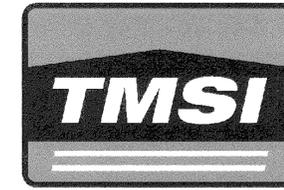


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION



**BIDDEFORD
OLD ORCHARD BEACH
SACO**

YORK

INTERSECTION OF MAIN ST. / ELM ST. (BIDDEFORD)

INTERSECTION OF HEATH ST. / FIRST ST. (OLD ORCHARD BEACH)

SCAMMON STREET (SACO)

WIN 14049.34

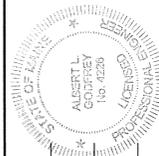
PROJECT LENGTH : 0.000 MILES

PLAN LEGEND

Town, County, State	Centerline-Existing	Centerline-Proposed
Property Lines	Travelway-Existing	Travelway-Proposed
R/W Lines-Existing	Railroad	Catch Basins
R/W Lines-Proposed	Culvert-Existing	Manholes
Culvert-Existing	Culvert Proposed	Proposed Underdrain
Culvert Proposed	Curbing Existing	Proposed Ditch
Curbing Existing	Curbing Proposed	Existing Ditch
Curbing Proposed	Type 1	Utility Poles
Type 1	Type 3	Fire Hydrants
Type 3	Type 5	Existing Water Line
Type 5	Outline of Bodies of Water	Existing San. Sewer
Outline of Bodies of Water	Ledge	Existing San. Sewer Manhole
Ledge	Buildings	Guardrail-Existing
Buildings	Trees	Guardrail-Proposed
Trees	Tree Line	Guardrail-Cable, Other
Tree Line	Clearing Limit Line	

INDEX OF SHEETS

Description	Sheet No.
Title Sheet	1
General Notes	2
Plan - Main St. / Elm St. (Biddeford)	3
Plan - Heath St. / First St. (Old Orchard Beach)	4
Plan - Scammon St. (Saco)	5



SIGNATURE: *Albert L. Godfrey*
4226
P.E. NUMBER
3-27-13
DATE

PROJECT INFORMATION	
PROGRAM	STATEWIDE MULTIMODAL PLANNING
PROJECT MANAGER	PENNY WALLACOURT
DESIGNER	AL GODFREY
CONSULTANT	TERRA MAGNA SERVICES, INC.
PROJECT RESIDENT	
CONTRACTOR	
PROJECT COMPLETION DATE	

BIDDEFORD / SACO /
OLD ORCHARD BEACH
NEW FREEDOM GRANT PROJECTS

TITLE SHEET

SHEET NUMBER
1
OF 5

PROJECT LOCATION:	Biddeford - Intersection of Main St. and Elm St. Old Orchard Beach - Intersection of First St. and Heath St. Saco - Mid-block crossing on Scammon St. 500 lf north of the Scammon St/ Elm St. intersection.
PROGRAM AREA:	Statewide Multimodal Planning
SCOPE OF WORK:	Improvement of sidewalks, ramps, crosswalks and associated amenities to provide improved handicapped pedestrian access

WIN 14049.34

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
APPROVED: *[Signature]*
COMMISSIONER: *[Signature]*
DATE: 4/11/13
CHIEF ENGINEER: *[Signature]*

Date: 4/3/2013

Username: jason

Division: HIGHWAY

Filename: ... \001_Title_01.dgn

Date: 4/14/2013

Username: jeason

Division: HIGHWAY

Filename: ... \002_GeneralNotes_01.dgn

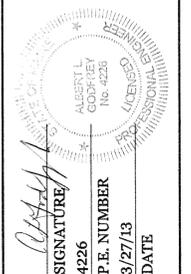
GENERAL NOTES

1. THE UTILITIES INVOLVED IN THIS CONTRACT ARE AS FOLLOWS:

CENTRAL MAINE POWER COMPANY
BOSTON & MAINE CORPORATION
BIDDEFORD & SACO WATER COMPANY
CITY OF BIDDEFORD
NORTHERN NEW ENGLAND TELEPHONE OPERATIONS, LLC.
TIME WARNER CABLE
MAINE FIBER COMPANY
OXFORD NETWORKS
UNITIL CORP.

- 2. ALL UTILITY FACILITIES SHALL BE ADJUSTED BY THE RESPECTIVE UTILITIES UNLESS OTHERWISE NOTED.
- 3. ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN WASTE AREAS APPROVED BY THE RESIDENT.
- 4. IF FOUNDATION MATERIAL IS REQUIRED UNDER CULVERTS, IT SHALL MEET THE REQUIREMENTS FOR GRANULAR BORROW - UNDERWATER BACKFILL AND WILL BE INCIDENTAL TO THE SECTION 603 PAY ITEMS.
- 5. PAVED WALKS SHALL BE CONSTRUCTED WITH 12" AGGREGATE BASE COURSE-GRAVEL, TYPE A, AND 2" HOT MIX ASPHALT UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE RESIDENT.
- 6. ANY NECESSARY CLEANING OF EXISTING PAVEMENT PRIOR TO PAVING SHALL BE INCIDENTAL TO THE RELATED PAVING ITEMS.
- 7. ANY NECESSARY CUTTING OF EXISTING CATCH BASINS TO ALLOW FOR PROPOSED PIPE CONNECTIONS WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO ITEM 603.
- 8. CATCH BASINS TO BE INSTALLED ON THIS PROJECT ARE INTENDED TO BE INSTALLED WITHOUT GRANITE CURB HEADER STONES. OFFSET FROM FACE OF CURB TO CENTER OF INLET GRATE SHALL BE 1.79'. GRADE ADJACENT NEW PAVEMENT SURFACE TO DRAIN TO GRATE.
- 9. ANY ROADWAY TRENCHING REQUIRED FOR CONNECTION OF DRAINAGE PIPE TO EXISTING CATCH BASINS OUTSIDE THE NOMINAL LIMITS OF WORK SHOWN ON THE PLANS SHALL BE REPAIRED BY REBASING WITH 24" AGGREGATE BASE COURSE-CRUSHED AND 6" HMA. PAY LIMITS WILL BE 3.5 FEET WIDE.
- 10. SAWCUTTING AND FINISHING OF CURB TYPE 1 SPECIFIED ON THE PLANS TO BE REMOVED, CUT AND RESET WILL BE CONSIDERED INCIDENTAL TO PAYMENT UNDER ITEM 609.38 RESET CURB TYPE 1.
- 11. IN AREAS WHERE CURB TYPE 1 WILL BE RESET, THE EXISTING CURB SUITABLE FOR USE AS TERMINAL ENDS SHALL BE CUT IF NECESSARY AND UTILIZED AS SUCH AND PAID FOR UNDER ITEM 609.38 (RESET CURB TYPE 1).
- 12. LOAM HAS BEEN ESTIMATED FOR DISTURBED LAWN AREAS. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS NOTED ON THE PLANS OR DESIGNATED BY THE RESIDENT.
- 13. UNLESS OTHERWISE NOTED, SEEDING METHOD NO. 1 SHALL BE UTILIZED ON ALL DISTURBED LAWNS AND DEVELOPED AREAS.
- 14. LOAM SHALL BE PLACED TO A NOMINAL DEPTH OF 4 INCHES UNLESS OTHERWISE NOTED OR DIRECTED.
- 15. ANY DAMAGE TO THE SLOPES CAUSED BY THE CONTRACTOR'S EQUIPMENT, PERSONNEL, OR OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT. ALL WORK, EQUIPMENT, AND MATERIALS REQUIRED TO MAKE REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 16. AREAS REQUIRING FILL ON THE PROJECT WILL COME FROM SUITABLE EXCAVATION FROM EXCAVATION AREAS.
- 17. NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
- 18. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION'S BEST MANAGEMENT PRACTICES FOR EROSION CONTROL & SEDIMENT CONTROL, FEBRUARY, 2008.
- 19. EXISTING TOPOGRAPHIC FEATURES SHOWN ON THE PLANS WERE DEVELOPED FROM AERIAL PHOTOGRAPHY AND FIELD REVIEW WITH APPROXIMATE MEASUREMENTS. NO THREE-DIMENSIONAL GROUND SURVEY WAS PERFORMED FOR THESE PROJECTS.
- 20. LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, APPROXIMATE SWING TIE MEASUREMENTS TO PAVEMENT MARKINGS OF APPROXIMATE UNDERGROUND UTILITY LOCATIONS PLACED IN THE FIELD BY THE UTILITIES. THIS INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CONTACT DIG SAFE AND EACH OF THE UTILITIES TO CONFIRM EXACT LOCATIONS AND DEPTHS OF UNDERGROUND UTILITIES PRIOR TO ANY EXCAVATIONS.
- 21. NO RIGHT OF WAY OR EASEMENTS HAVE BEEN ACQUIRED FOR THIS PROJECT. IF THE CONTRACTOR CHOOSES TO WORK OUTSIDE THE LIMITS SHOWN FOR ANY REASON, THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR OBTAINING OWNER PERMISSION, FOR REPAIR OF ANY DAMAGES, AND FOR ALL COSTS AND LIABILITY ASSOCIATED WITH WORKING ON PRIVATE PROPERTY.
- 22. ALL SAWCUTTING OF EXISTING PAVEMENT THAT MAY BE REQUIRED SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 23. EXISTING PAVEMENT MARKINGS TO REMAIN THAT ARE DISTURBED AS PART OF THE WORK PROCESS SHALL BE REPAINTED. PAYMENT FOR REPAIRING EXISTING MARKINGS SHALL BE MADE UNDER ITEM 627.75.
- 24. PAYMENT FOR REMOVAL OF EXISTING STRAIN POLES NOTED ON THE PLANS WILL BE INCIDENTAL. FOR STRAIN POLES IN PAVED AREAS OUTSIDE EXCAVATION LIMITS, FOUNDATIONS (IF ANY) SHALL BE REMOVED TO A DEPTH OF 12" BELOW GRADE, BACKFILLED WITH AGGREGATE BASE COURSE-CRUSHED, AND REPAVED WITH 3" HOT MIX ASPHALT. ALL WORK REQUIRED TO REMOVE STRAIN POLES AND TO MAKE NOTED REPAIRS WILL BE INCIDENTAL TO PAY ITEM 643.71.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
WIN 14049.34
HIGHWAY PLANS



DATE	3-13
BY	JLE/MSH
PROJ. MANAGER	P. VALLANCOURT
DESIGN-DETAILED	ALG
CHECKED-REVIEWED	ALG
DESIGN-DET. ALEDO	
DESIGN-DET. ALEDO	
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
FIELD CHANGES	

BIDDEFORD / SACO /
OLD ORCHARD BEACH
NEW FREEDOM GRANT PROJECTS
GENERAL NOTES

SHEET NUMBER
2
OF 5

Date: 4/8/2013

Username: jason

Filename: ...003_HDPlan BIDDEFORD MAIN ELM.dgn Division: HIGHWAY

Ⓒ INSTALL PEDESTAL POLE ON 18" FOUNDATION AT BACK OF SIDEWALK, 12" FROM MAIN ST., (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (RIGHT) SIGN. INSTALL 2" NON-METALLIC CONDUIT TO JUNCTION BOX.

Ⓓ REMOVE EXISTING PEDESTRIAN SIGNAL, PUSHBUTTON & SIGNS. INSTALL (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (LEFT) SIGN ON EXISTING SIGNAL POLE.

Ⓐ INSTALL NEW ELM STREET PEDESTAL POLE ON 18" FOUNDATION. SET CENTER 5' FROM NEW RAMP OPENING AND 3.5' BACK FROM NEW CURB. INSTALL (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (RIGHT) SIGN. INSTALL 2" NON-METALLIC CONDUIT TO JUNCTION BOX.

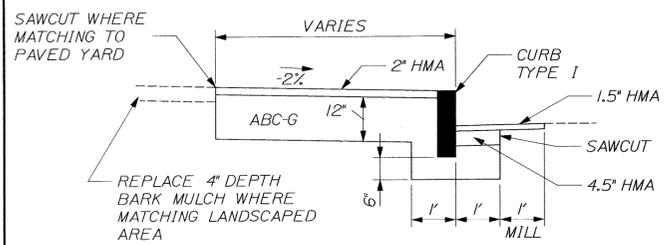
Ⓑ REMOVE PEDESTRIAN SIGNAL, PUSHBUTTON AND SIGN FROM EXISTING SIGNAL POLE. INSTALL (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (LEFT) SIGN.

Ⓔ INSTALL PEDESTAL POLE ON 18" FOUNDATION AT BACK OF SIDEWALK, (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (RIGHT) SIGN. INSTALL 2" NON-METALLIC CONDUIT TO POLE Ⓒ.

Ⓕ INSTALL PEDESTAL POLE ON 18" FOUNDATION AT 10' FROM POLE Ⓔ. (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (LEFT) SIGN. INSTALL 2" NON-METALLIC CONDUIT TO JUNCTION BOX. REMOVE EXISTING PEDESTRIAN SIGNAL, PUSHBUTTON AND SIGNS FROM VEHICULAR SIGNAL POLE.

Ⓖ REMOVE EXISTING PEDESTRIAN SIGNAL, PUSHBUTTON AND SIGN. INSTALL (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (LEFT) SIGN ON EXISTING SIGNAL POLE.

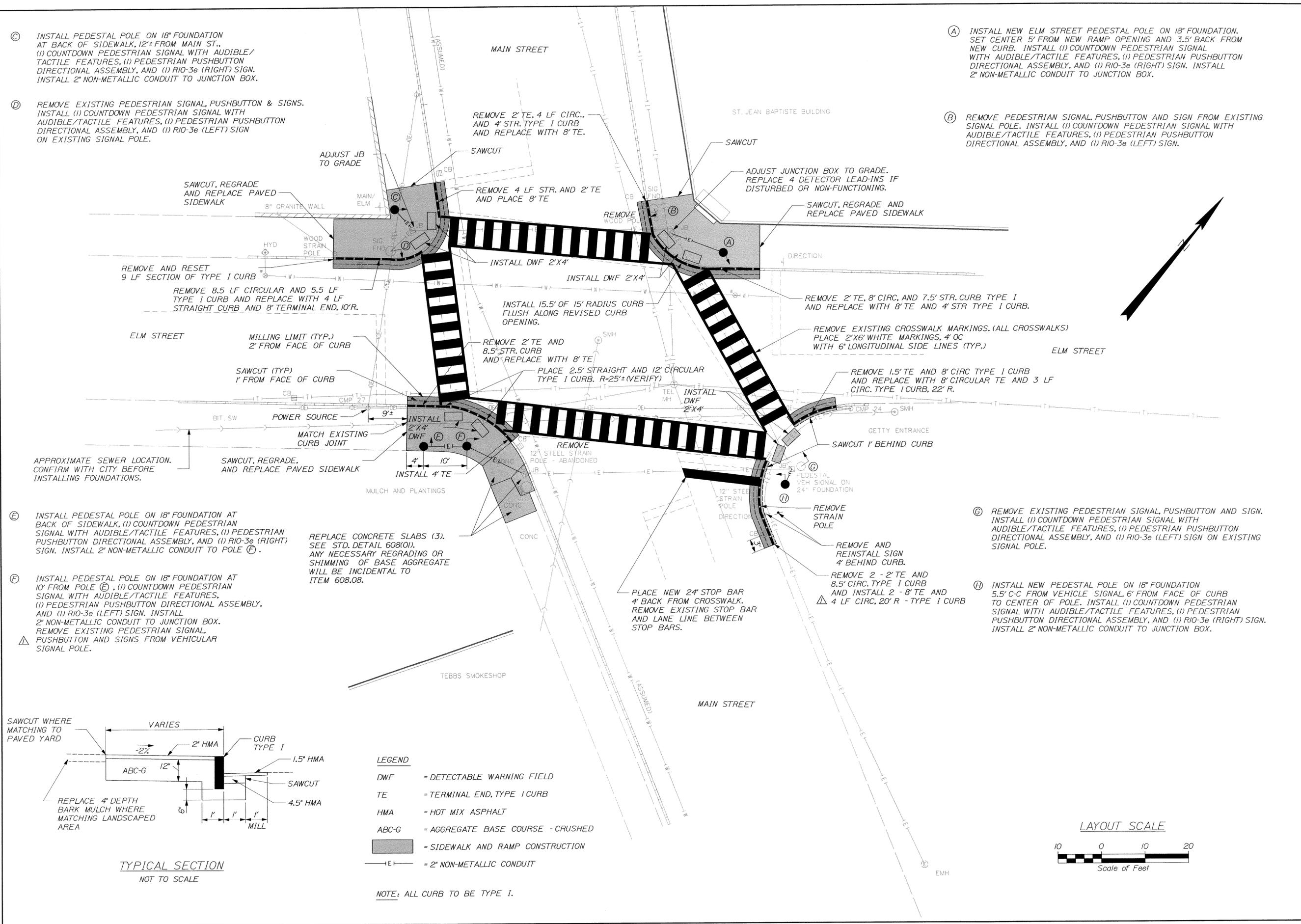
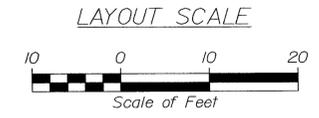
Ⓖ INSTALL NEW PEDESTAL POLE ON 18" FOUNDATION 5.5' C-C FROM VEHICLE SIGNAL, 6' FROM FACE OF CURB TO CENTER OF POLE. INSTALL (1) COUNTDOWN PEDESTRIAN SIGNAL WITH AUDIBLE/TACTILE FEATURES, (1) PEDESTRIAN PUSHBUTTON DIRECTIONAL ASSEMBLY, AND (1) RIO-3e (RIGHT) SIGN. INSTALL 2" NON-METALLIC CONDUIT TO JUNCTION BOX.



TYPICAL SECTION
NOT TO SCALE

- LEGEND**
- DWF = DETECTABLE WARNING FIELD
 - TE = TERMINAL END, TYPE I CURB
 - HMA = HOT MIX ASPHALT
 - ABC-G = AGGREGATE BASE COURSE - CRUSHED
 - [Hatched Area] = SIDEWALK AND RAMP CONSTRUCTION
 - [Line with 'E'] = 2" NON-METALLIC CONDUIT

NOTE: ALL CURB TO BE TYPE I.



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
WIN 14049.34
HIGHWAY PLANS

SIGNATURE: [Signature]
DATE: March 28, 2013

PROJ. MANAGER	P. VALLANCOURT	DATE	BY
DESIGN-DETAILED	AIG	3-13	J.E.
CHECKED-REVIEWED	AIG	3-13	
DESIGN-DETAILED			
REVISIONS	1	4-8-13	
REVISIONS	2		
REVISIONS	3		
REVISIONS	4		
FIELD CHANGES			

BIDDEFORD
MAIN ST. / ELM ST.
PLANS

SHEET NUMBER
3
OF 5

Date: 4/1/2013

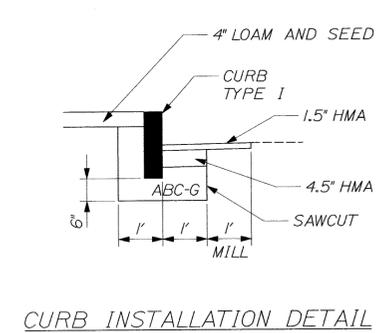
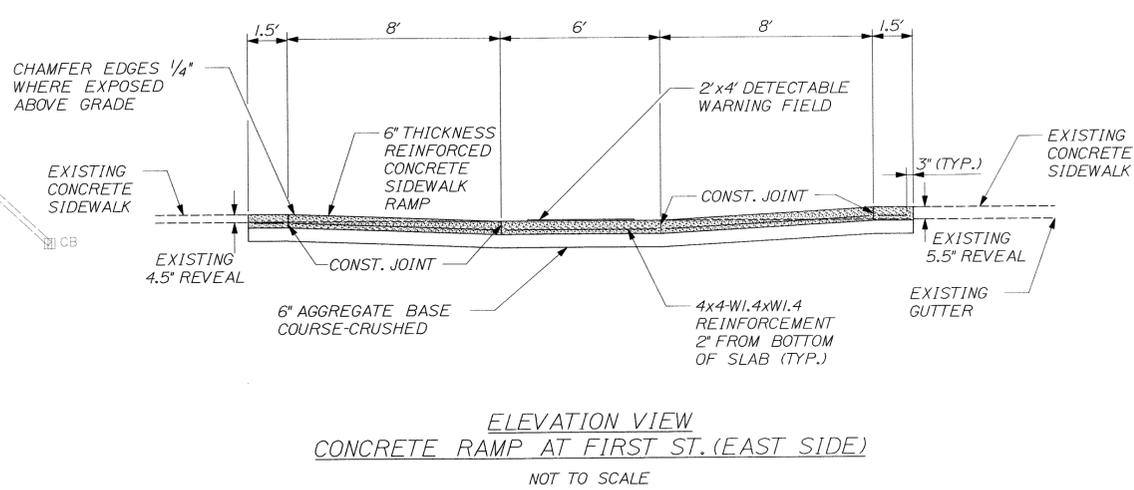
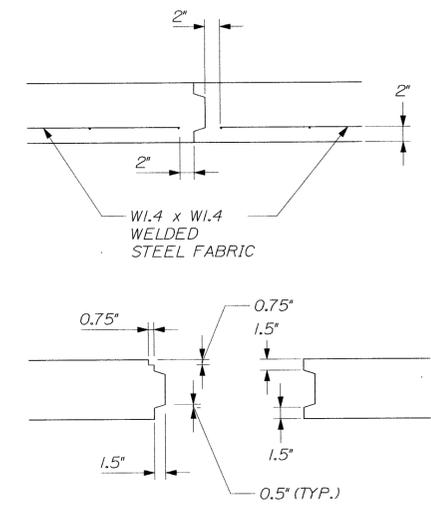
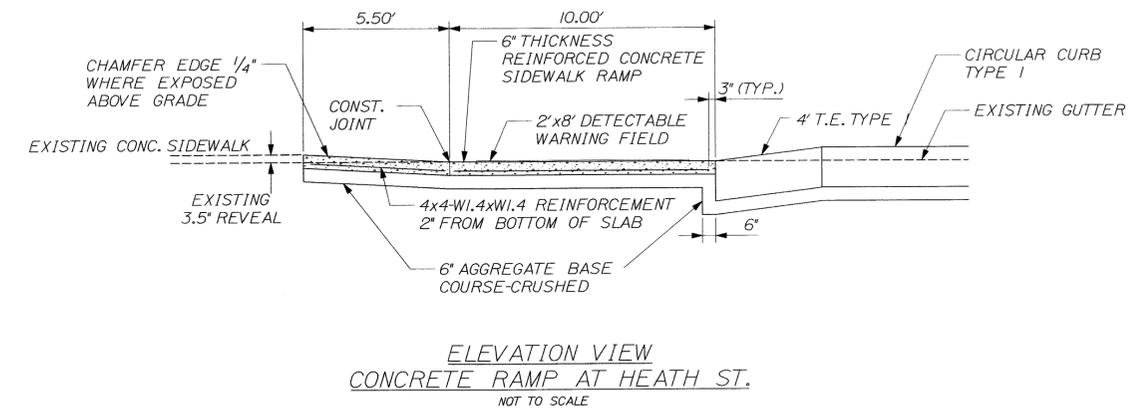
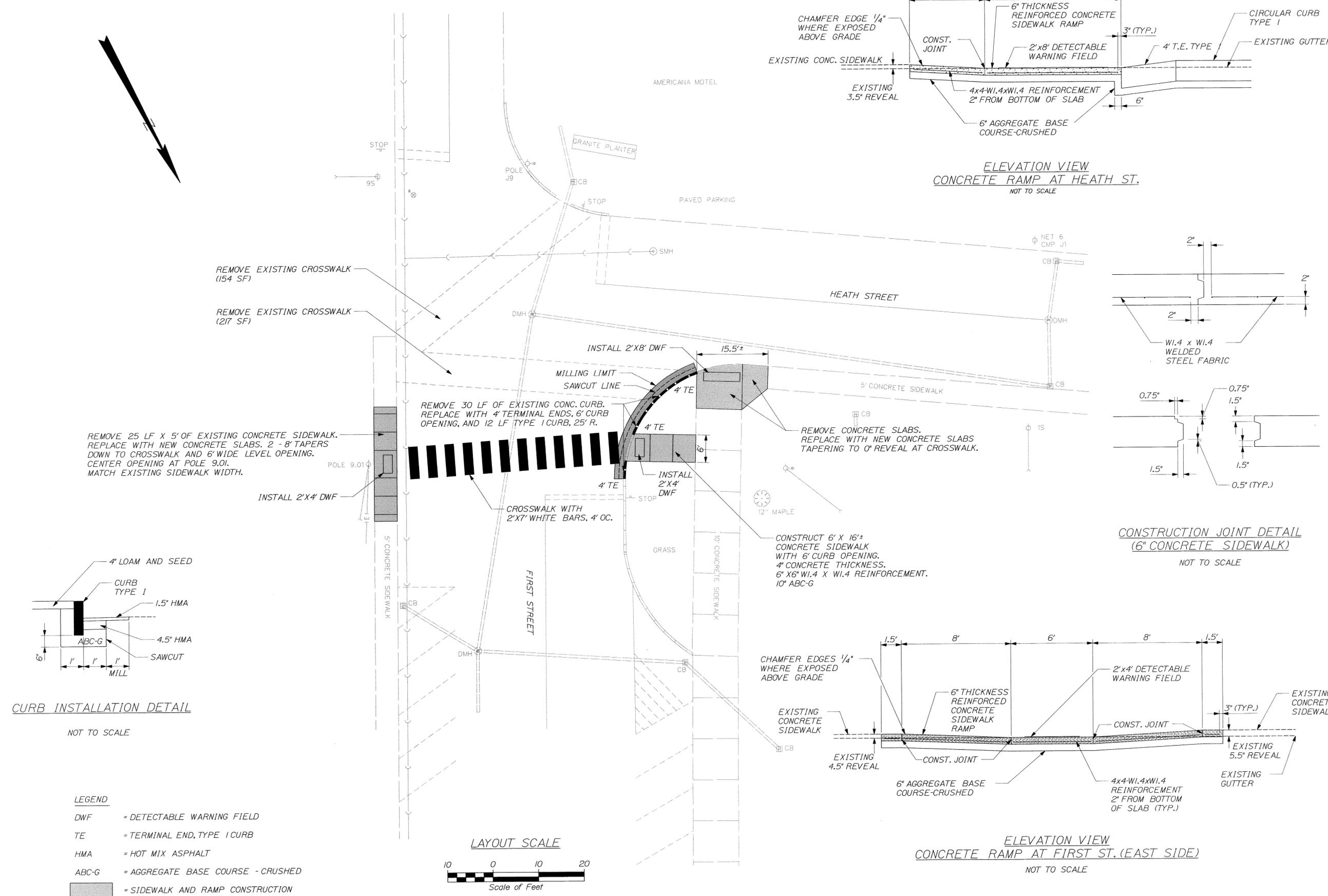
Username: jason

Filename: ...004_HDPlan_00B_Heath and First.dgn Division: HIGHWAY

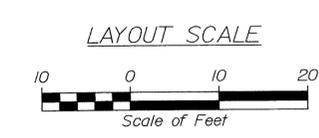
STATE OF MAINE
ALBERT L. GODFREY
16-1423
P.E. NUMBER
SIGNATURE
DATE
MARCH 28, 2013

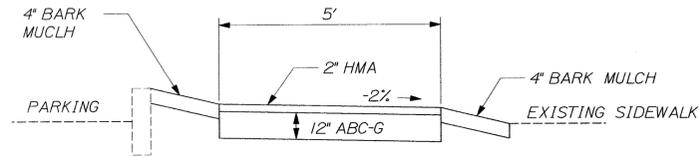
DATE	BY	REVISIONS
3-13	JLE / VSR	DESIGN-DETAILED
3-13	JLE / VSR	CHECKED-REVIEWED
		DESIGN-DETAILED
		DESIGN-DETAILED
		REVISIONS 1
		REVISIONS 2
		REVISIONS 3
		REVISIONS 4
		FIELD CHANGES

OLD ORCHARD BEACH
HEATH ST / FIRST ST
PLANS

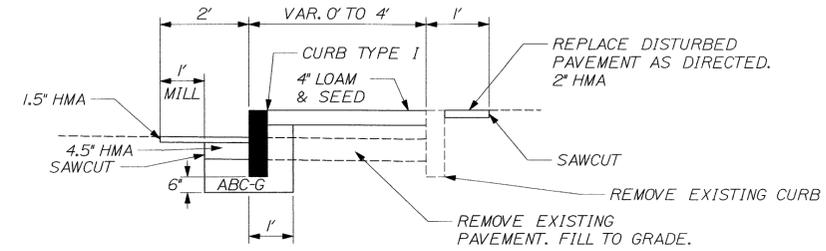


- LEGEND**
- DWF = DETECTABLE WARNING FIELD
 - TE = TERMINAL END, TYPE I CURB
 - HMA = HOT MIX ASPHALT
 - ABC-G = AGGREGATE BASE COURSE - CRUSHED
 - [Hatched Box] = SIDEWALK AND RAMP CONSTRUCTION

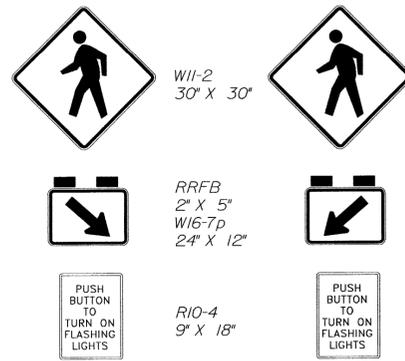




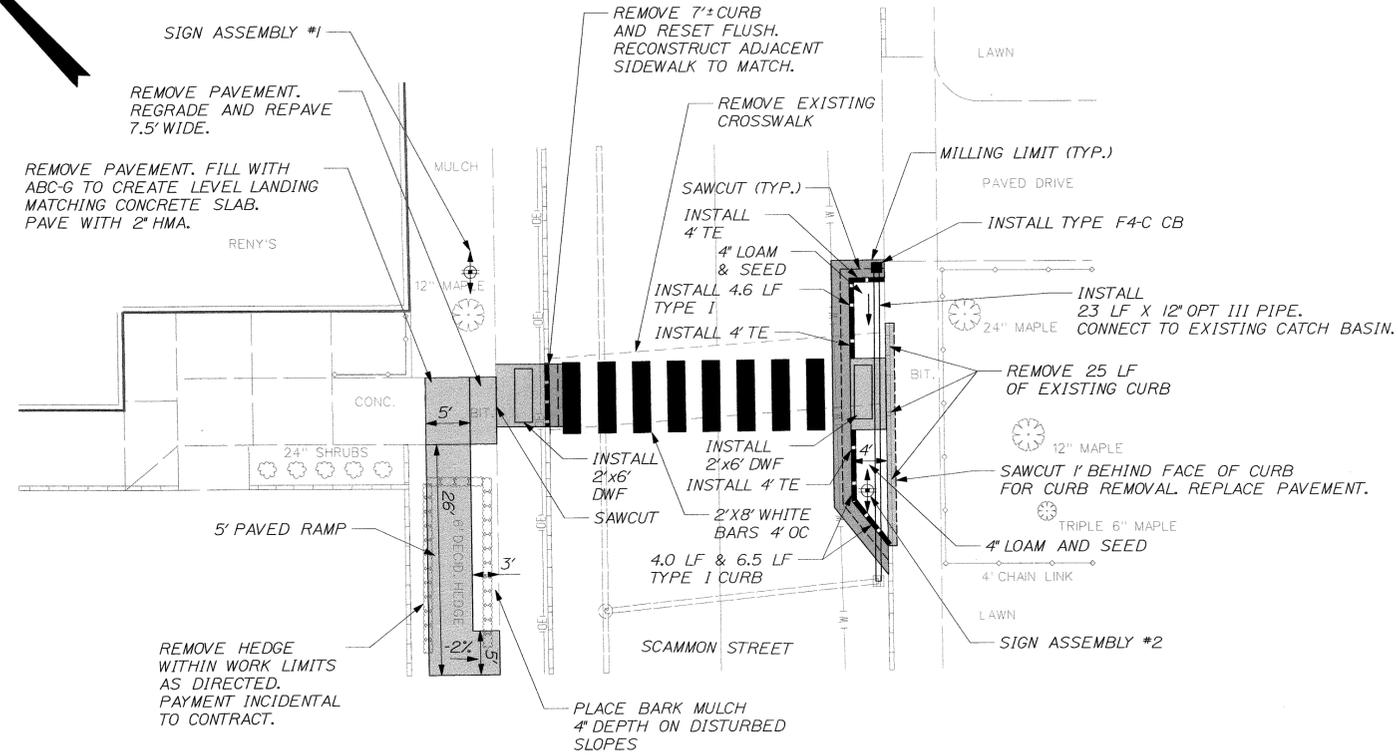
RAMP TYPICAL SECTION
NOT TO SCALE



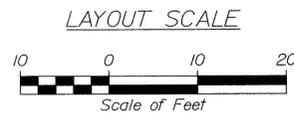
BUMP-OUT TYPICAL SECTION
NOT TO SCALE



SIGN ASSEMBLY DETAIL
(SIX SIGNS TOTAL AT EACH ASSEMBLY LOCATION)



- LEGEND
- DWF = DETECTABLE WARNING FIELD
 - TE = TERMINAL END, TYPE I CURB
 - HMA = HOT MIX ASPHALT
 - ABC-G = AGGREGATE BASE COURSE - CRUSHED
 - [Shaded Box] = SIDEWALK AND RAMP CONSTRUCTION



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
WIN 14049.34

PROF. MANAGER P VALLANCOURT
DESIGNED/REVISED A/G
CHECKED/REVISED A/G
DESIGNED/FILED J/E
REVISIONS 1
REVISIONS 2
REVISIONS 3
REVISIONS 4
FIELD CHANGES

DATE 3-13
BY J/E

SIGNATURE
4226
P.E. NUMBER
March 28, 2013
DATE

DATE	BY	PROF. MANAGER P VALLANCOURT	DESIGNED/REVISED A/G	CHECKED/REVISED A/G	DESIGNED/FILED J/E	REVISIONS 1	REVISIONS 2	REVISIONS 3	REVISIONS 4	FIELD CHANGES
3-13	J/E									

SACO STREET
SCAMMON STREET
PLANS

SHEET NUMBER
5
OF 5

Date: 4/1/2013
Username: jason
Filename: ... \005_HDPlan_SACO_SCAMMON STREET.dgn Division: HIGHWAY