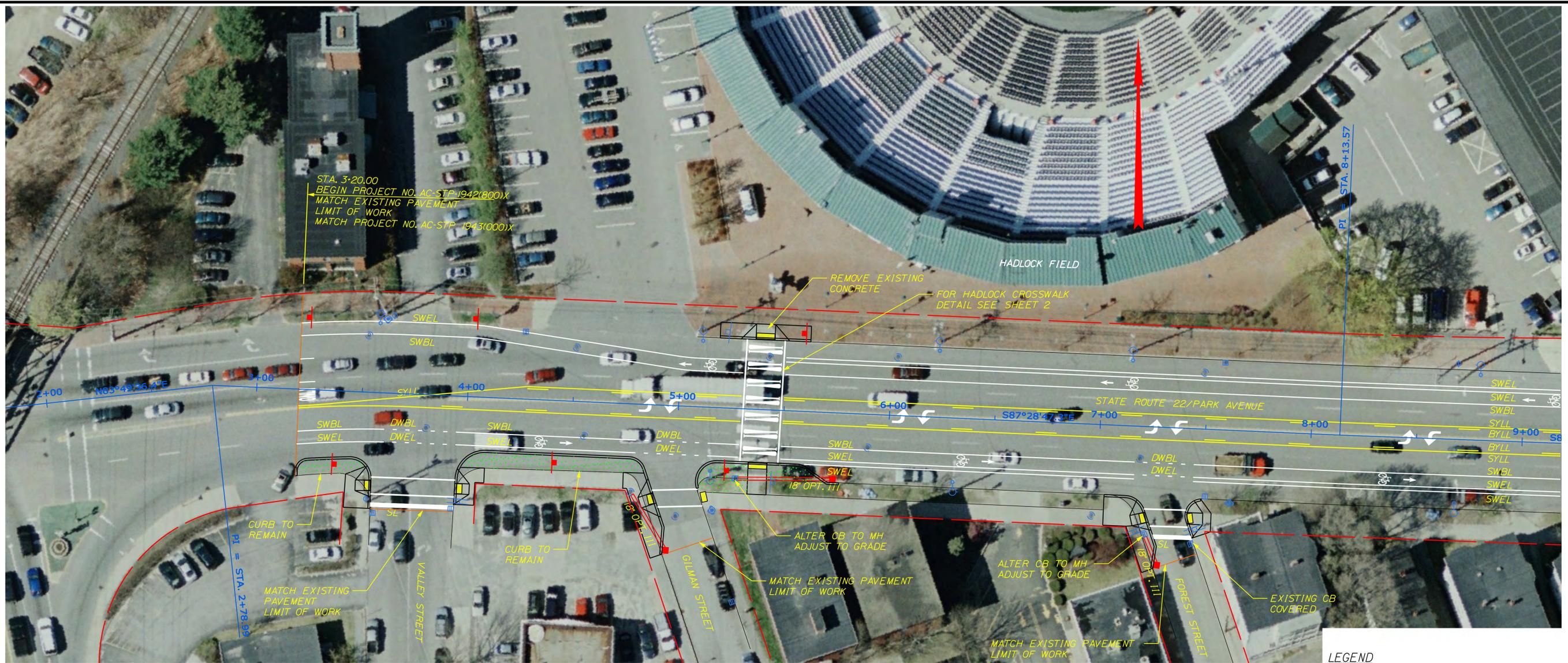
PROJ. MANAGER: T. Errico
DATE: 4/6/2012
BY: TAE
CHECKED/REVIEWED: KEK
DESIGN/DETAILED: TAE
REVISIONS: 1
REVISIONS: 2
REVISIONS: 3
REVISIONS: 4
FIELD CHANGES

PORTLAND
PARK AVENUE
DETAILS

SHEET NUMBER
2
OF 9

TYLIN INTERNATIONAL

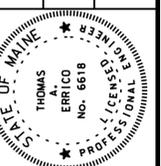
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Division: HIGHWAY
Username: pmcclure
Date: 2/6/2013



- BYLL - 4" BROKEN YELLOW LANE LINE (10' SEGMENT W/ 30' GAP)
- BWLL - 4" BROKEN WHITE LANE LINE (10' SEGMENT W/ 30' GAP)
- DWBL - 6" DOTTED WHITE BIKE LANE LINE (2' SEGMENT W/ 4' GAP)
- DWEL - 4" DOTTED WHITE LANE LINE (2' SEGMENT W / 4' GAP)
- DYCL - 4" DOUBLE YELLOW CENTERLINE
- SL - 24" WHITE STOP LINE
- SWBL - 6" SOLID WHITE BIKE LANE LINE
- SWEL - 4" SOLID WHITE EDGE LINE
- SWLL - 4" SOLID WHITE LANE LINE
- SYCL - 4" SOLID YELLOW LANE LINE
- SYLL - 4" SOLID YELLOW LANE LINE
- CROSSWALK - 8" SOLID WHITE LINE (8' OUT TO OUT)

LEGEND

- PROPOSED CURB RAMP
- DETECTABLE WARNING FIELD
- PROPOSED CATCH BASIN
- PROPOSED LAWN
- PROPOSED CURB
- PROPOSED PEDESTRIAN HEAD
- PROPOSED JUNCTION BOX
- PROPOSED PEDESTAL POLE
- PROPOSED VEHICULAR HEAD
- PROPOSED SIGN ON POST
- PROPOSED CONDUIT
- EXISTING CATCH BASIN
- EXISTING HYDRANT
- EXISTING LIGHT POLE
- EXISTING JUNCTION BOX
- EXISTING MH COVER
- EXISTING TRAFFIC SIGNAL POLE
- EXISTING ORNAMENTAL LIGHT POLE
- EXISTING CMP POLE
- EXISTING SIGN ON POST
- EXISTING MAST ARM



PROJ. MANAGER	T. Errico	DATE	4/6/2012
DESIGN-DETAILED	TAE	BY	TAE
CHECKED-REVIEWED	KEK	DATE	7/13/2012
DESIGN-DETAILED		SIGNATURE	6618
REVISIONS 1		P.E. NUMBER	02-06-2013
REVISIONS 2		DATE	
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

PORTLAND
PARK AVENUE
PLANS

SHEET NUMBER

4

OF 9

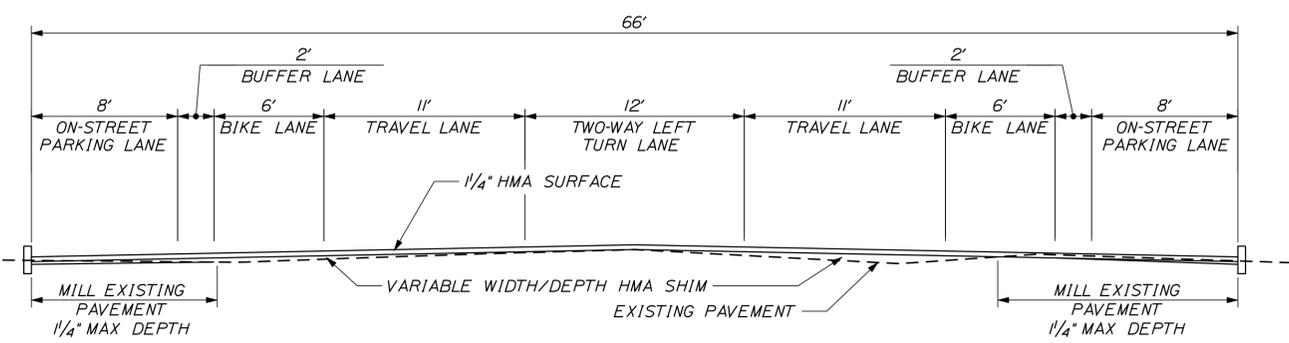


LEGEND

- PROPOSED CURB RAMP
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- PROPOSED PEDESTAL POLE
- PROPOSED VEHICULAR HEAD
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- PROPOSED CONDUIT
- EXISTING CATCH BASIN
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HMA OVERLAY CURB TO CURB



TYPICAL SECTION



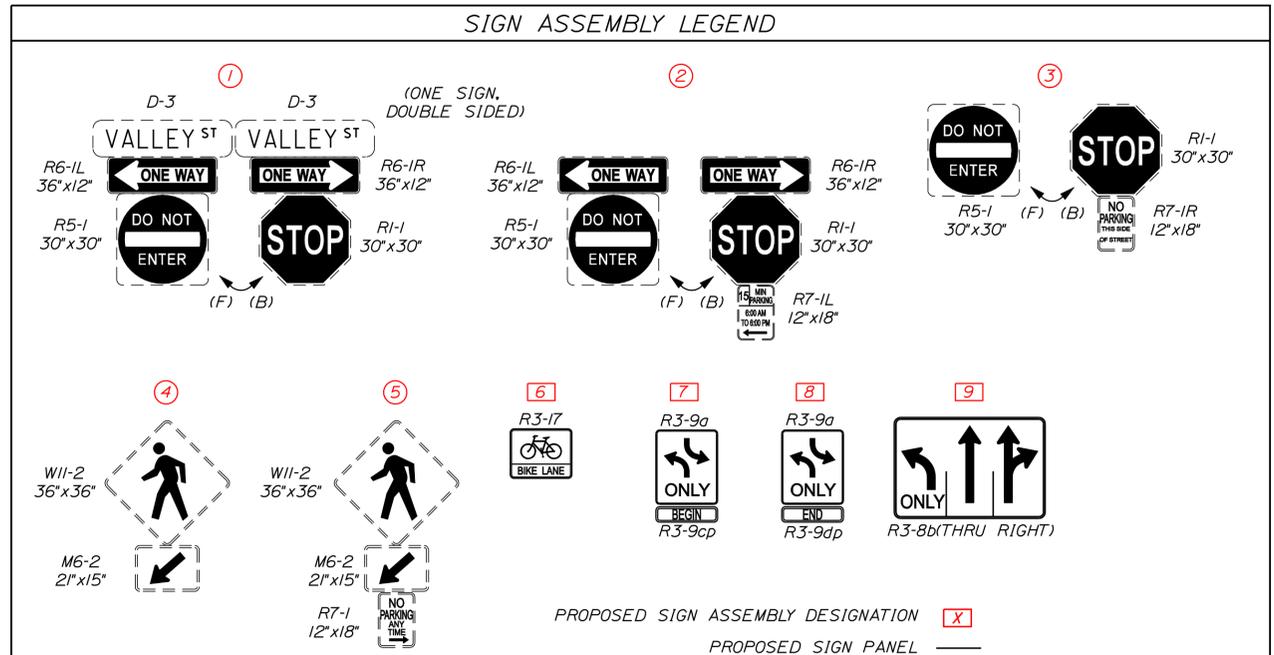
PLAN

STATE OF MAINE DEPARTMENT OF TRANSPORTATION		AC-STP-1942(800)X	
		WIN 19428.00	
PROJ. MANAGER	T. Errico	DATE	4/6/2012
DESIGN-DETAILED	TAE	BY	T. Errico
CHECKED-REVIEWED	KEK	DATE	7/13/2012
DESIGN-DETAILED		SIGNATURE	6618
REVISIONS 1		P.E. NUMBER	02-06-2013
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REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			
PORTLAND PARK AVENUE		PLANS	
SHEET NUMBER			
6			
OF 9			
TYLIN INTERNATIONAL			

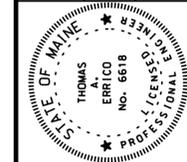


LEGEND

PROPOSED CURB RAMP	
DETECTABLE WARNING FIELD	
PROPOSED CATCH BASIN	
PROPOSED ESPLANADE	
PROPOSED CURB	
PROPOSED PEDESTRIAN HEAD	
PROPOSED JUNCTION BOX	
PROPOSED PEDESTAL POLE	
PROPOSED VEHICULAR HEAD	
PROPOSED SIGN ON POST	
PROPOSED CONDUIT	
PROPOSED SIGN ASSEMBLY DESIGNATION	
EXISTING SIGN ASSEMBLY DESIGNATION	
EXISTING CATCH BASIN	
EXISTING HYDRANT	
EXISTING LIGHT POLE	
EXISTING JUNCTION BOX	
EXISTING MH COVER	
EXISTING TRAFFIC SIGNAL POLE	
EXISTING ORNAMENTAL LIGHT POLE	
EXISTING CMP POLE	
EXISTING SIGN ON POST	
EXISTING HANGING SIGN	
EXISTING MAST ARM	



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
AC-STP-1942(800)X
WIN
19428.00



PROJ. MANAGER	T. Errico
DESIGN DETAILED	TAE
CHECKED/REVIEWED	KEK
DESIGN DETAILED	
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
FIELD CHANGES	
DATE	4/6/2012
BY	TAE
DATE	7/13/2012
SIGNATURE	THOMAS A. ERICO
P.E. NUMBER	6618
DATE	02-06-2013

PORTLAND
PARK AVENUE
SIGNAL & SIGNING PLANS

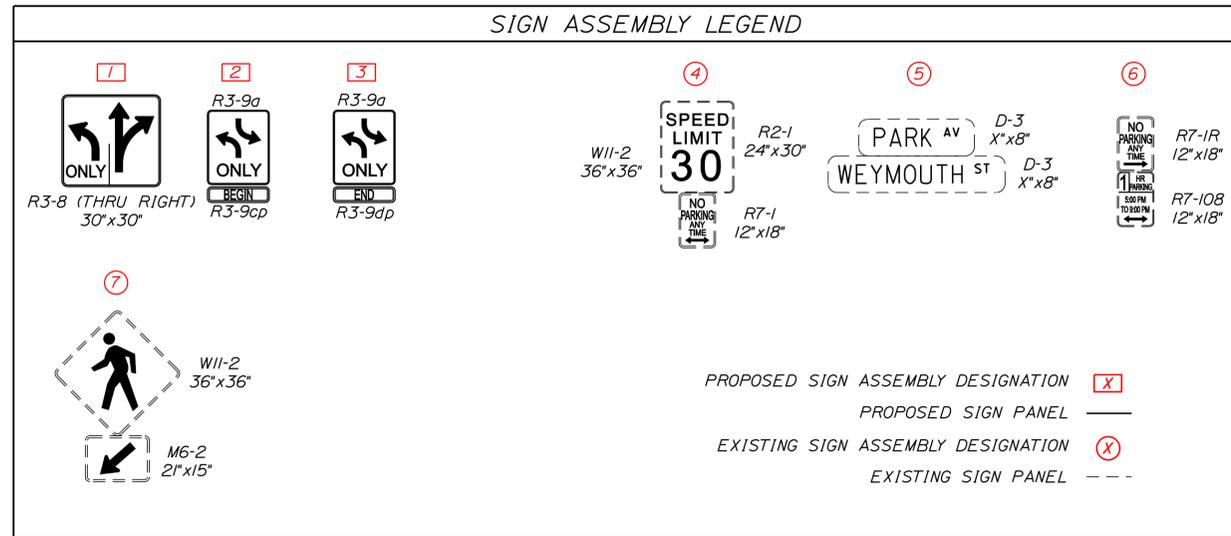
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OF 9



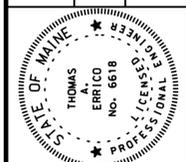


LEGEND

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- DETECTABLE WARNING FIELD
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STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
AC-STP-1942(800)X
WIN
19428.00



PROJ. MANAGER	T. Errico
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DESIGN DETAILED	
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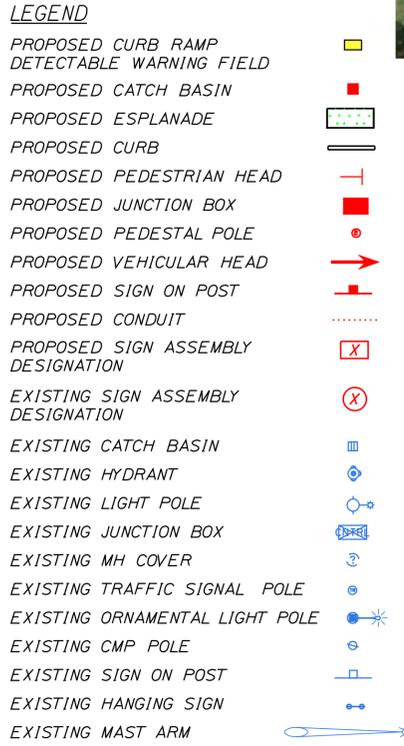
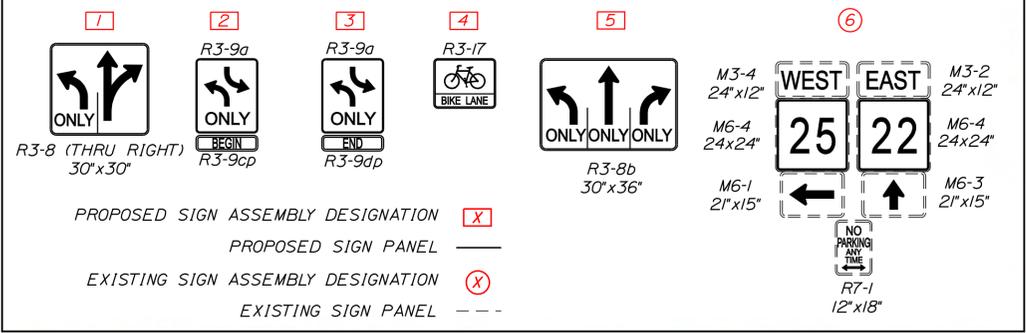
PORTLAND
PARK AVENUE
SIGNAL & SIGNING PLANS

SHEET NUMBER
8
OF 9



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 Username: pmcclure
 Date: 2/6/2013

SIGN ASSEMBLY LEGEND



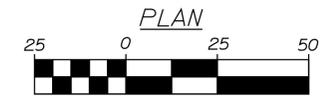
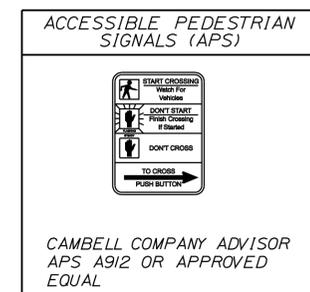
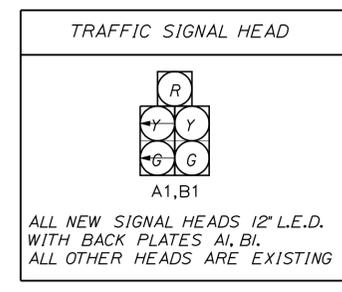
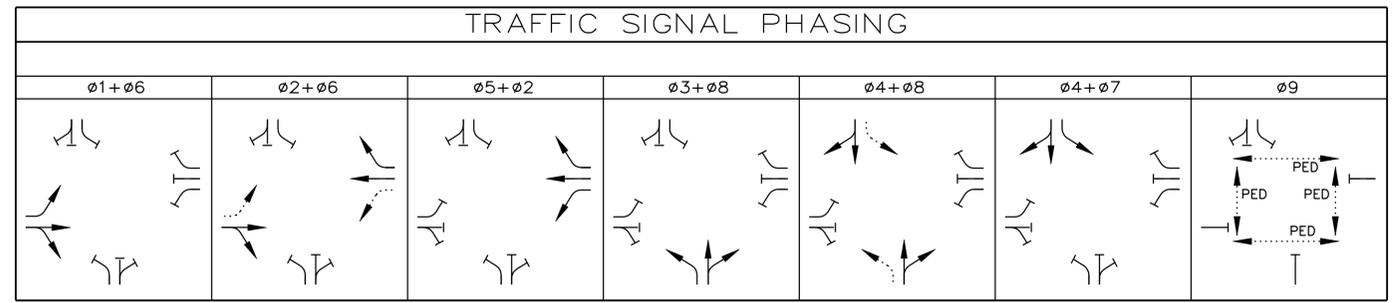
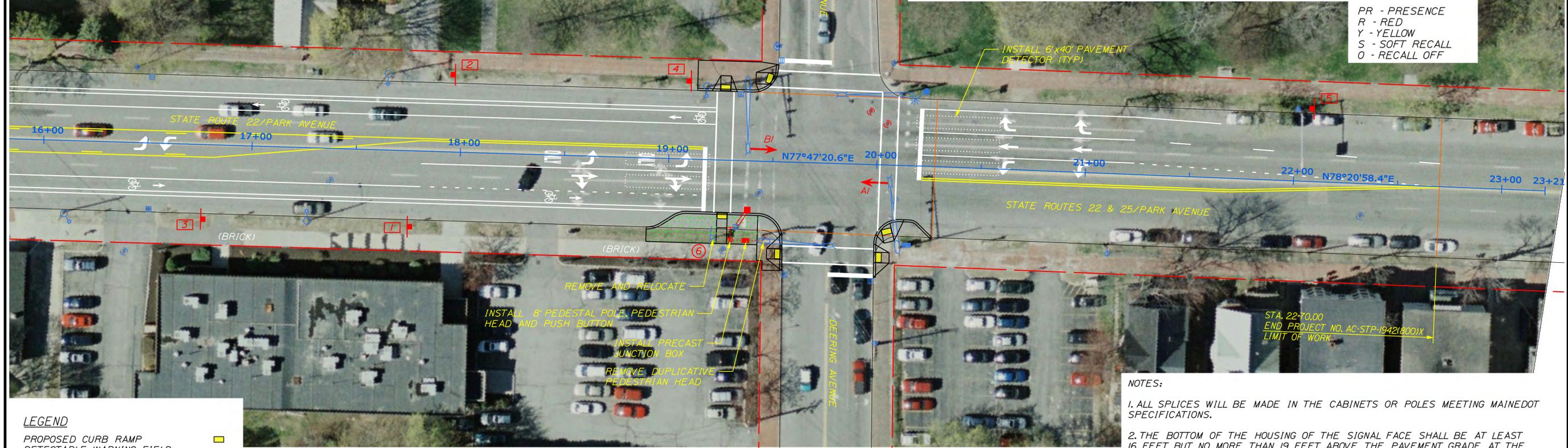
EQUIPMENT SCHEDULE

ITEM	QUANTITY
PEDESTAL POLES	1
5-SECTION SIGNAL HEAD	2
PEDESTRIAN SIGNAL HEAD	1
PUSH BUTTON	1
PRECAST CONCRETE JUNCTION BOX	1
BID ALTERNATE 1 6' x 40' DETECTORS	5
BID ALTERNATE 2 VIDEO DETECTION	1

SIGNAL TIMING SCHEDULE

ITEM/PHASE	1	2	3	4	5	6	7	8	9
MINIMUM INITIAL	3	10	3	3	3	10	3	3	-
MAXIMUM INITIAL	-	-	-	-	-	-	-	-	-
PASSAGE TIME	2	2	2	2	2	2	2	2	-
MAXIMUM 1	15	40	15	25	15	40	15	25	-
MAXIMUM 2	-	-	-	-	-	-	-	-	-
YELLOW	3	3	3	3	3	3	3	3	-
ALL RED	2	2	2	2	2	2	2	2	-
PEDESTRIAN WALK	-	-	-	-	-	-	-	-	7
PEDESTRIAN CLEARANCE	-	-	-	-	-	-	-	-	19
RECALL	0	S	0	0	0	S	0	0	-
DETECTION OPERATION	PR	-							
PREEMPTION PRIORITY	-	-	-	-	-	-	-	-	-
FLASH	-	-	-	-	-	-	-	-	-
DUAL ENTRY	-	-	-	-	-	-	-	-	-

PR - PRESENCE
 R - RED
 Y - YELLOW
 S - SOFT RECALL
 O - RECALL OFF



- NOTES:**
- ALL SPLICES WILL BE MADE IN THE CABINETS OR POLES MEETING MAINEDOT SPECIFICATIONS.
 - THE BOTTOM OF THE HOUSING OF THE SIGNAL FACE SHALL BE AT LEAST 16 FEET BUT NO MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
 - ALL MATERIALS AND WORK COMPLETED UNDER THIS CONTRACT SHALL CONFORM TO THE MAINEDOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS, AND WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY AND THE CONTRACTOR IS RESPONSIBLE FOR FINDING EXACT LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL CONTACT DIG SAFE AT 1-888-DIG-SAFE.
 - CONTRACTOR SHALL CONTACT AND COORDINATE WITH UTILITIES UPON AWARD OF THE CONTRACT. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES SO THAT TRAFFIC SIGNAL INSTALLATIONS DONE AFTER THE UTILITIES HAVE BEEN RELOCATED BY THE RESPECTIVE UTILITY COMPANIES.
 - CONTRACTOR IS RESPONSIBLE FOR OBTAINING MAINEDOT AND CITY OPENING PERMITS, IF NECESSARY.
 - IT IS THE INTENT OF THE WORK TO HAVE A COMPLETE OPERATIONAL, TESTED AND ACCEPTED TRAFFIC SIGNAL UPON COMPLETION OF THIS CONTRACT.
 - THE PROPOSED TRAFFIC SIGNAL TIMING AND PHASING SHALL BE INPUT BY THE CONTRACTOR UNDER OBSERVATION BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE 48 HOURS NOTICE TO THE ENGINEER.
 - ALL PUSH BUTTONS SHALL BE LOCATED WITHIN 1' FROM THE EDGE OF THE SIDEWALK. IF PLACED GREATER THAN 1' FROM SIDEWALK, CONTRACTOR SHALL PAVE AN APRON TO THE BUTTON FOR A DISTANCE NOT TO EXCEED 5'.
 - ALL PEDESTRIAN SIGNAL HEADS SHALL BE GELCORE MODEL NO. PS7-CFC9-01A.
 - PEDESTAL POLES SHALL BE 18" DIAMETER FOUNDATIONS.

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION
 AC-STP-1942(800)X
 WIN 19428.00

PROFESSOR OF ENGINEERING
 THOMAS A. ERICCO
 No. 6618

DATE: 02-06-2013

PORTLAND
 PARK AVENUE
 SIGNAL & SIGNING PLANS

SHEET NUMBER
 9
 OF 9

TYLIN INTERNATIONAL