

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



DEER ISLE-SEDGWICK HANCOCK COUNTY DEER ISLE-SEDGWICK BRIDGE OVER EGGEMOGGIN REACH STATE ROUTE 15 PROJECT LENGTH 0.539 mi. BRIDGE MAINTENANCE REPAIRS BRIDGE NO. 3257

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SPECIFICATIONS

Design: Load Factor Design per AASHTO Standard Specifications for Highway Bridges, 17th Edition 2002 and Interim Specifications through March 2005.

DESIGN LOADING

Live LoadH20
Site Specific Load Posting

TRAFFIC DATA

Current (2005) AADT	3,470
Future (2025) AADT	5,210
DHV - % of AADT	12%
Design Hour Volume	625
Heavy Trucks (% of AADT)	7%
Heavy Trucks (% of DHV)	5%
Directional Distribution (% of DHV)	50%
18 kip Equivalent P 2.0	104
18 kip Equivalent P 2.5	100
Design Speed (mph)	25

HYDROLOGIC DATA*

Mean Higher High Water (MHHW)	6.4 ft
Mean High Water (MHW)	5.3 ft
Mean Tide Level (MTL)	0.3 ft
Mean Low Water (MLW)	-4.7 ft
Mean Lower Low Water (MLLW)	-5.1 ft

* Elevations based on NVGD 29.

MATERIALS

Structural Steel:	
AASHTO M 270 (ASTM A 709, Grade 36)	$F_y = 36,000$ psi
AASHTO M 164 (ASTM A 325)	$F = 120,000$ psi
Bronze Bushings:	
ASTM B 22 Alloy UNS C91300	$F_A = 3,000$ psi

UTILITIES

Bangor Hydro-electric Company
FairPoint Communications
FHWA Aerodynamics Laboratory
Genex Systems
MaineDOT Bridge Maintenance
MaineDOT Electrical Department
Time Warner Cable

MAINTENANCE OF TRAFFIC

A minimum of one lane of traffic 10'-0" wide, using flaggers, shall be maintained during work hours throughout construction. The Contractor shall remove all lane closures and open the bridge to unrestricted traffic for a minimum of one 2-hour period on a minimum of one weekday (Monday - Friday) each week, between the hours of 7:00 am and 5:00 pm.

PROJECT LOCATION:	Deer Isle-Sedgewick Bridge which carries State Route 15 over Eggemoggin Reach. Lat./Long. 44°17'39" N 68°41'19" W
PROGRAM AREA:	Bridge Program
OUTLINE OF WORK:	Bridge Superstructure Rehabilitation consisting of structural steel, stay cable and bearing repairs.

WIN 10062.10

HP-1006(210)

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

APPROVED: *[Signature]*
COMMISSIONER: *[Signature]*
DATE: 5/9/13
CHIEF ENGINEER: *[Signature]* 5/8/13



Evan Lowell
SIGNATURE
9065
P.E. NUMBER
May 6, 2013
DATE

PROGRAM	BRIDGE
PROJECT MANAGER	S. BOGGE
DESIGNER	E. LOWELL
CONSULTANT	TRANSYSTEMS
PROJECT RESIDENT	
CONTRACTOR	
PROJECT COMPLETION DATE	

DEER ISLE-SEDGWICK
DEER ISLE-SEDGWICK BRIDGE
TITLE SHEET

SHEET NUMBER
1
OF 19



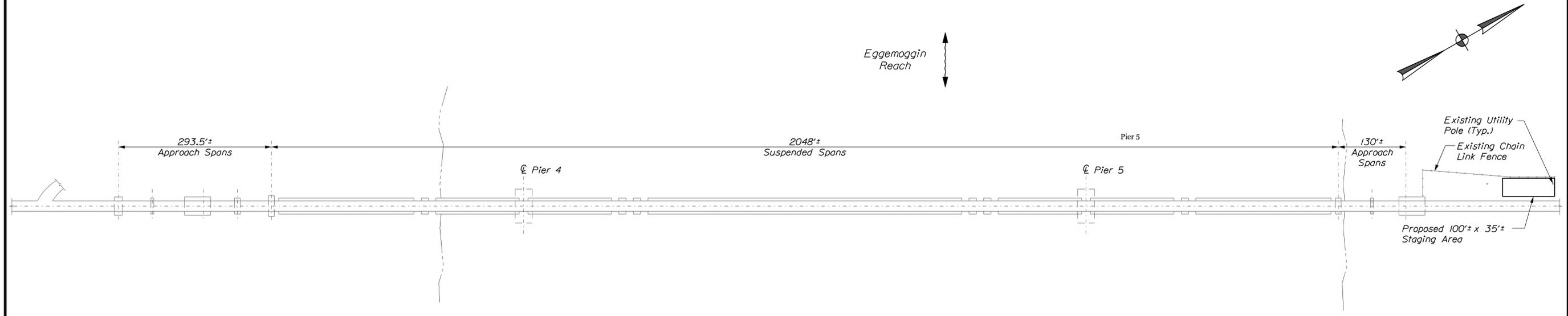
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Date: 5/7/2013

Date: 5/7/2013

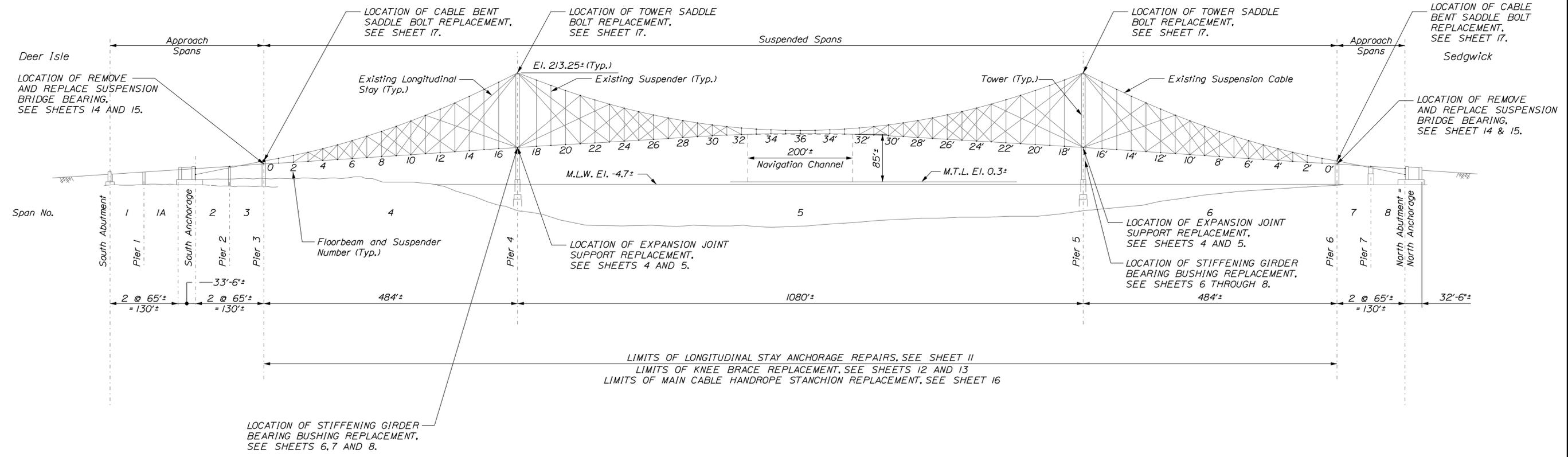
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Division: BRIDGE

Filename: ... \Design Plans\003_GPE.dgn



GENERAL PLAN



GENERAL ELEVATION

NOTES:

1. FOR GENERAL NOTES SEE SHEET 2.
2. TENSIONING TRANSVERSE STAY CABLES TO BE PERFORMED AT SUSPENDER LOCATIONS 10, 11, 23, 24, 25, 25', 24', 23', 11' AND 10'. SEE SHEETS 9 AND 10.
3. FOR TRAFFIC MANAGEMENT PLAN SEE SHEETS 18 AND 19.

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION
 HP-1006(210)
 WIN
 10062.10
 BRIDGE NO. 3257
 BRIDGE PLANS

PROJ. MANAGER	DATE	BY	DATE
E. LOWELL	4/24/13	E. LOWELL	4/24/13
S. JAHNES	4/24/13	E. LOWELL	4/24/13
S. JAHNES	4/24/13	S. JAHNES	4/24/13

DESIGN	DATE	BY	DATE
DESIGN DETAILED	4/24/13	S. JAHNES	4/24/13
CHECKED/REVIEWED	4/24/13	E. LOWELL	4/24/13
DESIGN DETAILED	4/24/13	S. JAHNES	4/24/13
REVISIONS			
FIELD CHANGES			

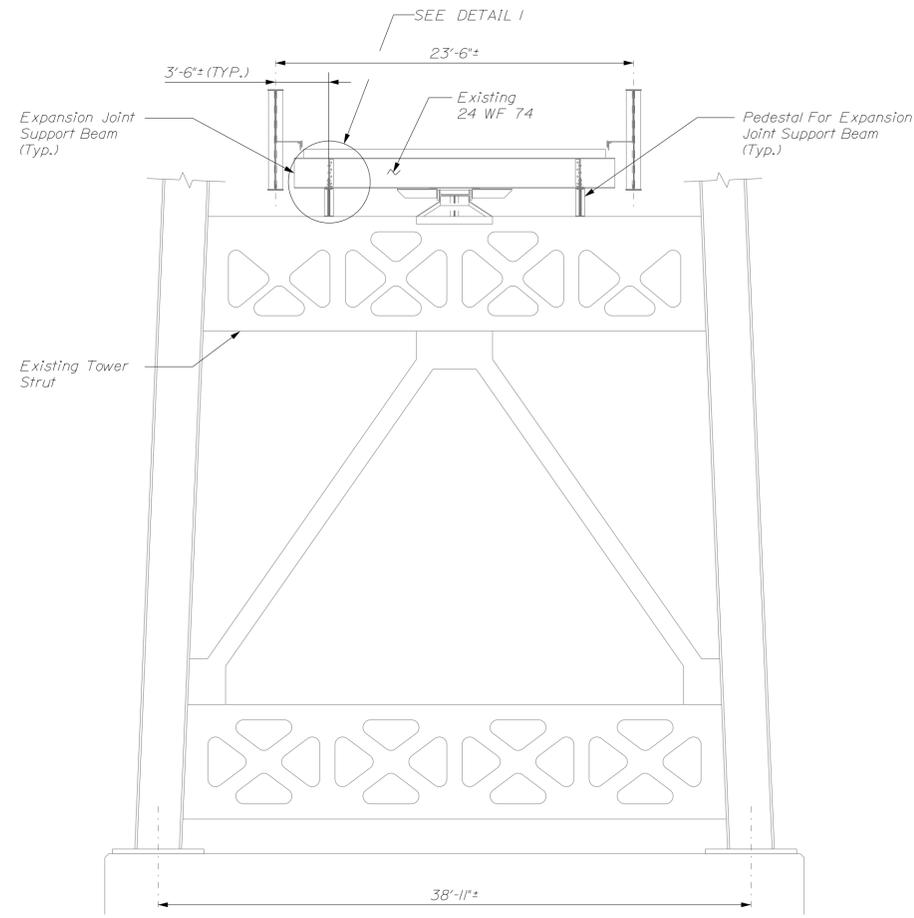
DEER ISLE-SEDGWICK BRIDGE
 EGGEMOGGIN REACH
 DEER ISLE-SEDGWICK HANCOCK COUNTY
 GENERAL PLAN & ELEVATION

SHEET NUMBER

3

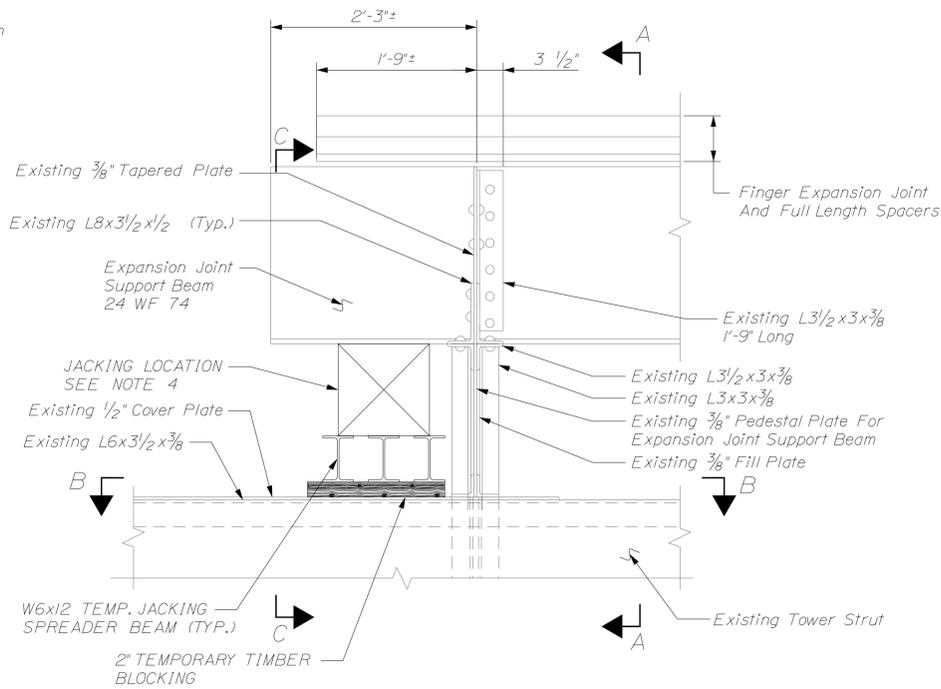
OF 19





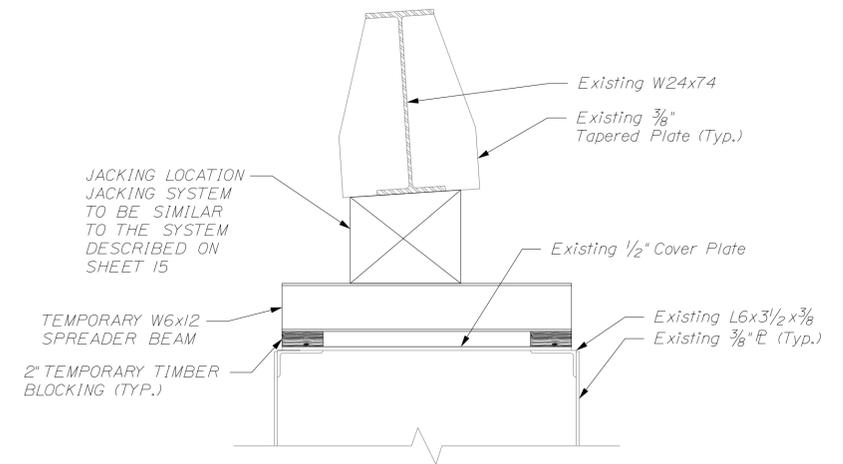
NOTE:
EXISTING MAINTENANCE CATWALK NOT SHOWN FOR CLARITY. SEE SPECIAL PROVISIONS.

SOUTH ELEVATION, MAIN TOWER AT PIER 4
(MAIN TOWER AT PIER 5 SIMILAR)



DETAIL I - EXISTING

(SOUTH ELEVATION OF WEST PEDESTAL SUPPORT CONNECTION AT PIER 4 SHOWN, EAST PEDESTAL SUPPORT CONNECTION OPPOSITE HAND) (PEDESTAL SUPPORTS AT PIER 5 SIMILAR)



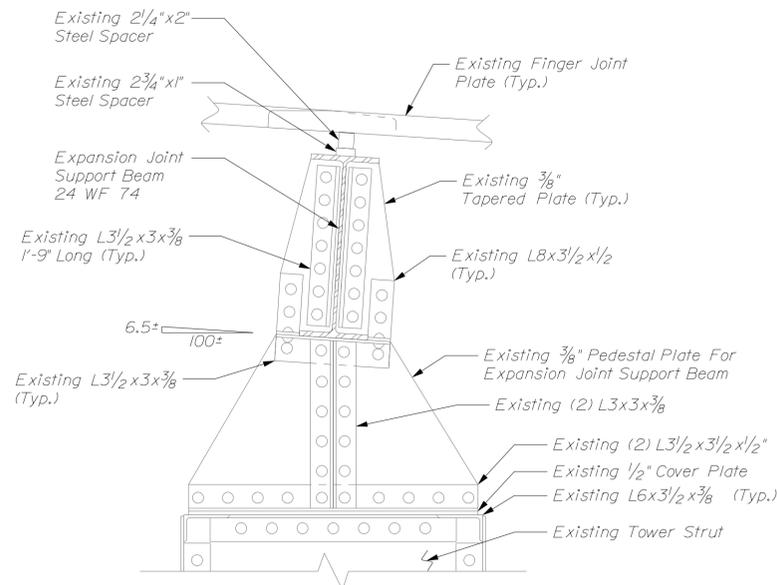
SECTION C-C - JACKING SUPPORT

NOTES:

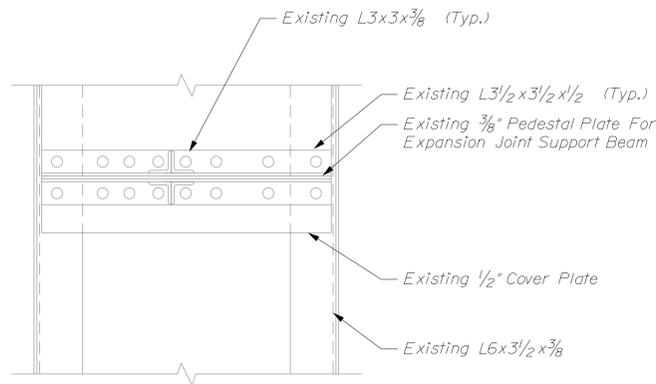
- FOR GENERAL NOTES SEE SHEET SEE SHEET 2.
- THE EXISTING EXPANSION JOINT SUPPORT BEAM PEDESTAL SHALL NOT BE REMOVED UNTIL ALL MATERIALS ARE ON-SITE FOR THE REPLACEMENT OF THE PEDESTAL.
- THE TRAVEL LANE ABOVE THE REPAIR LOCATION SHALL BE CLOSED TO ALL TRAFFIC DURING THE COURSE OF THE WORK.
- THE ANTICIPATED JACKING LOAD IS 8 KIPS. THE ANTICIPATED MAXIMUM LOAD TO BE CARRIED BY THE TEMPORARY SUPPORT IS 20 KIPS. ALL COMPONENTS OF THE TEMPORARY SUPPORT SHALL HAVE A MINIMUM SAFETY FACTOR OF 2.0. SEE SHEET 15 FOR REMAINING JACKING NOTES.
- ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.8302.

SUGGESTED JACKING AND TEMPORARY SUPPORT SEQUENCE:

- INSTALL A LANE CLOSURE ABOVE THE EXPANSION JOINT SUPPORT PEDESTAL TO BE REPLACED.
- LOCATE ALL CONTRACTOR WORK VEHICLES A MINIMUM OF 20 FEET FROM THE FINGER JOINT.
- INSTALL TEMPORARY SPREADER BEAMS, TIMBER BLOCKING, JACKING ASSEMBLY AND TEMPORARY SUPPORT FOR THE EXPANSION JOINT SUPPORT BEAM ON TOP OF THE TOWER STRUT.
- CLOSE THE BRIDGE TO ALL TRUCK TRAFFIC.
- JACK AND BLOCK THE EXPANSION JOINT SUPPORT BEAM TO REMOVE ALL LOAD FROM THE PEDESTAL TO BE REPLACED. ONCE SUPPORT BEAM IS BLOCKED, TRAFFIC WILL BE ALLOWED IN THE ADJACENT LANE.
- REMOVE EXISTING PEDESTAL COMPONENTS.
- CLEAN AND PRIME COAT EXISTING STEEL WITHIN WORK AREA.
- INSTALL NEW PEDESTAL COMPONENTS, AND SNUG TIGHT ALL NEW HIGH STRENGTH BOLTS.
- RELEASE THE JACK.
- TIGHTEN ALL NEW BOLTS.
- REMOVE THE JACKING ASSEMBLY AND TEMPORARY SUPPORT.
- PAINT ALL STEEL WITHIN WORK AREA.
- REOPEN THE BRIDGE TO ALL TRAFFIC.



SECTION A-A - EXISTING



SECTION B-B - EXISTING

PROJ. MANAGER	DATE	BY	DATE	SIGNATURE	P.E. NUMBER	DATE
E. LOWELL	4/24/13	S. JAHNES	4/24/13			
E. LOWELL	4/24/13	T. STUOPS	4/24/13			
R. BACK	4/24/13	S. JAHNES	4/24/13			
DESIGN DETAILED						
DESIGN REVIEWED						
DESIGN APPROVED						
REVISIONS 1						
REVISIONS 2						
REVISIONS 3						
REVISIONS 4						
FIELD CHANGES						

DEER ISLE-SEDGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY
EXPANSION JOINT SUPPORT
PEDESTAL REPLACEMENT 1 OF 2

SHEET NUMBER

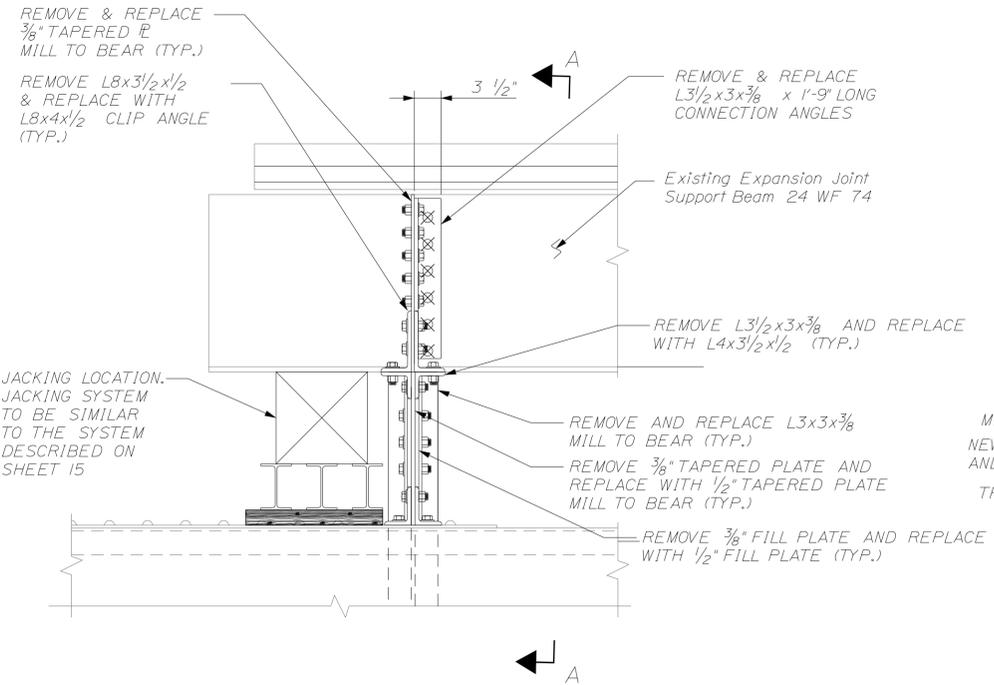
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Date: 5/7/2013

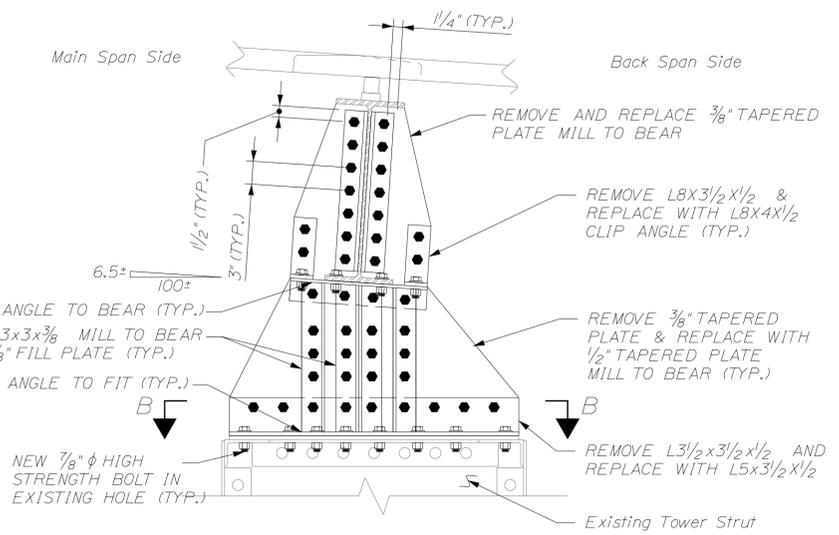
Username: S JAHNES

Division: BRIDGE

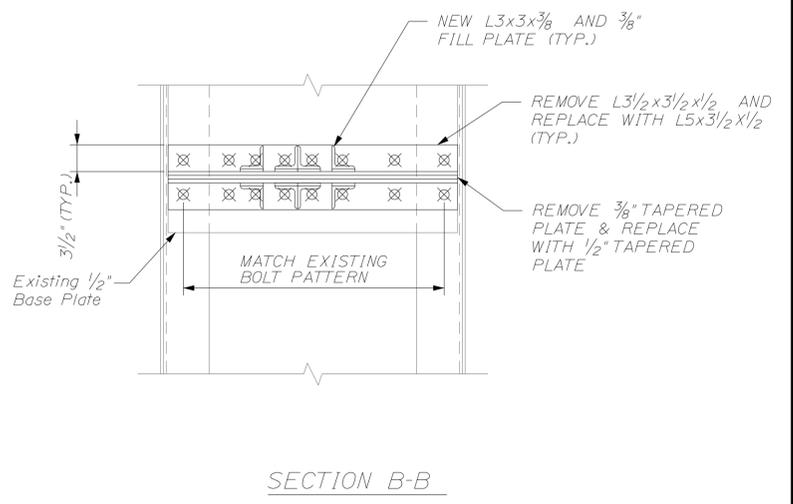
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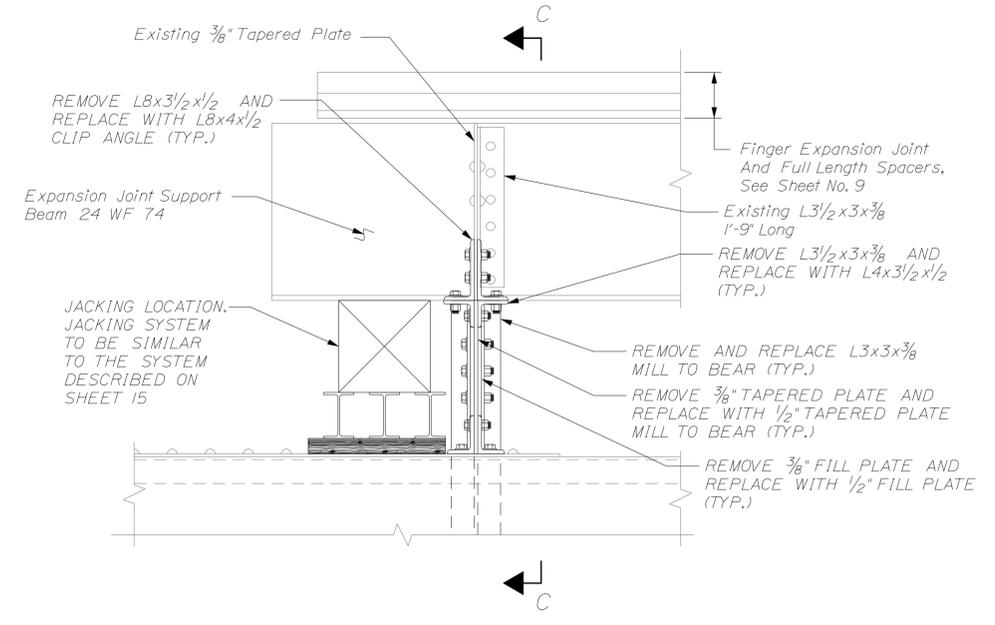
DETAIL I - PROPOSED PIER 5 EXPANSION JOINT SUPPORT BEAM STIFFENER AND PEDESTAL REPLACEMENT
(SOUTH ELEVATION OF WEST PEDESTAL SHOWN, EAST PEDESTAL SUPPORT CONNECTION OPPOSITE HAND)



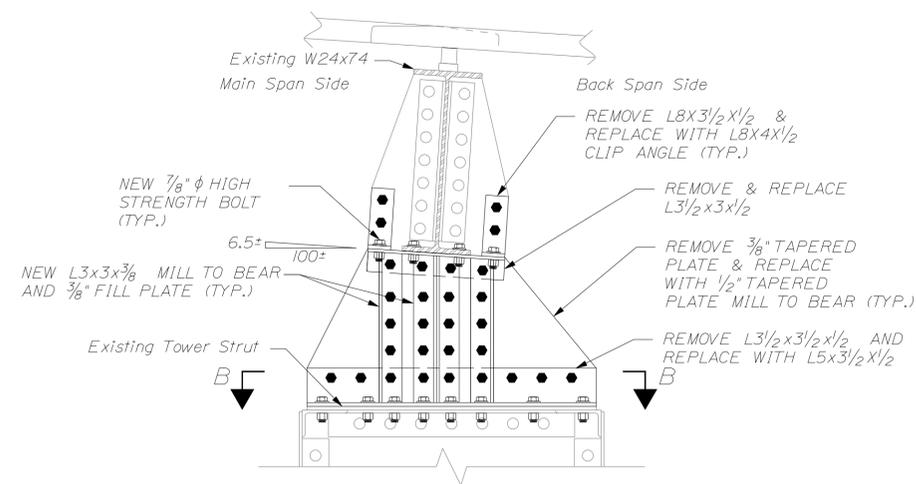
SECTION A-A



SECTION B-B



DETAIL I - PROPOSED PIER 4 EXPANSION JOINT SUPPORT BEAM PEDESTAL REPLACEMENT
(SOUTH ELEVATION OF WEST PEDESTAL SHOWN, EAST PEDESTAL SUPPORT CONNECTION OPPOSITE HAND)



SECTION C-C - PROPOSED PIER 4 PEDESTAL REPAIR

WEST ELEVATION OF THE EXPANSION JOINT SUPPORT PEDESTALS AT PIER 4 (SOUTH TOWER) SHOWN.

NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.8302.

LEGEND	
SYM.	DESCRIPTION
○	EXISTING FASTENER
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH BOLT IN EXISTING HOLE
●	PLACE NEW HIGH STRENGTH BOLT IN NEW HOLE

STATE OF MAINE DEPARTMENT OF TRANSPORTATION HP-1006(210)	BRIDGE NO. 3257 WIN 10062.10 BRIDGE PLANS
DEER ISLE-SEDGWICK BRIDGE EGGEMOGGIN REACH DEER ISLE-SEDGWICK HANCOCK COUNTY	EXPANSION JOINT SUPPORT PEDESTAL REPLACEMENT 2 OF 2
5	SHEET NUMBER

TEMPORARY SUPPORT NOTES:

TIMBER

- ALL TIMBER SHALL BE SQUARE AND FREE OF LOOSE KNOTS, DECAY AND SPLITS.
- HARDWOOD MAY BE USED FOR TIMBER NOT IN DIRECT CONTACT WITH SLINGS. HARDWOOD MAY BE USED FOR ALL TIMBER IF A 1"4" RUBBER MAT IS USED BETWEEN THE SLINGS AND THE TIMBER.
- TIMBER IN DIRECT CONTACT WITH SLINGS SHALL BE DOUGLAS FIR-LARCH COMMERCIAL GRADE NO. 2; MIXED SOUTHERN PINE COMMERCIAL GRADE NO. 2 OR OTHER TIMBER WITH A COMPRESSIVE STRENGTH PARALLEL TO THE GRAIN EQUAL TO OR GREATER THAN 300 PSI.
- THE TIMBER SHALL BE CUT, PLANED, OR ROUTED TO FIT AND BE IN FULL BEARING AT ALL LOCATIONS AS SHOWN IN THE DESIGN SKETCHES AND AS DIRECTED BY THE ENGINEER. METAL SHIMS (1/16") SHALL BE USED AS REQUIRED TO ENSURE FULL BEARING, BUT SHALL NOT BE IN DIRECT CONTACT WITH THE SLINGS OF THE JACKING ASSEMBLY.

RIGGING (SEE TABLE FOR RIGGING ITEMS TO BE SUPPLIED BY MAINEDOT)

- SLINGS DESIGNATED IN THE ATTACHED DESIGN SKETCHES SHALL BE TUFLEX ENDLESS ROUNDSLINGS PART NO. EN150 WITH A WORKING LOAD CAPACITY OF 13.2 KIPS FOR VERTICAL HITCH AND 26.4 KIPS FOR BASKET HITCH OR APPROVED EQUAL. SLINGS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND THE ATTACHED SKETCHES.
- THE SHACKLES PROVIDED SHALL BE 17 TON CROSBY G-213 FORGED SHACKLES OR APPROVED EQUAL. SHACKLES SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND THE ATTACHED SKETCHES.
- RATCHETS SHALL BE 1 3/8" PATTERSON STEAMBOAT RATCHETS TYPE ASA ANGLE-SHANK WITH A PELICAN-HOOK ON EACH END AND A MINIMUM BREAKING STRENGTH OF 85 KIPS OR APPROVED EQUAL. RATCHETS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND THE ATTACHED SKETCHES.
- ALL RIGGING COMPONENTS SHALL BE IN SOUND WORKING ORDER AND NOT HAVE BEEN SUBJECTED TO LOADS ABOVE THE MANUFACTURER'S RECOMMENDATIONS.
- ALL RIGGING COMPONENTS SHALL BE INSPECTED PRIOR TO INSTALLING THE TEMPORARY SUPPORT AT EACH LOCATION. COMPONENTS SHOWING SIGNS OF WEAR OR DAMAGE SHALL NOT BE USED.
- ALL RIGGING COMPONENTS SHALL BE USED ONLY AS DESIGNATED BY THE MANUFACTURER.
- LOAD ON SHACKLES SHALL BE CENTRALIZED BY USE OF WASHERS OR SIMILAR METHOD.
- SLINGS SHALL BE BLOCKED WITH TIMBER AT SHARP EDGES.
- CLAMPS OR SIMILAR METHODS SHALL BE USED AS REQUIRED TO SECURE BLOCKING AND PREVENT SLIPPAGE OF LOAD.

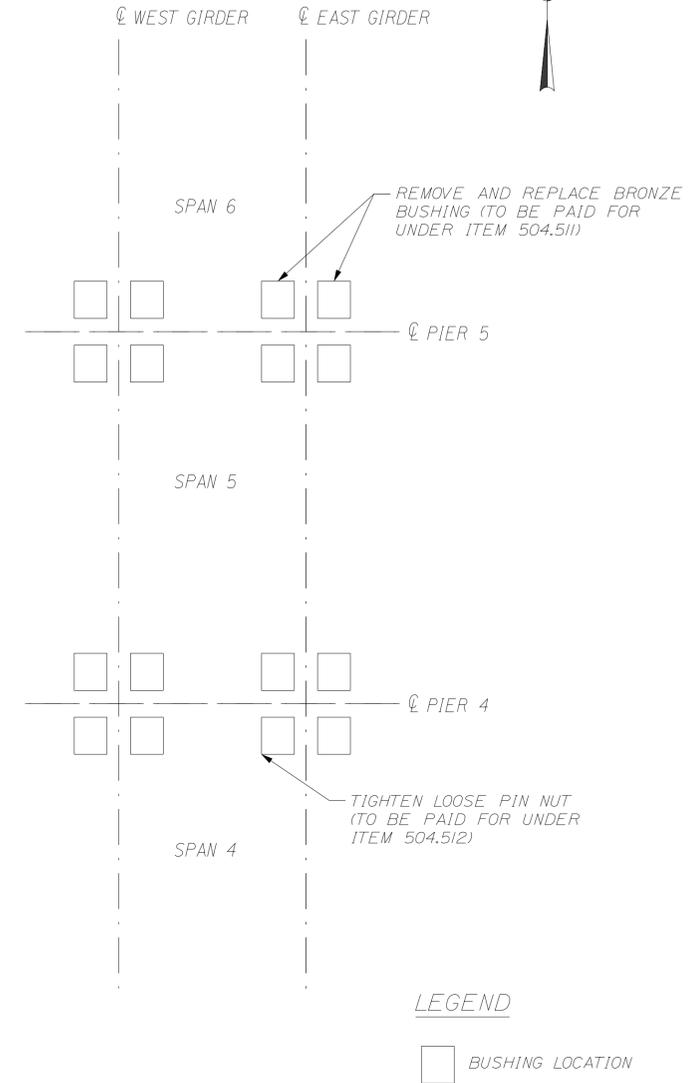
SUGGESTED TEMPORARY SUPPORT SEQUENCE

- THE BUSHING REPLACEMENT OPERATION SHALL OCCUR AT ONLY ONE BEARING AT A TIME.
- PROVIDE A TEMPORARY LANE CLOSURE ABOVE THE REPAIR AREA AND REMOVE ALL TRUCK TRAFFIC FROM THE BRIDGE.
- REMOVE SAFETY WALK GRATING AS NECESSARY TO PROVIDE ACCESS TO THE WORK AREA AND TO INSTALL THE TEMPORARY SUPPORT.
- INSTALL TEMPORARY SUPPORT ASSEMBLY.
- REMOVE ALL WORK VEHICLES FROM THE LANE CLOSURE IN THE SPAN WITH THE TEMPORARY SUPPORT.
- JACK ONE END OF THE TRUSS BEAM TO REMOVE ALL LOAD FROM THE PIN TO ALLOW FOR THE REMOVAL OF THE EXISTING BUSHINGS AND INSTALLATION OF NEW BUSHINGS.
- REPLACE BUSHINGS (SEE SUGGESTED BUSHING REPLACEMENT SEQUENCE).
- RELEASE THE JACKS.
- REMOVE THE TEMPORARY SUPPORT ASSEMBLY AND REPLACE THE SAFETY WALK GRATING.
- REMOVE THE TEMPORARY LANE CLOSURE AND REOPEN THE BRIDGE TO ALL TRAFFIC.

SUGGESTED BUSHING REPLACEMENT SEQUENCE (1 LOCATION, 2 BUSHINGS PER LOCATION)

- REMOVE ALL LOAD FROM THE BEARING PIN (SEE SUGGESTED TEMPORARY SUPPORT SEQUENCE).
- REMOVE THE COTTER PIN AND LOOSEN THE STEEL NUTS OF THE BEARING PIN.
- REMOVE THE EXISTING BUSHING(S) FROM THE BEARING HOUSING.
- CLEAN ACCUMULATED GREASE, PAINT, AND DEBRIS FROM THE PIN, PIN PLATE AND SLIDING SURFACE OF THE BEARING HOUSING.
- RESIDENT TO INSPECT THE PIN, PIN PLATES, AND SLIDING SURFACE OF THE BEARING HOUSING FOR SIGNS OF WEAR OR DAMAGE. DOCUMENT INSPECTION FINDINGS.
- INSTALL NEW BUSHING(S) ON THE BEARING PIN IN THE BEARING HOUSING.
- SECURE NEW BUSHING(S) WITH NUT AND COTTER PIN.
- GREASE THE NEW BUSHING(S).
- PLACE LOAD ON THE PIN.

RIGGING ITEM	QUANTITY TO BE SUPPLIED BY MAINEDOT (SEE NOTE 2)
6' TUFLEX ENDLESS ROUNDSLING - EN150	2
7' TUFLEX ENDLESS ROUNDSLING - EN150	2
8' TUFLEX ENDLESS ROUNDSLING - EN150	10
1 1/2" CROSBY FORGED SHACKLES	16
1 3/8" PATTERSON STEAMBOAT RATCHET (24" BARREL)	8



STIFFENING GIRDER BEARING REPAIR LOCATIONS

NOTES

- FOR GENERAL NOTES SEE SHEET 2.
- THE TEMPORARY SUPPORT MATERIALS SUPPLIED BY THE DEPARTMENT WILL BE AVAILABLE AT THE HANCOCK, MAINEDOT MAINTENANCE LOT. JEFF NAUM, MDOT BRIDGE MAINTENANCE 207-446-1235 SHALL BE CONTACTED A MINIMUM OF 3 DAYS IN ADVANCE OF THE DATE THE CONTRACTOR WILL RETRIEVE THE MATERIALS. THE CONTRACTOR IS RESPONSIBLE FOR LOADING, DELIVERY AND RETURNING THE MATERIALS.
- THE TRAVEL LANE ABOVE THE REPAIR LOCATION SHALL BE CLOSED TO ALL TRAFFIC.
- ACCESS VEHICLES AND CONSTRUCTION VEHICLES USED DURING THE REPLACEMENT OPERATIONS SHALL NOT BE LOCATED IN THE SPAN SUPPORTED BY THE TEMPORARY SUPPORT.
- THE TEMPORARY SUPPORT SHALL NOT BE USED IN WINDS ABOVE 30 MPH.
- THE ANTICIPATED MAXIMUM LOAD CARRIED BY THE TEMPORARY SUPPORT IS 12.5 TONS (25 KIPS). THE MINIMUM SAFETY FACTOR AGAINST BREAKING FOR THIS CONDITION SHALL BE 5 TO 1 FOR ALL RIGGING COMPONENTS.

PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGN-DETAILED	S. JAHNES	BY	E. LOWELL
CHECKED-REVIEWED	T. STUOPS	DATE	4/24/13
DESIGN-DETAILED	R. BAC	SIGNATURE	
REVISIONS 1		P.E. NUMBER	
REVISIONS 2		DATE	
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			

DEER ISLE-SEDGWICK BRIDGE	EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY	
STIFFENING GIRDER BEARING	
BUSHING REPAIRS 1 OF 3	

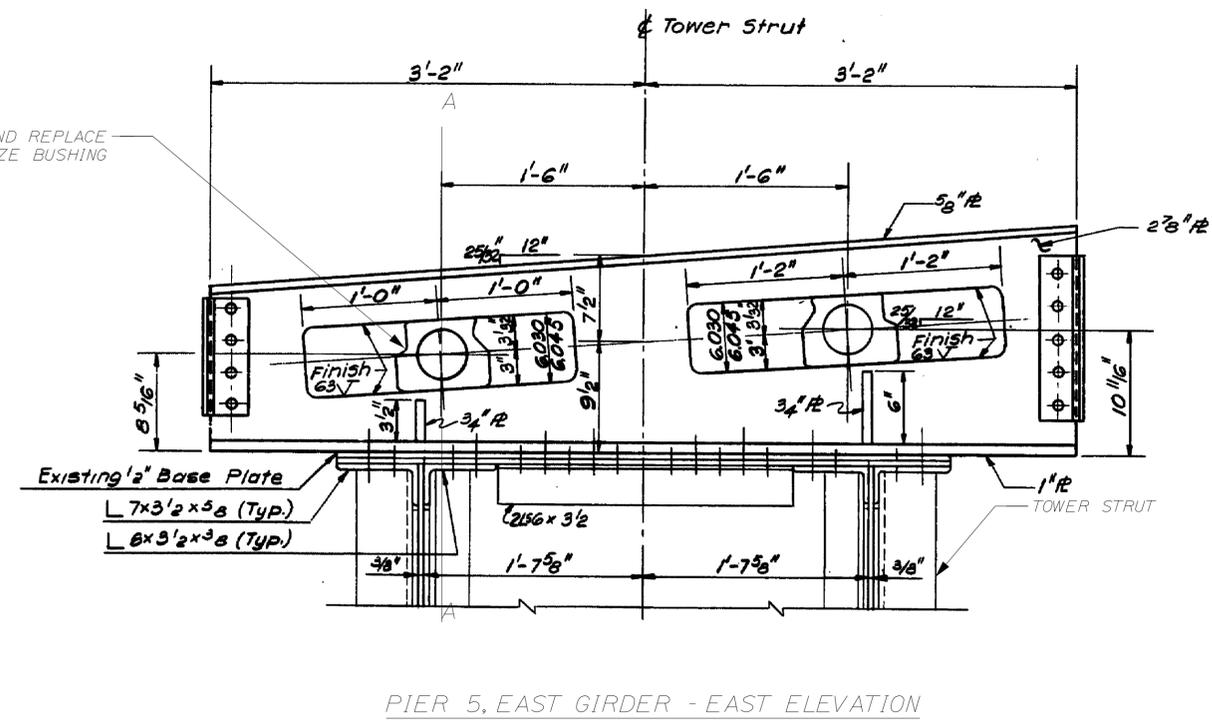
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OF 19	

Date: 5/7/2013

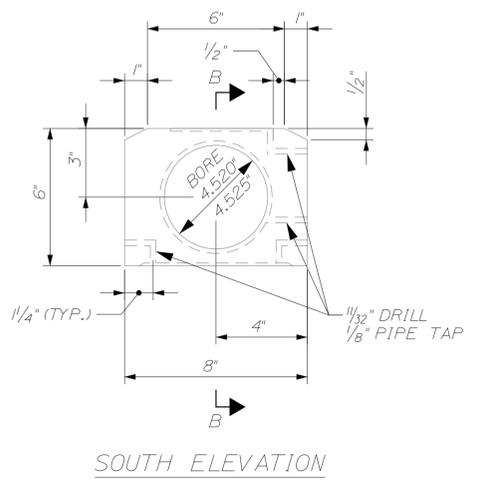
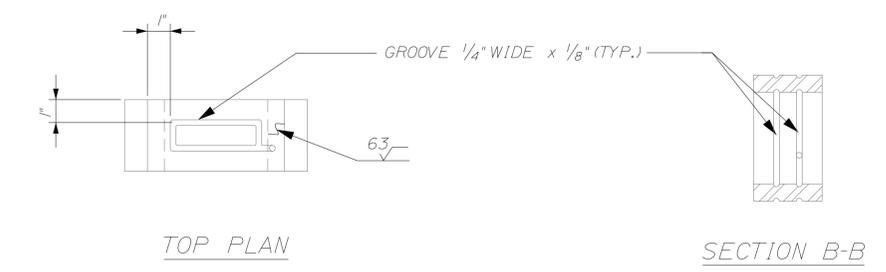
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Division: BRIDGE

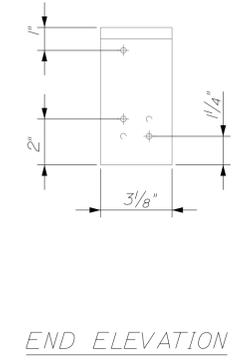
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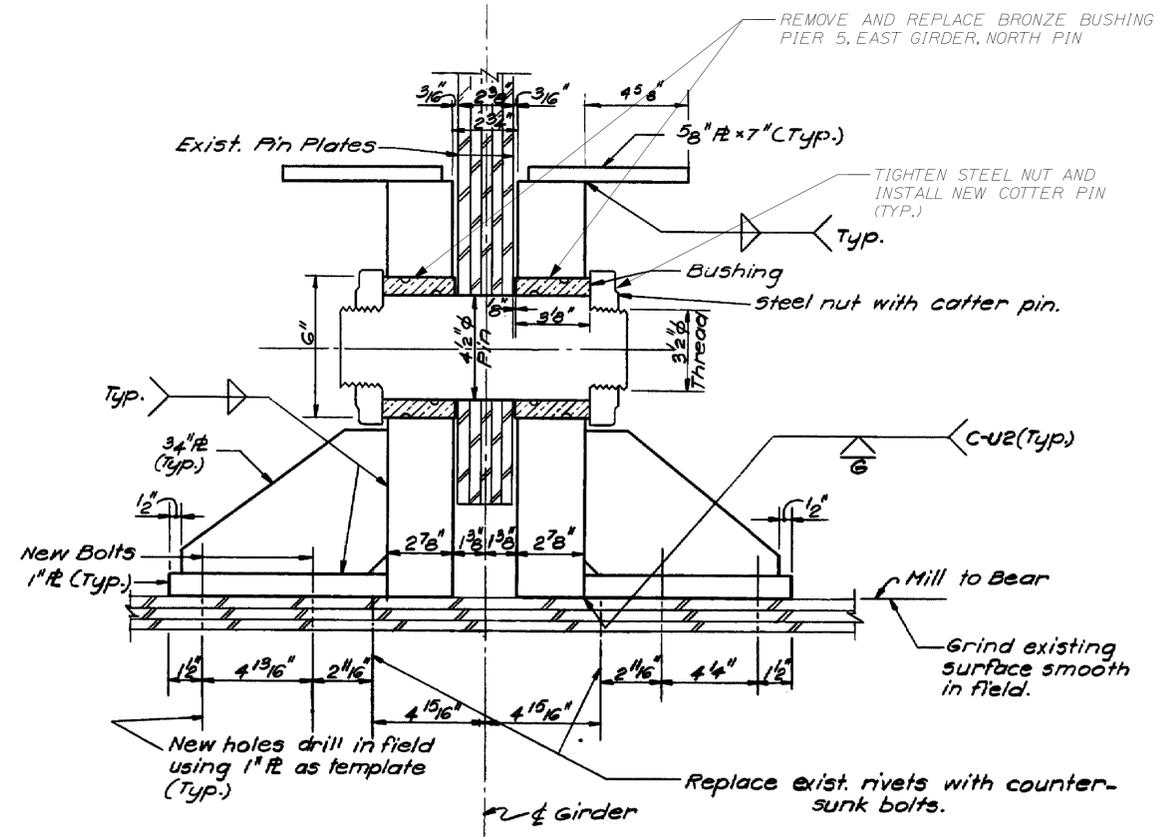
PIER 5, EAST GIRDER - EAST ELEVATION



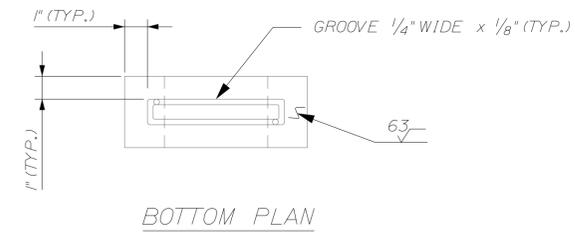
SOUTH ELEVATION



END ELEVATION



SECTION A-A
(TOWER STRUT NOT SHOWN FOR CLARITY)



BOTTOM PLAN

BRONZE BUSHING DETAILS

NOTES:

1. FOR GENERAL NOTES SEE SHEET 2
2. ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.511 UNLESS OTHERWISE NOTED.

STATE OF MAINE		DEPARTMENT OF TRANSPORTATION		HP-1006(210)		WIN 10062.10		BRIDGE NO. 3257		BRIDGE PLANS	
DEER ISLE-SEDGWICK BRIDGE		EGGEMOGGIN REACH		DEER ISLE-SEDGWICK HANCOCK COUNTY		STIFFENING GIRDER BEARING		BUSHING REPAIRS 2 OF 3		SHEET NUMBER	
PROJ. MANAGER E. LOWELL		DESIGNER S. JAHNES		CHECKED E. LOWELL		DESIGNED T. STUOPS		DATE 4/24/13		SIGNATURE	
DESIGNED R. BACK		CHECKED S. JAHNES		DESIGNED E. LOWELL		DESIGNED T. STUOPS		DATE 4/24/13		SIGNATURE	
REVISIONS 1		REVISIONS 2		REVISIONS 3		REVISIONS 4		FIELD CHANGES		P.E. NUMBER	
REVISIONS 1		REVISIONS 2		REVISIONS 3		REVISIONS 4		FIELD CHANGES		DATE	

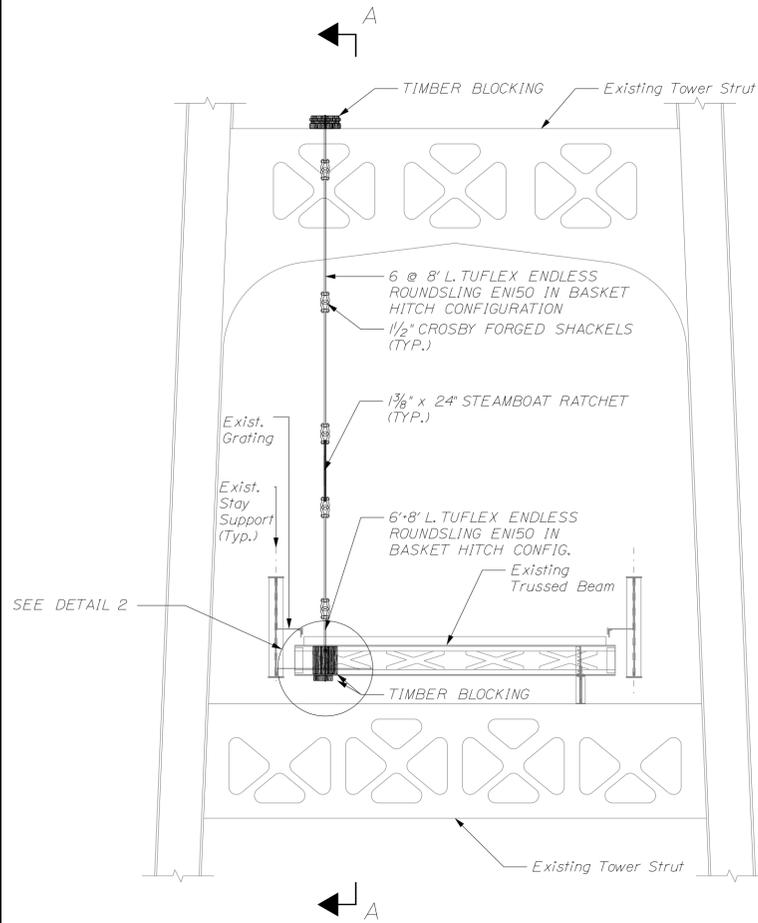
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Date: 5/7/2013

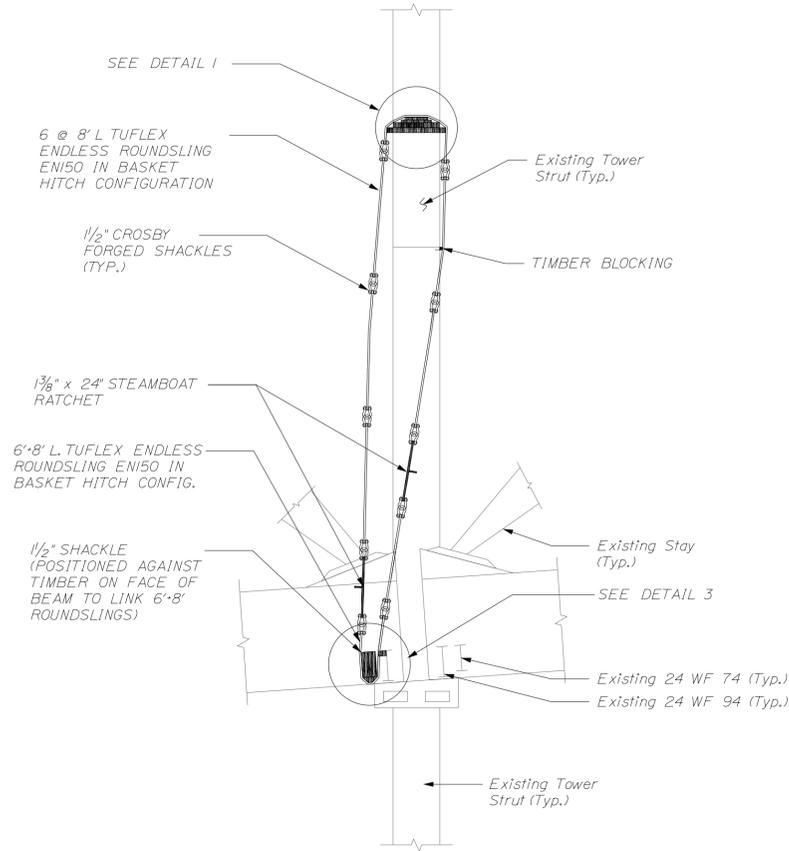
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Division: BRIDGE

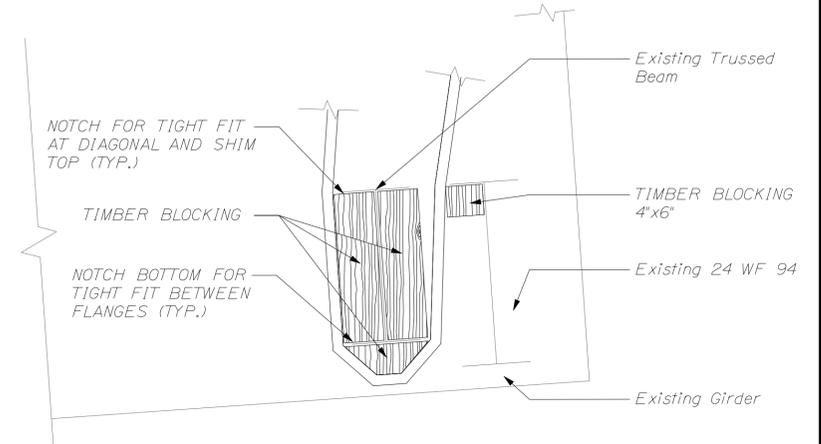
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BUSHING REPAIRS TEMPORARY SUPPORT PIER 5 NORTH ELEVATION

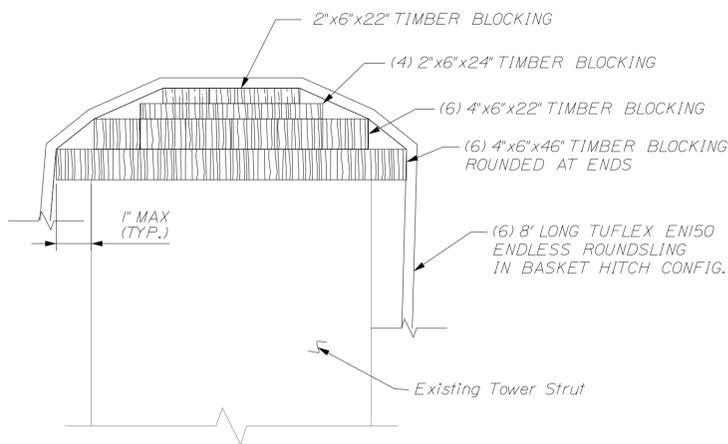


SECTION A-A

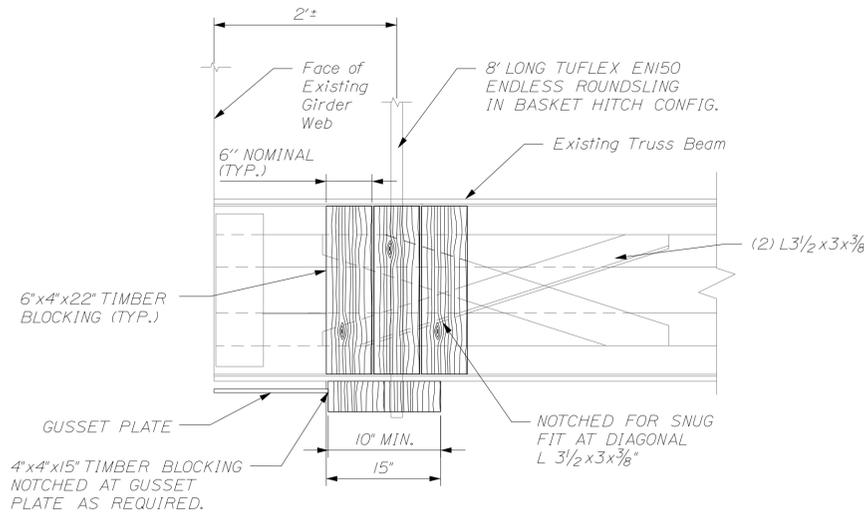


NOTE: BLOCKING TO EXTEND 1" BEYOND EDGE OF FLANGE.

DETAIL 3



DETAIL 1



DETAIL 2

NOTES:

1. FOR GENERAL NOTES SEE SHEET 2.
2. FOR TEMPORARY SUPPORT NOTES, SUGGESTED TEMPORARY SUPPORT SEQUENCE, AND SUGGESTED BUSHING REPLACEMENT SEQUENCE, SEE SHEET 6.
3. TUFLEX ENDLESS ROUNDSLINGS, 16 @ 1/2" FORGED SHACKLES AND STEAMBOAT RATCHETS TO BE SUPPLIED BY THE DEPARTMENT FOR THE CONTRACTOR'S USE. ALL REMAINING MATERIALS TO BE SUPPLIED BY THE CONTRACTOR. SEE SHEET 6.
4. ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.511.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
HP-1006(210)
WIN
10062.10
BRIDGE NO. 3257
BRIDGE PLANS

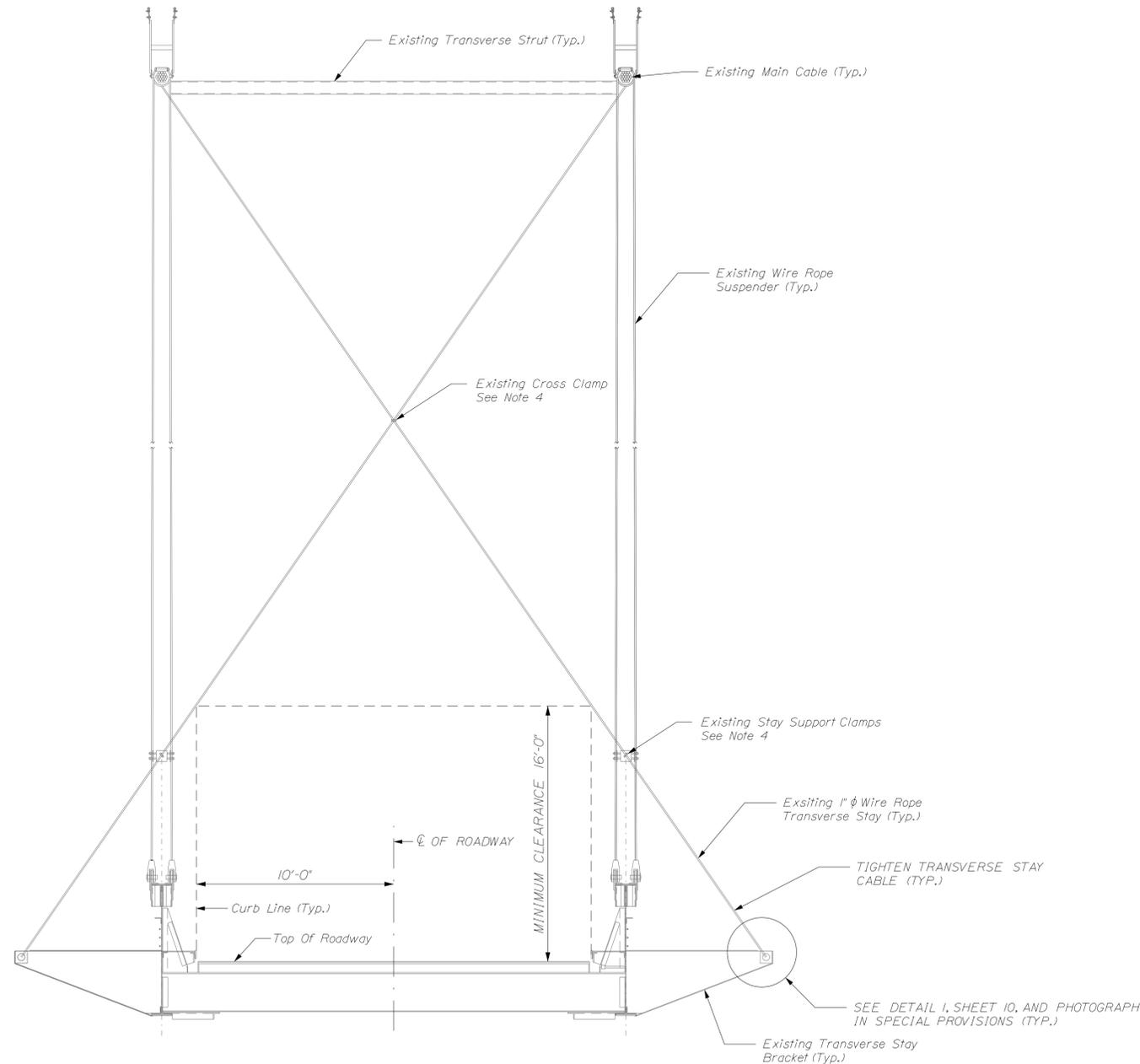
DATE	4/24/13	BY	E. LOWELL	PROJ. MANAGER	E. LOWELL
DESIGNED	S. JAHNES	CHECKED	T. STUOPS	DESIGNED	S. JAHNES
DESIGNED	R. BACK	DESIGNED	R. BACK	DESIGNED	R. BACK
REVISIONS	1	REVISIONS	1	REVISIONS	1
REVISIONS	2	REVISIONS	2	REVISIONS	2
REVISIONS	3	REVISIONS	3	REVISIONS	3
REVISIONS	4	REVISIONS	4	REVISIONS	4
FIELD CHANGES		FIELD CHANGES		FIELD CHANGES	

DATE	4/24/13	BY	E. LOWELL	PROJ. MANAGER	E. LOWELL
DESIGNED	S. JAHNES	CHECKED	T. STUOPS	DESIGNED	S. JAHNES
DESIGNED	R. BACK	DESIGNED	R. BACK	DESIGNED	R. BACK
REVISIONS	1	REVISIONS	1	REVISIONS	1
REVISIONS	2	REVISIONS	2	REVISIONS	2
REVISIONS	3	REVISIONS	3	REVISIONS	3
REVISIONS	4	REVISIONS	4	REVISIONS	4
FIELD CHANGES		FIELD CHANGES		FIELD CHANGES	

DEER ISLE-SEDGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY
STIFFENING GIRDER BEARING
BUSHING REPAIRS 3 OF 3

SHEET NUMBER

8



TYPICAL SUSPENDED SPAN TRANSVERSE SECTION AT TRANSVERSE STAY CABLES

NOTE: THIS SECTION IS TYPICAL AT SUSPENDER LOCATIONS 10, 11, 23, 24, 25, 25', 24', 23', 11' AND 10'.

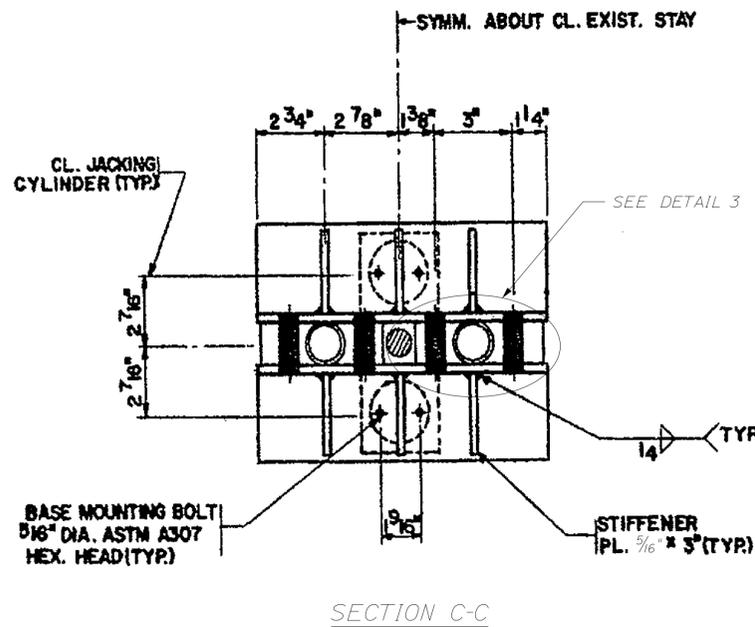
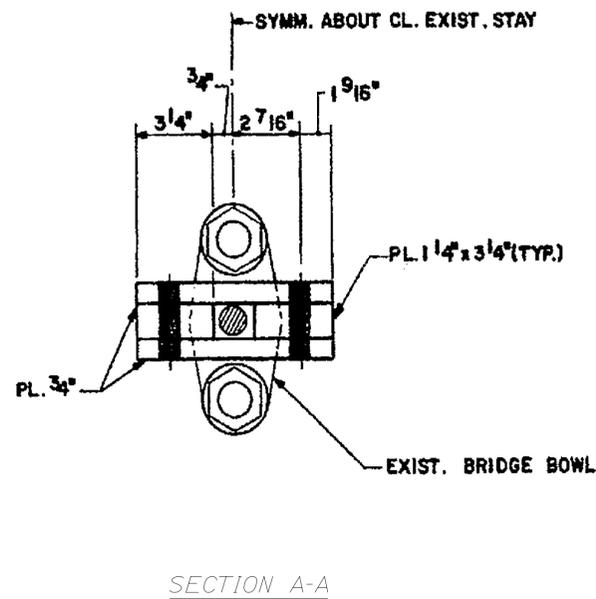
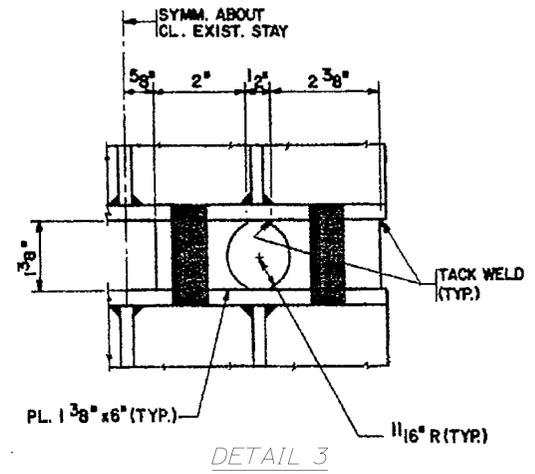
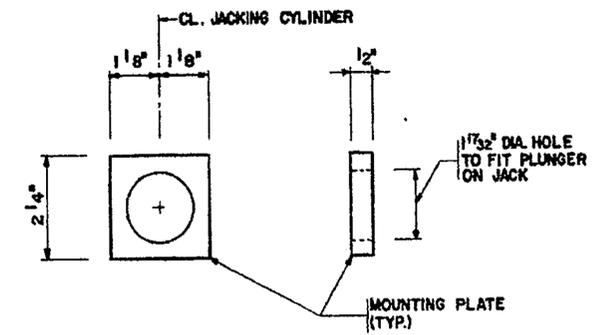
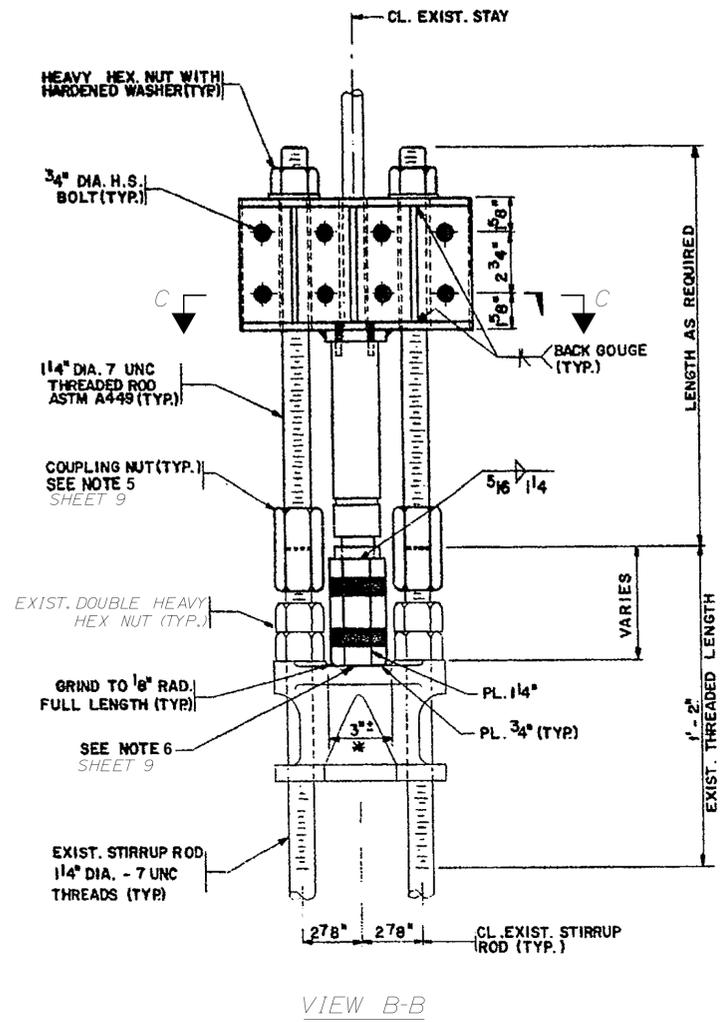
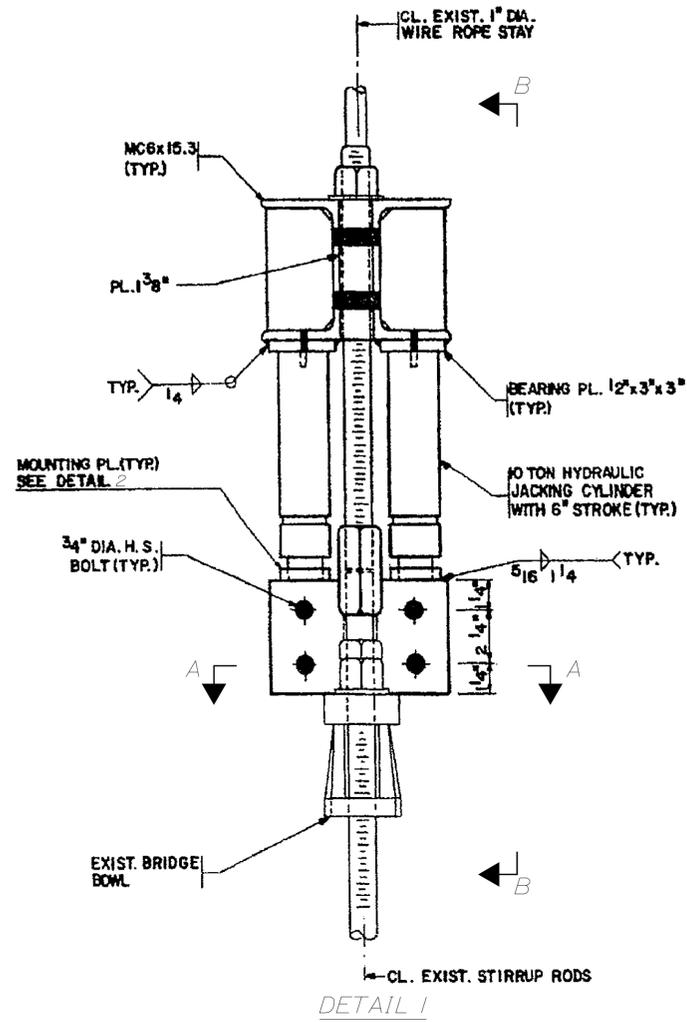
SUGGESTED SEQUENCE OF CONSTRUCTION (ITEM 504.69):

- CLEAN THE SURFACE OF THE EXISTING BRIDGE BOWL AND EXISTING DOUBLE HEAVY HEX NUTS TO BEAR METAL.
- INSTALL TEMPORARY THREADED ROD EXTENSIONS AND JACKING SYSTEM.
- TEMPORARILY SUPPORT THE TRANSVERSE STAY NOT BEING TIGHTENED TO MAINTAIN A 16'-0" CLEARANCE ON THE OPEN LANE.
- LOOSEN THE EXISTING CROSS CLAMPS AND THE EXISTING STAY SUPPORT CLAMP AT THE SUSPENDER ADJACENT TO THE JACKING OPERATIONS.
- TENSION TRANSVERSE CABLE, VERIFY PROPER CLEARANCE AND TIGHTEN DOUBLE HEAVY HEX NUTS.
- TIGHTEN STAY SUPPORT CLAMPS AT THE SUSPENDER ADJACENT TO JACKING OPERATIONS.
- REMOVE JACK AND THREADED ROD EXTENSIONS, REMOVE TEMPORARY VERTICAL CLEARANCE SUPPORT, INSTALL THREADED ROD EXTENSIONS AND JACK ON OPPOSITE SIDE.
- TENSION TRANSVERSE CABLE, VERIFY PROPER CLEARANCE AND TIGHTEN DOUBLE HEAVY HEX NUTS.
- TIGHTEN EXISTING CROSS CLAMPS AND EXISTING TRANSVERSE CROSS CLAMP.

NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- THE EXISTING TRANSVERSE STAYS SHALL BE TENSIONED TO A MINIMUM 10,000 LBS AND A MAXIMUM OF 10,500 LBS.
- THE STAY JACKING ASSEMBLY SHOWN ON SHEET 10 IS SUGGESTED AND THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE DESIGN TO THE RESIDENT FOR APPROVAL.
- THE EXISTING CROSS CLAMPS AT THE TRANSVERSE STAY INTERSECTION AND STAY SUPPORT CLAMPS AT THE SUSPENDERS SHALL BE LOOSENED PRIOR TO TENSIONING THE CABLES AND RE-TIGHTENED AFTER TENSIONING.
- MATERIALS:
STRUCTURAL STEEL PLATES AND SHAPES:
ASTM A709 GRADE 36 (UNLESS OTHERWISE NOTED)
STRUCTURAL BOLTS, NUT AND WASHERS:
ASTM A325 HIGH STRENGTH (UNLESS OTHERWISE NOTED)
THREADED RODS:
ASTM A449
COUPLING NUTS:
ASTM A563 GRADE C, D OR DH OR ASTM A194 GRADE 2 OR 2H.
- CLEAN THE SURFACE OF THE EXISTING BRIDGE BOWL TO SSPC-SP11 POWER TOOL CLEANING TO BARE METAL TO PROVIDE PROPER BEARING.
- THE JACKING OPERATIONS OF ONE PAIR OF TRANSVERSE CABLES AT ONE SUSPENDER LOCATION SHALL BE COMPLETED ONE AT A TIME AND IN THE SAME DAY.
- THE CONTRACTOR SHALL FIELD VERIFY DIMENSIONS PRIOR TO FABRICATION OF THE STAY JACKING ASSEMBLY.
- JACKING EQUIPMENT
EACH JACKING ASSEMBLY SHALL INCLUDE TWO JACKING CYLINDERS CONNECTED TO A SINGLE HYDRAULIC PUMP, AS FOLLOWS:
JACKING CYLINDERS: SINGLE-ACTING HYDRAULIC JACKING CYLINDER WITH 6" STROKE. JACKING CYLINDERS FOR TRANSVERSE STAYS SHALL BE 10 TON CAPACITY.
PUMPS: 0 TO 10,000 PSI MINIMUM, TWO STAGE, EQUIPPED WITH CALIBRATED HYDRAULIC GAGE.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL STEEL ITEMS AND CATALOG CUTS FOR ALL HYDRAULIC JACKING EQUIPMENT FOR THE STAY JACKING ASSEMBLIES TO THE RESIDENT FOR REVIEW AND APPROVAL.
- THE STAY JACKING ASSEMBLIES SHOWN ON SHEET 10 SHALL BE FURNISHED BY THE CONTRACTOR AND SHALL BECOME THE PROPERTY OF MAINE DOT AT THE COMPLETION OF THE WORK. ITEMS TO BE DELIVERED TO THE MAINE DOT SHALL INCLUDE ALL STRUCTURAL STEEL PLATES AND SHAPES, PINS, THREADED RODS, COUPLING NUTS, BOLTS AND HYDRAULIC JACKING EQUIPMENT USED TO PERFORM THE STAY ADJUSTMENTS.
- ALL WORK REQUIRED TO TENSION THE TRANSVERSE STAY CABLES SHALL BE INCLUDED UNDER ITEM 504.69.
- ALL WORK REQUIRED TO FURNISH, INSTALL AND REMOVE THE TRANSVERSE STAY JACKING ASSEMBLIES AND THEIR DELIVERY TO THE MAINE DOT SHALL BE INCLUDED IN THE WORK UNDER ITEM 504.69.

STATE OF MAINE		DEPARTMENT OF TRANSPORTATION	
HP-1006(210)		WIN	
BRIDGE NO. 3257		10062.10	
BRIDGE PLANS			
PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGN DETAILED	S. JAHNES	BY	E. LOWELL
CHECKED/REVIEWED	T. STUOPS	DATE	4/24/13
DESIGN DETAILED	R. BAC	SIGNATURE	
REVISIONS 1		P.E. NUMBER	
REVISIONS 2		DATE	
REVISIONS 3			
REVISIONS 4			
FIELD CHANGES			
DEER ISLE-SEDGWICK BRIDGE			
EGGEMOGGIN REACH			
DEER ISLE-SEDGWICK HANCOCK COUNTY			
TENSIONING TRANSVERSE STAY CABLES 1 OF 2			
SHEET NUMBER			
9			
OF 19			

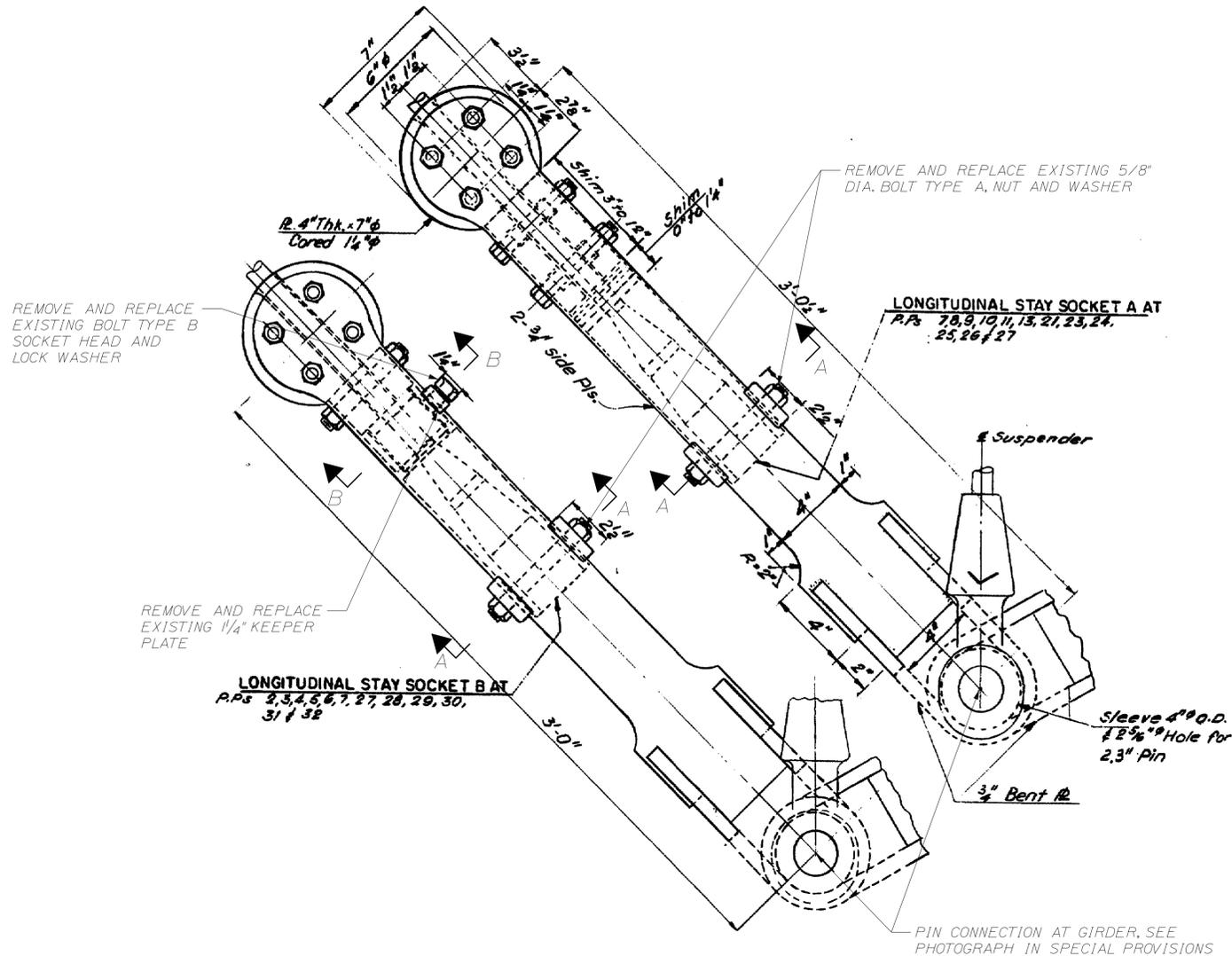


NOTES:

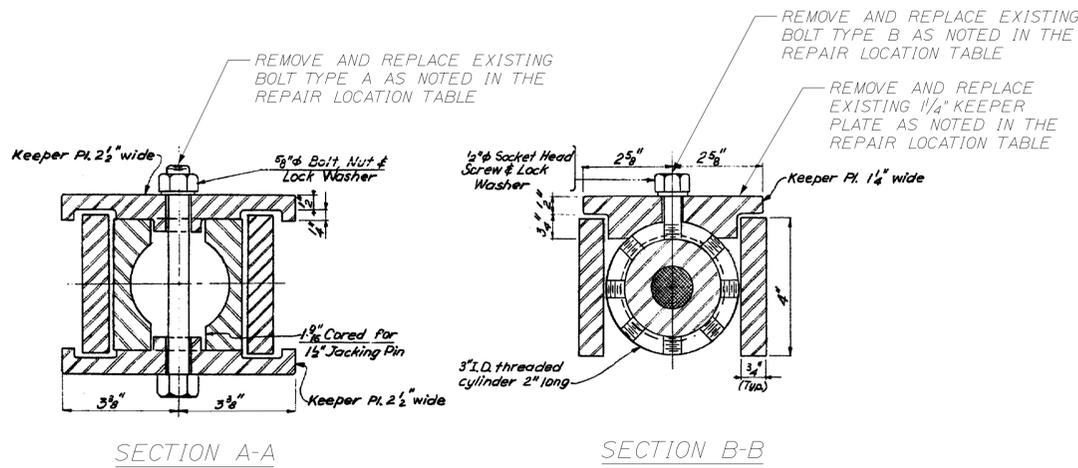
1. FOR GENERAL NOTES SEE SHEET 2.
2. FOR NOTES AND SUGGESTED SEQUENCE ON TENSIONING TRANSVERSE STAY CABLES, SEE SHEET 9.
3. ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.69.

PROJ. MGR.	DATE	BY	DATE	SIGNATURE	P.E. NUMBER	DATE
E. LOWELL	4/24/13	E. LOWELL	4/24/13			
S. JAHNES	4/24/13	S. JAHNES	4/24/13			
T. STUOPS		T. STUOPS				
R. BACK		R. BACK				
REVISIONS 1						
REVISIONS 2						
REVISIONS 3						
REVISIONS 4						
FIELD CHANGES						

DEER ISLE-SEGDWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEGDWICK HANCOCK COUNTY
TENSIONING TRANSVERSE
STAY CABLES 2 OF 2



LONGITUDINAL STAY CONNECTION AT GIRDER



SECTION A-A

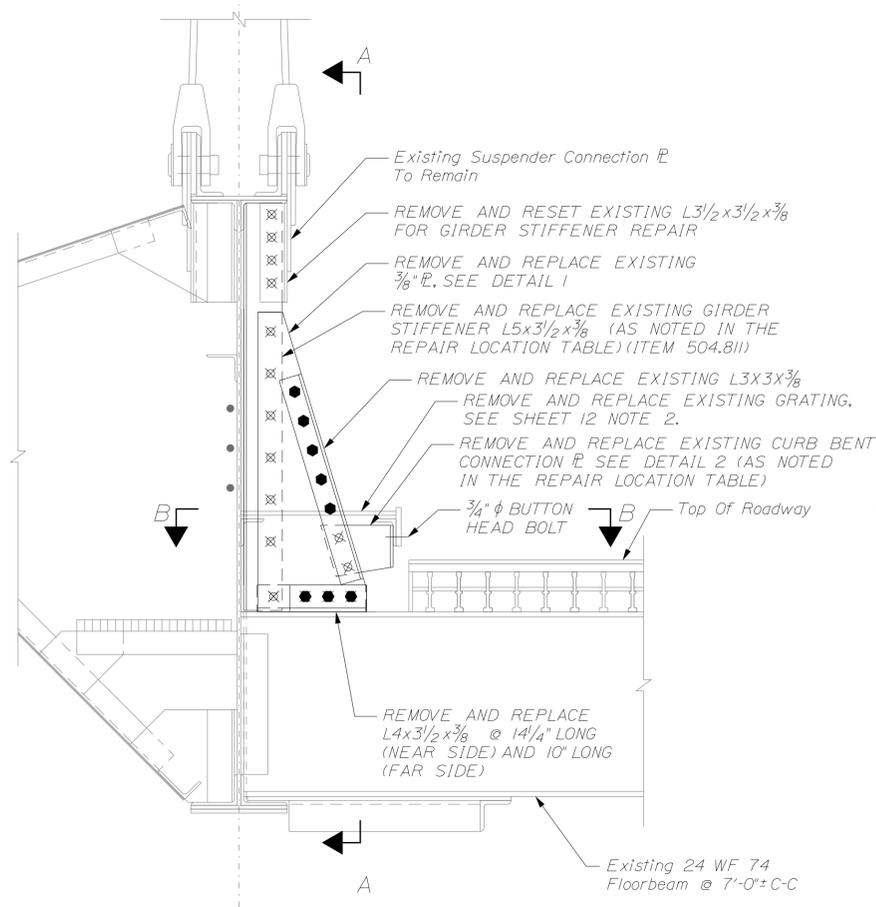
SECTION B-B

CABLE	LOCATION		REPAIR LOCATIONS	
	PANEL POINT	SIDE OF BENT	REMOVE AND REPLACE BOLT	NEW KEEPER PLATE
West Cable	4	North	Type B	Yes
West Cable	5	North	Type A	
West Cable	6	South	Type A	Yes
West Cable	6	North	Type A	
West Cable	7	South	Type A	
West Cable	8	North	Type A	
West Cable	11	South	Type A	
West Cable	13	North	Type A	
West Cable	21	South	Type A	
West Cable	24	North	Type A	
West Cable	25	South	Type A	
West Cable	27	North	Type A	
West Cable	28	North	Type B	Yes
West Cable	29	North	Type A and B	Yes
West Cable	32	North	Type B	Yes
West Cable	32	North	Type A	
West Cable	30	North	Type A	
West Cable	22	North	Type A	
West Cable	13	South	Type A	
West Cable	13	North	Type A	
West Cable	10	South	Type A	
West Cable	9	South	Type A	
West Cable	9	North	Type A	
West Cable	8	North	Type A	
West Cable	7	South	Type A	
West Cable	6	South	Type A	
West Cable	6	South	Type B	Yes
West Cable	5	South	Type A	
West Cable	4	South	Type A	
West Cable	3	North	Type A	
West Cable	2	North	Type A and B	Yes
East Cable	2	North	Type A	
East Cable	3	North	Type A	
East Cable	5	South	Type A	
East Cable	7	South	Type A	
East Cable	8	North	Type A	
East Cable	9	South	Type A	
East Cable	10	South	Type A	
East Cable	11	South	Type A	
East Cable	6	South	Type B	Yes
East Cable	23	South	Type A	
East Cable	25	North	Type A	
East Cable	27	North	Type B	Yes
East Cable	32	North	Type A	
East Cable	31	North	Type A	
East Cable	30	North	Type A	
East Cable	30	South	Type B	Yes
East Cable	28	South	Type B	Yes
East Cable	24	South	Type A	
East Cable	24	North	Type A	
East Cable	23	South	Type A	
East Cable	23	North	Type A	
East Cable	21	North	Type A	
East Cable	13	South	Type A	
East Cable	11	North	Type A	
East Cable	10	North	Type A	
East Cable	9	South	Type A	
East Cable	8	South	Type A	
East Cable	7	South	Type A	
East Cable	7	North	Type A and B	Yes
East Cable	6	South	Type A and B	Yes
East Cable	6	North	Type A	
East Cable	5	North	Type A and B	

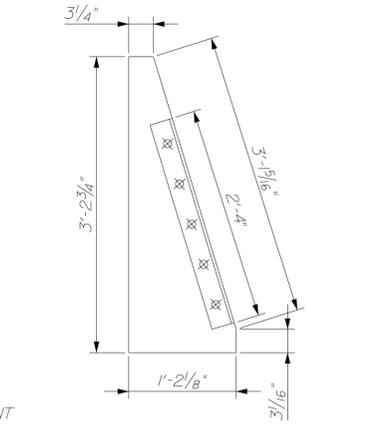
NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- DETAILS SHOWN FOR BRACKETS AND LONGITUDINAL STAYS ARE REPRESENTATIVE OF THE EXISTING BRACKETS, AND ARE NOT GUARANTEED TO BE CORRECT. ALL REQUIRED WORK TO REMOVE AND REPLACE THE KEEPER PLATE ON THE LONGITUDINAL STAYS TO BE PAID FOR UNDER ITEM 504.671. ALL REQUIRED WORK TO REMOVE AND REPLACE THE KEEPER BOLT ON THE LONGITUDINAL STAYS TO BE PAID FOR UNDER ITEM 504.672 AND 509.673.
- REMOVE AND REPLACE ONLY ONE BOLT AT A TIME.

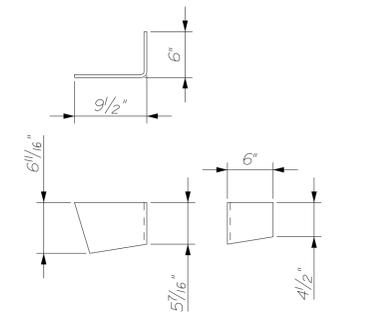
DATE	BY	PROJ. MGR.	DESIGN DET.	CHECKED	DESIGN DET.	REVISIONS	FIELD CHANGES
4/24/13	E. LOWELL	E. LOWELL	S. JAHNES	S. JAHNES	R. BACK	1	
4/24/13	E. LOWELL	E. LOWELL	S. JAHNES	S. JAHNES	R. BACK	2	
4/24/13	S. JAHNES	S. JAHNES	S. JAHNES	S. JAHNES	R. BACK	3	
						4	



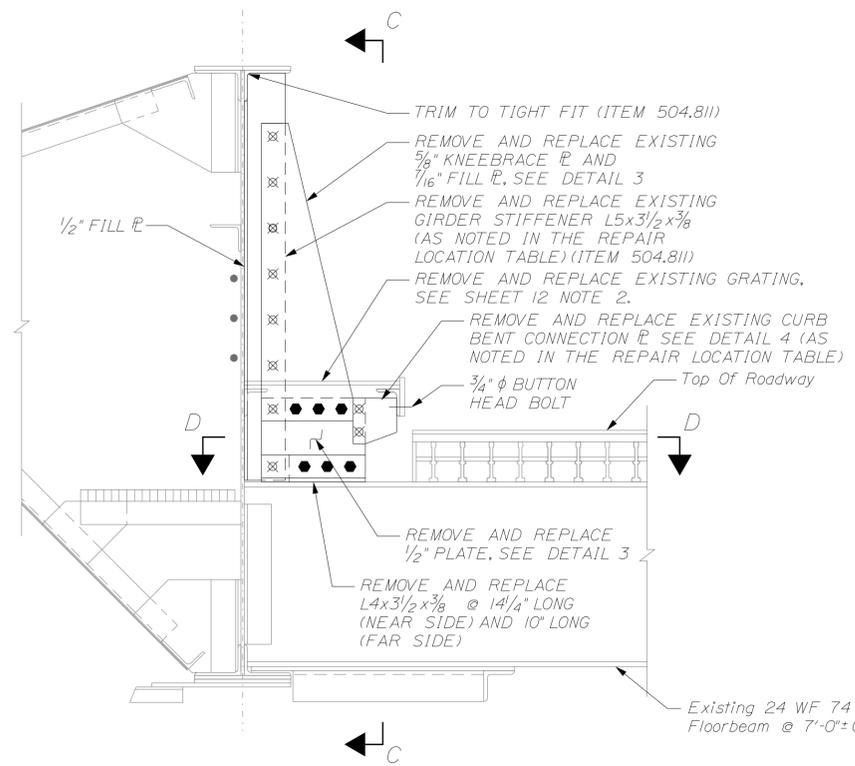
PROPOSED FLOORBEAM KNEEBRACE REPAIR TYPE A



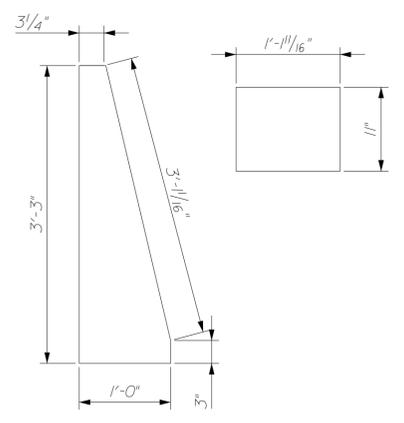
DETAIL 1 - FLOORBEAM KNEE BRACE TYPE A PLATE



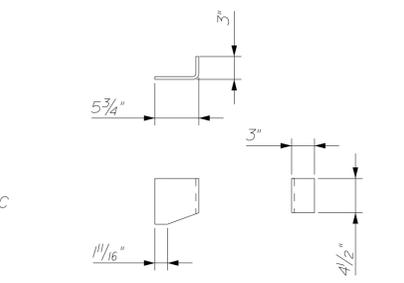
DETAIL 2 - FLOORBEAM KNEEBRACE TYPE A CURB CONNECTION BENT PLATE



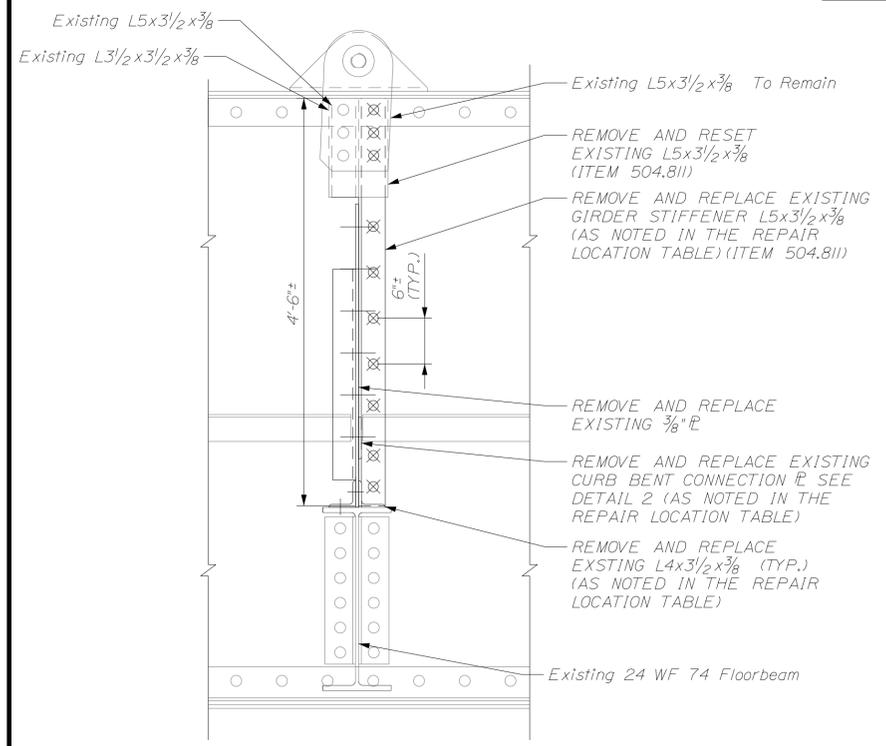
PROPOSED FLOORBEAM KNEEBRACE REPAIR TYPE B



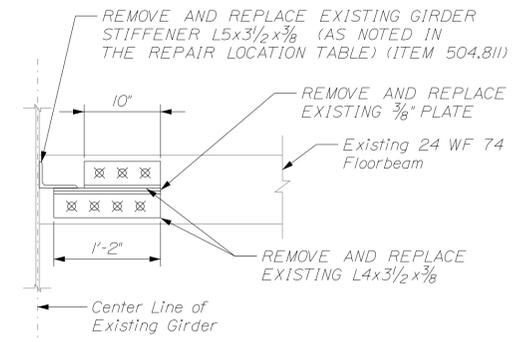
DETAIL 3 - FLOORBEAM KNEE BRACE TYPE B PLATE



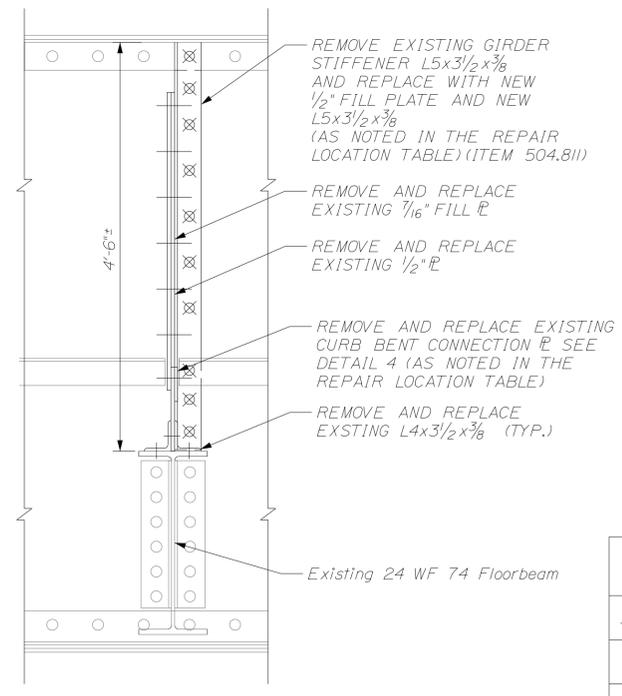
DETAIL 4 - FLOORBEAM KNEEBRACE TYPE B CURB CONNECTION BENT PLATE



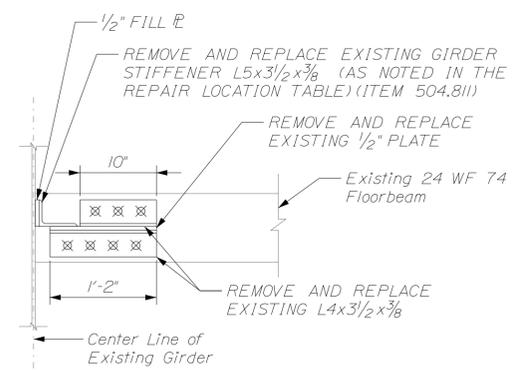
SECTION A-A



SECTION B-B



SECTION C-C



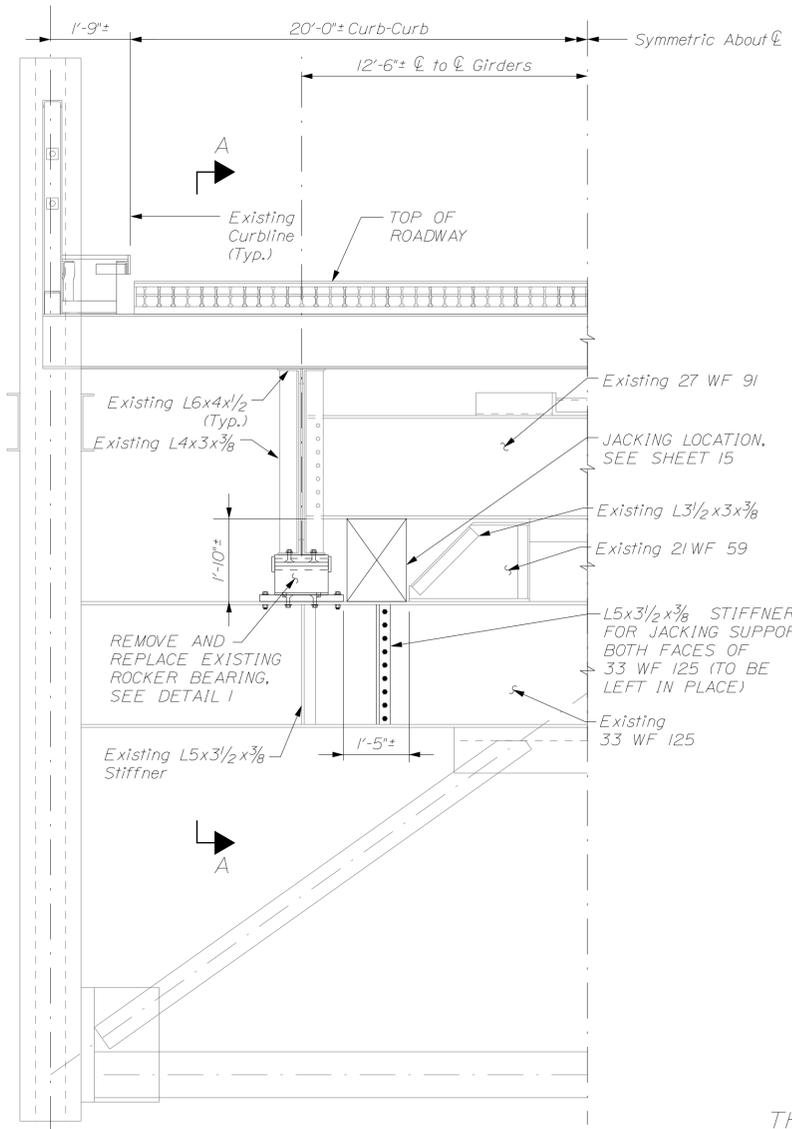
SECTION D-D

LEGEND	
SYM.	DESCRIPTION
○	EXISTING FASTENER
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH BOLT IN EXISTING HOLE
●	PLACE NEW HIGH STRENGTH BOLT IN NEW HOLE

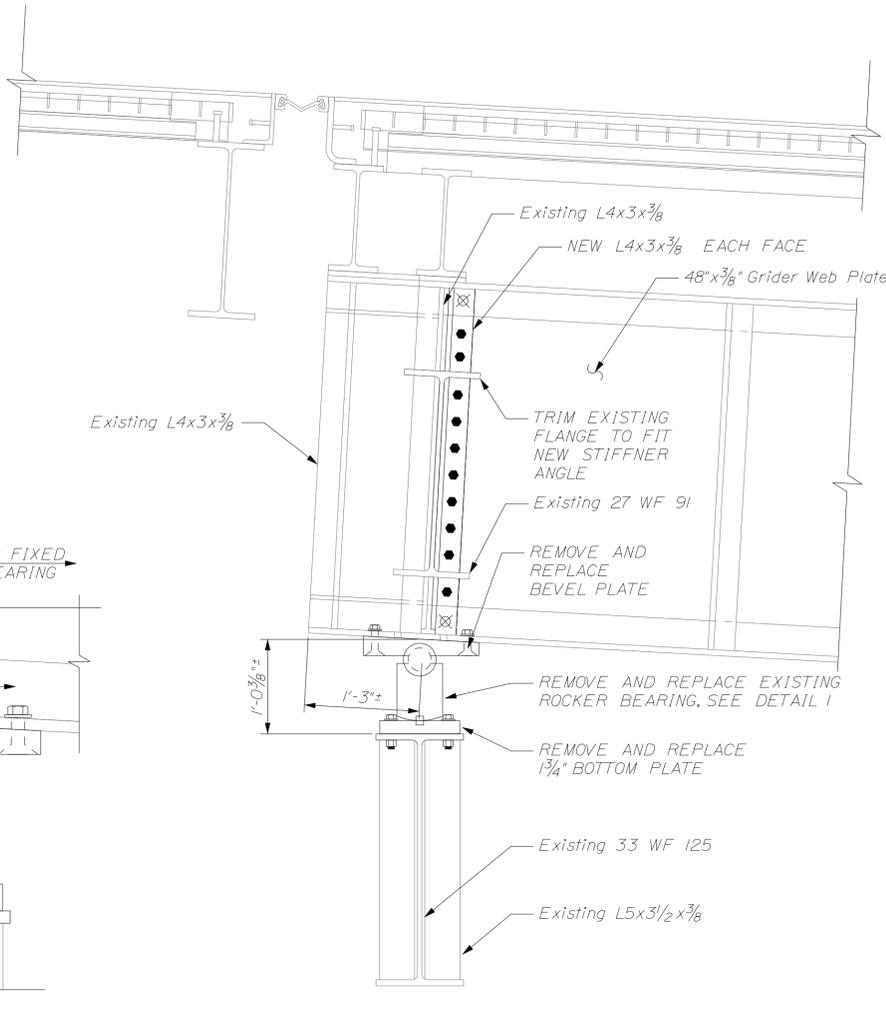
- NOTES:
- FOR GENERAL NOTES SEE SHEET 2.
 - FOR NOTES ON KNEEBRACE REPAIRS TYPE A AND B SEE SHEET 12.
 - WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 504.722 UNLESS OTHERWISE NOTED.

DATE	SIGNATURE	P.E. NUMBER	DATE
4/24/13	E. LOWELL		
4/24/13	T. STUOPS		
4/24/13	S. JAHNES		

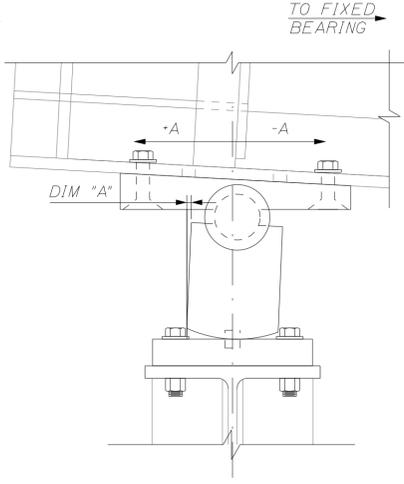
PROJ. MANAGER	BY	DATE
E. LOWELL	E. LOWELL	4/24/13
	T. STUOPS	4/24/13
	S. JAHNES	4/24/13



ROCKER BEARING REPAIR



SECTION A-A

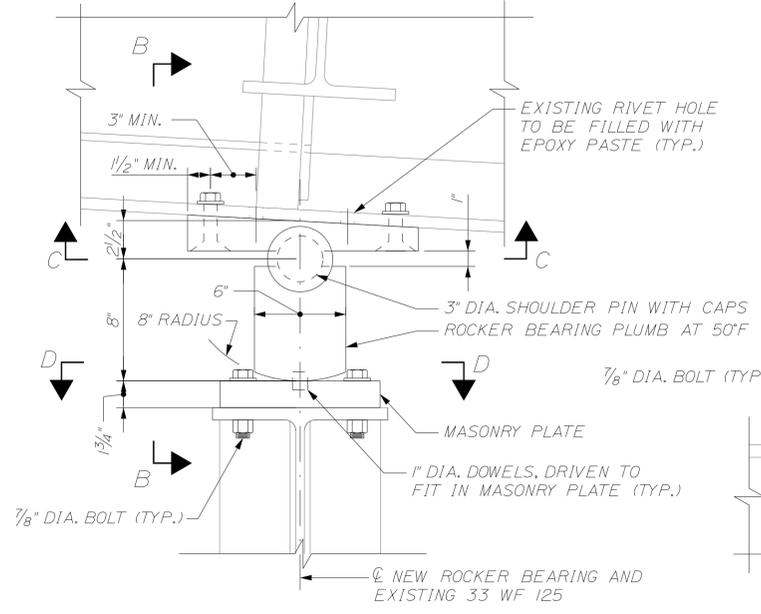


THEORETICAL ROCKER BEARING - DIM "A"

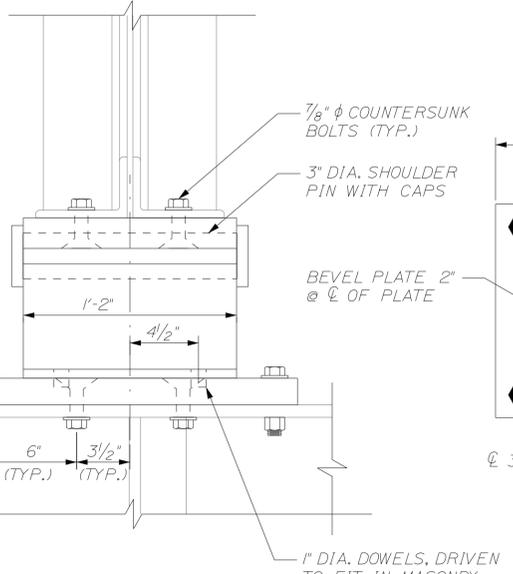
NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- BEARING ASSEMBLY SHALL BE ASSHTO DESIGNATION M270 GRADE 36, ZONE T2.
- SOLE PLATE AND BOTTOM PLATE SHALL BE MACHINED TO ENSURE TRUE PARALLEL SURFACES.
- BEARING CONTACT SURFACES SHALL HAVE FINISHES AS SPECIFIED BY AASHTO.
- THE REPLACEMENT OF A BEARING SHALL NOT COMMENCE UNTIL ALL REPLACEMENT MATERIALS ARE ON SITE.
- ADJUST SOLE PLATE FOR TEMPERATURE
 - A) NEW SOLE PLATE SHALL BE SET SO THAT THE ROCKER IS VERTICAL UNDER FULL DEAD LOAD AT A TEMPERATURE OF 50°. ANY ADJUSTMENTS SHALL BE MADE AS PER TEMPERATURE TABLE FOR POSITIONING ROCKER (SEE NOTE 9 BELOW).
 - B) SECURE THE BOLTS UPON FINAL ADJUSTMENT OF MASONRY PLATE AS APPROVED BY THE RESIDENT.
- ALL DIMENSIONS SHOWN ARE FROM ORIGINAL DESIGN DRAWINGS. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO FABRICATION.
- THE OVERALL EXISTING DIMENSION FROM THE BOTTOM OF THE BOTTOM FLANGE OF THE GIRDER TO THE TOP FLANGE OF THE EXISTING 33 WF 125 SHALL BE MEASURED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADJUST THE SOLE PLATE AND BOTTOM PLATE THICKNESS AS BASED ON ACTUAL FIELD MEASUREMENTS. THIS INFORMATION SHALL BE SUPPLIED ON THE SHOP DRAWINGS.
- STEEL TEMPERATURE MEASUREMENTS SHALL BE MADE BY COMPUTING THE AVERAGE TEMPERATURE OF THE GIRDER TAKEN AT A MINIMUM OF THREE LOCATIONS.
- ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 523.32.

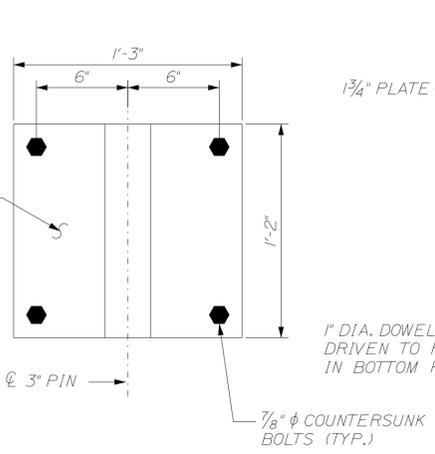
TEMPERATURE TABLE FOR POSITIONING BEARING													
STEEL TEMP (°F)	-A						+A						
	-10	0	10	20	30	40	50	60	70	80	90	100	110
PIER 3 AND 6 (INCHES)	0.6	0.5	0.4	0.3	0.2	0.1	0	0.1	0.2	0.3	0.4	0.5	0.6



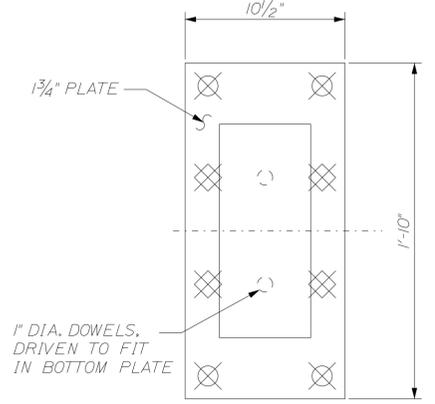
ROCKER BEARING DETAIL - I



SECTION B-B



SECTION C-C



SECTION D-D

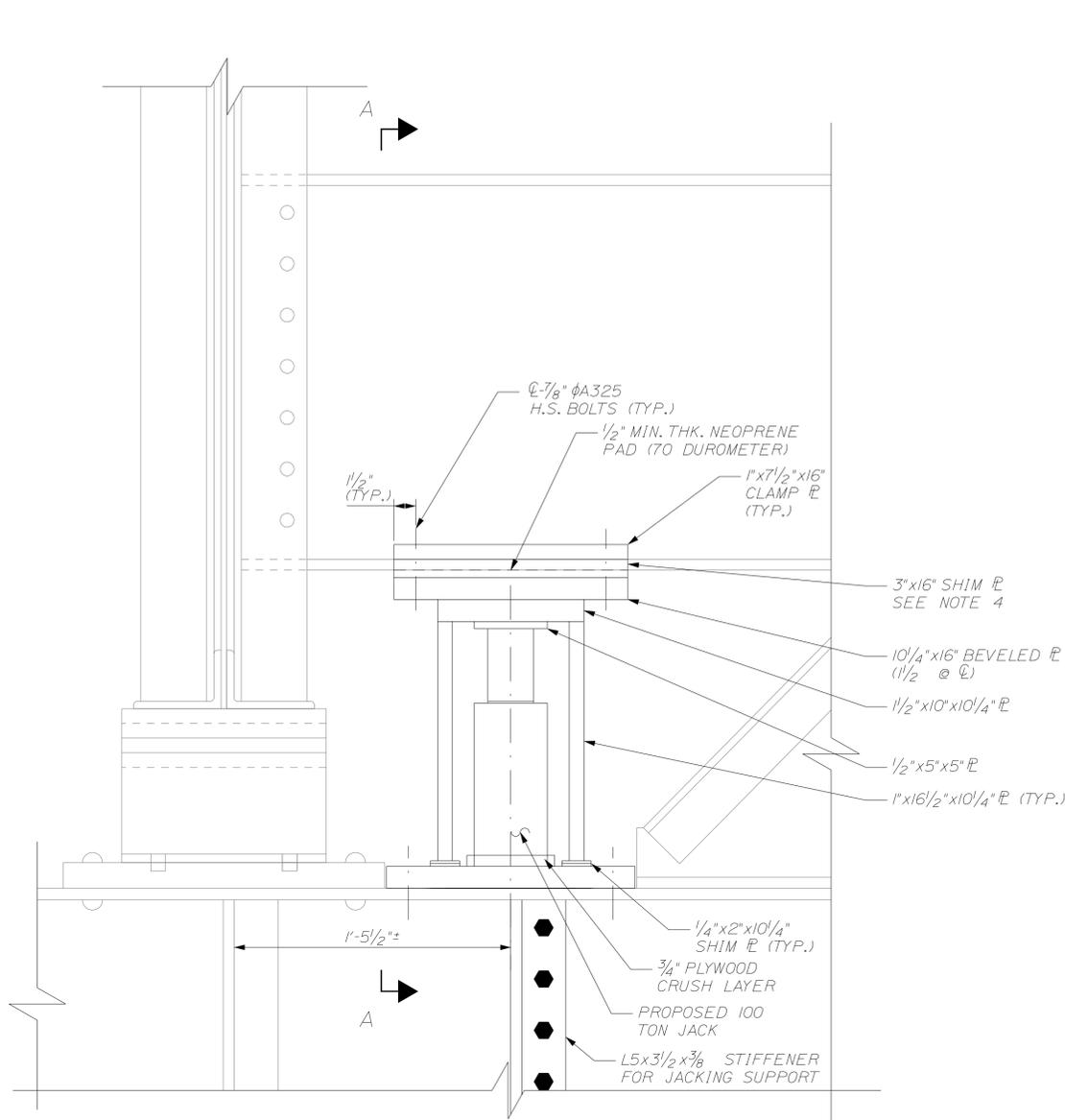
LEGEND	
SYM.	DESCRIPTION
○	EXISTING FASTENER
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH BOLT IN EXISTING HOLE
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH COUNTER SUNK BOLT IN EXISTING HOLE
●	PLACE NEW HIGH STRENGTH BOLT IN NEW HOLE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
HP-1006(210)
WIN 10062.10
BRIDGE NO. 3257
BRIDGE PLANS

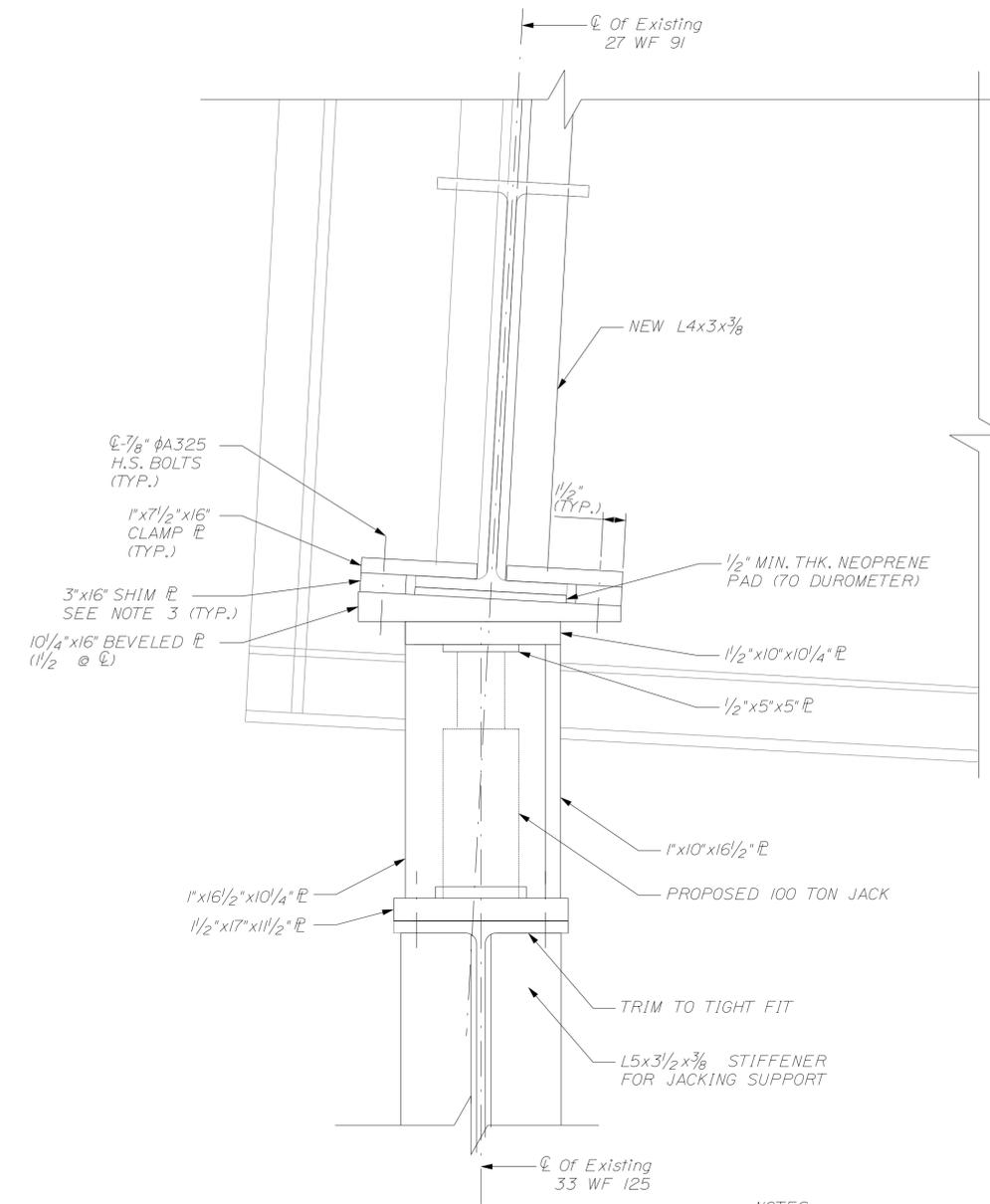
PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGN DETAILED	S. JAHNES	BY	E. LOWELL
CHECKED/REVIEWED	T. STUOPS	DATE	4/24/13
DESIGN DETAILED	R. BAC	BY	S. JAHNES
REVISIONS 1		DATE	
REVISIONS 2		DATE	
REVISIONS 3		DATE	
REVISIONS 4		DATE	
FIELD CHANGES		DATE	

DEER ISLE-SEDGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY
SUSPENSION BRIDGE BEARING
REPLACEMENT 1 OF 2

SHEET NUMBER
14
OF 19



ROCKER BEARING JACKING



SECTION A-A

JACKING NOTES:

- TEMPORARY SUPPORT DETAILS SHOWN ARE INTENDED TO FORM A UNIFORM BASIS FOR BIDDING ONLY. THE TEMP. SUPPORT SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MAINE AND IN THE EMPLOY OF THE CONTRACTOR. THE CONTRACTOR SHALL SUBMIT SHOP AND WORKING DRAWINGS AND SUPPORTING CALCULATIONS FOR WRITTEN APPROVAL BY THE RESIDENT PRIOR TO JACKING.
- THE ANTICIPATED JACKING LOAD IS 30 KIPS. THE ANTICIPATED MAXIMUM BLOCKING LOAD IS 50 KIPS. ALL COMPONENTS TO HAVE A MINIMUM SAFETY FACTOR OF 2.0. THE CONTRACTOR SHALL NOT JACK BEYOND 10% OF THE ANTICIPATED JACKING LOAD WITHOUT THE APPROVAL OF THE ENGINEER.
- LIVE LOAD SHALL NOT BE PERMITTED ON EITHER LANE DURING THE JACKING. LIVE LOAD ON THE ADJACENT LANE SHALL BE PERMITTED ONCE THE JACK IS BLOCKED.
- THE BOLSTER SHALL BE SHIMMED AND THE JACK PRESSURE RELEASED WHEN THE ADEQUATE SUPPORT HAS BEEN ACHIEVED. ADEQUATE SUPPORT SHALL BE DEFINED AS A SUSTAINED 1/16" LIFT AT THE EXISTING BEARING UNDER DEAD LOAD ONLY.
- GIRDERS SHALL BE JACKED AND BEARINGS REPLACED ONE AT A TIME. GIRDERS SHALL NOT BE JACKED UNTIL BEARING MATERIALS ARE ON SITE.
- CLEAN ALL SURFACES OF THE EXISTING STEEL AT LOCATIONS OF CLAMP PLATES, BOLSTERS AND NEOPRENE TO ENSURE PROPER FIT.
- ALL WELDS SHALL BE 5*16" FILET WELDS ALL AROUND UNLESS OTHERWISE NOTED.

NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- FOR NEW BEARING DETAILS AND NOTES, SEE SHEET 14.
- SHIM PLATE THICKNESS SHALL BE EQUAL TO THE TOTAL THICKNESS OF THE REQUIRED BOTTOM FLANGE COMPONENTS (I.E. BOTTOM FLANGE, COVER PLATES, NEOPRENE) - 1/16"
- SUPPORT MEMBERS SHALL BE WELDED ALL AROUND WITH 5/16" FILLET WELDS UNLESS OTHERWISE NOTED.
- ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 523.32.

LEGEND	
SYM.	DESCRIPTION
○	EXISTING FASTENER
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH BOLT IN EXISTING HOLE
⊗	REMOVE EXISTING FASTENER AND PLACE NEW HIGH STRENGTH COUNTER SUNK BOLT IN EXISTING HOLE
●	PLACE NEW HIGH STRENGTH BOLT IN NEW HOLE

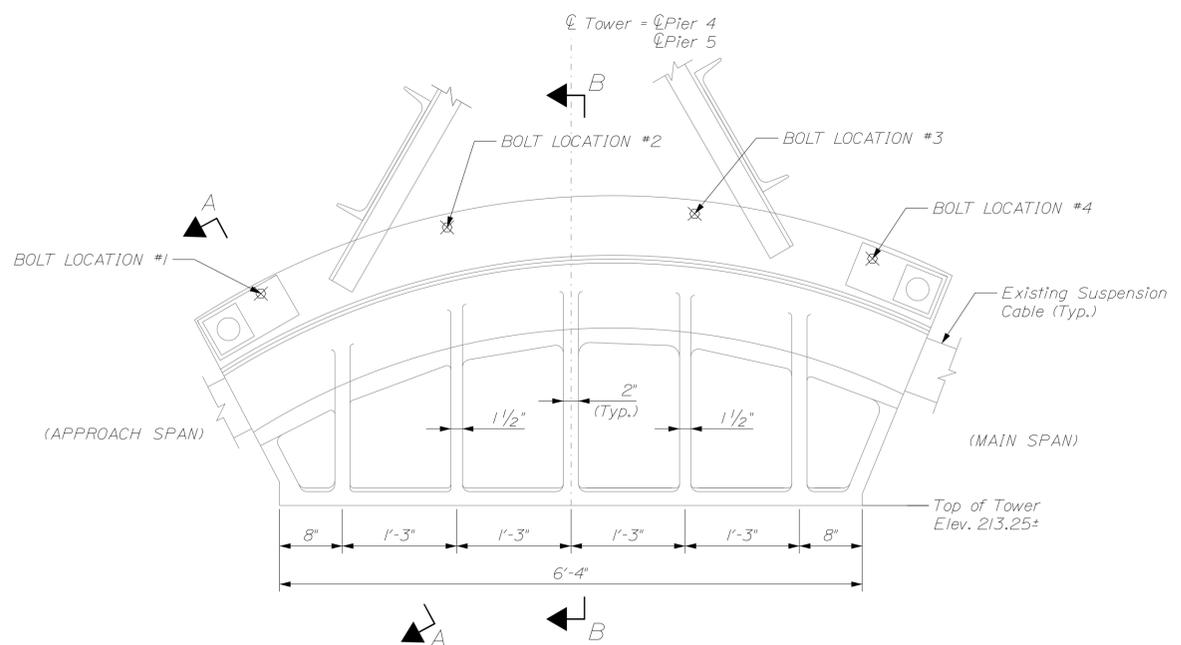
STATE OF MAINE		DEPARTMENT OF TRANSPORTATION		HP-1006(210)		WIN		BRIDGE NO. 3257		BRIDGE PLANS	
DEER ISLE-SEDGWICK BRIDGE		EGGEMOGGIN REACH		HANCOCK COUNTY		SUSPENSION BRIDGE BEARING REPLACEMENT 2 OF 2		SHEET NUMBER		15	
DESIGNED BY: E. LOWELL		CHECKED BY: S. JAHNES		DATE: 4/24/13		SIGNATURE:		P.E. NUMBER:		DATE:	
DESIGNED BY: S. JAHNES		CHECKED BY: T. STLOPPS		DATE: 4/24/13		SIGNATURE:		P.E. NUMBER:		DATE:	
DESIGNED BY: R. BACK		CHECKED BY: R. BACK		DATE: 4/24/13		SIGNATURE:		P.E. NUMBER:		DATE:	
REVISIONS 1		REVISIONS 2		REVISIONS 3		REVISIONS 4		FIELD CHANGES		DATE:	

PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGN/DETAILED	S. JAHNES	BY	E. LOWELL
CHECKED/REVIEWED	T. STLOPPS	DATE	4/24/13
DESIGN/DETAILED	R. BACK	BY	S. JAHNES
REVISIONS 1		DATE	
REVISIONS 2		DATE	
REVISIONS 3		DATE	
REVISIONS 4		DATE	
FIELD CHANGES		DATE	

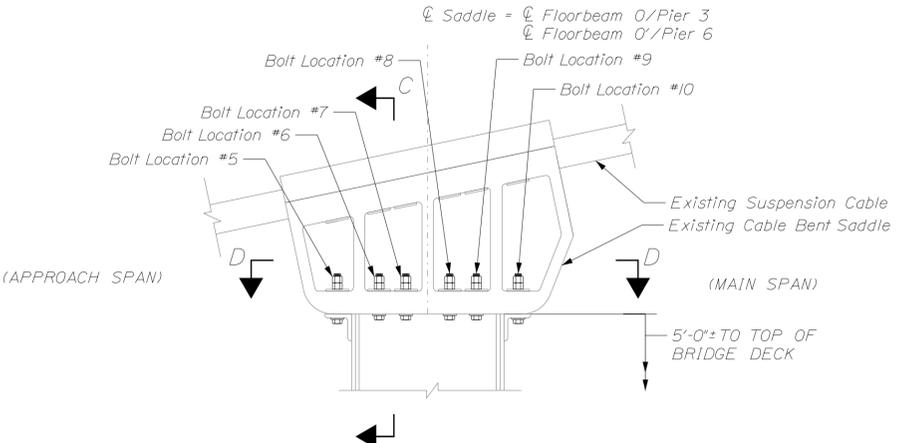
PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGN/DETAILED	S. JAHNES	BY	E. LOWELL
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DEER ISLE-SEDEGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDEGWICK HANCOCK COUNTY
CABLE SADDLE REPAIRS

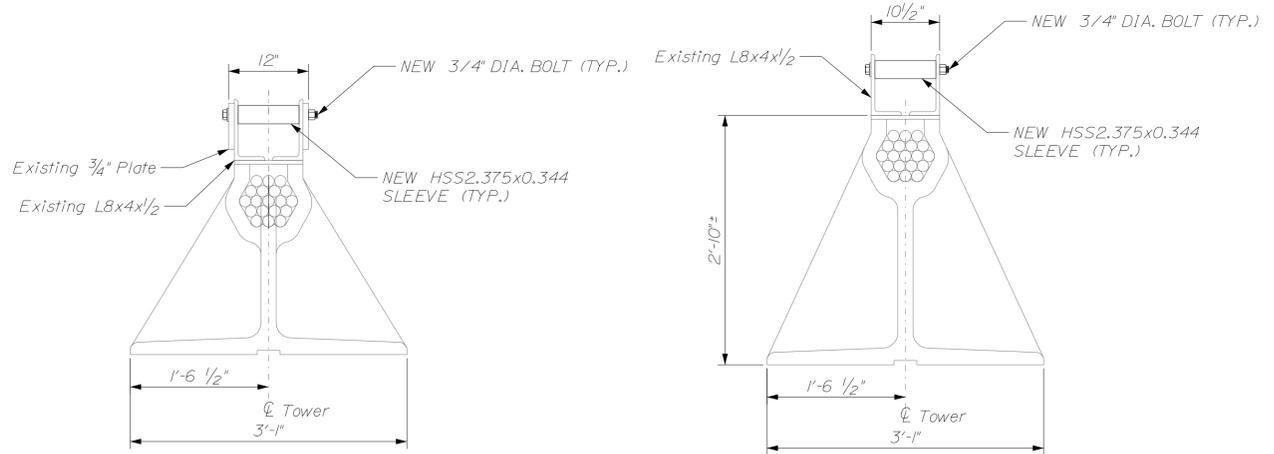
SHEET NUMBER
17
OF 19



TOWER SADDLE ELEVATION
(PIER 4 EAST SADDLE - EAST ELEVATION)
(PIER 4 WEST SADDLE - EAST ELEVATION)
(PIER 5 EAST SADDLE - WEST ELEVATION)
(PIER 5 WEST SADDLE - WEST ELEVATION)



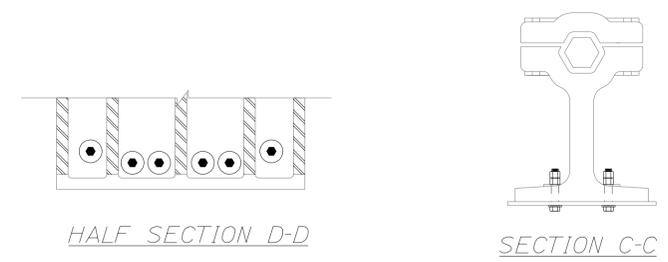
CABLE BENT SADDLE ELEVATION
(PIER 3 EAST SADDLE - WEST ELEVATION OPPOSITE HAND)
(PIER 3 WEST SADDLE - EAST ELEVATION)
(PIER 6 EAST SADDLE - WEST ELEVATION)
(PIER 6 WEST SADDLE - EAST ELEVATION OPPOSITE HAND)



SECTION A-A

SECTION B-B

TOWER SADDLE BOLT REPLACEMENT LOCATION		
PIER	SADDLE	BOLT LOCATION
PIER 3	EAST	#5
PIER 4	WEST	#3
PIER 4	EAST	#1
PIER 4	EAST	#2
PIER 4	EAST	#3



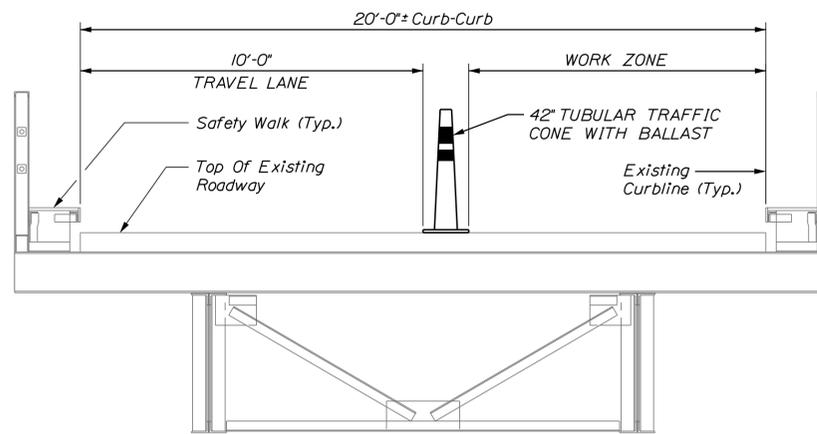
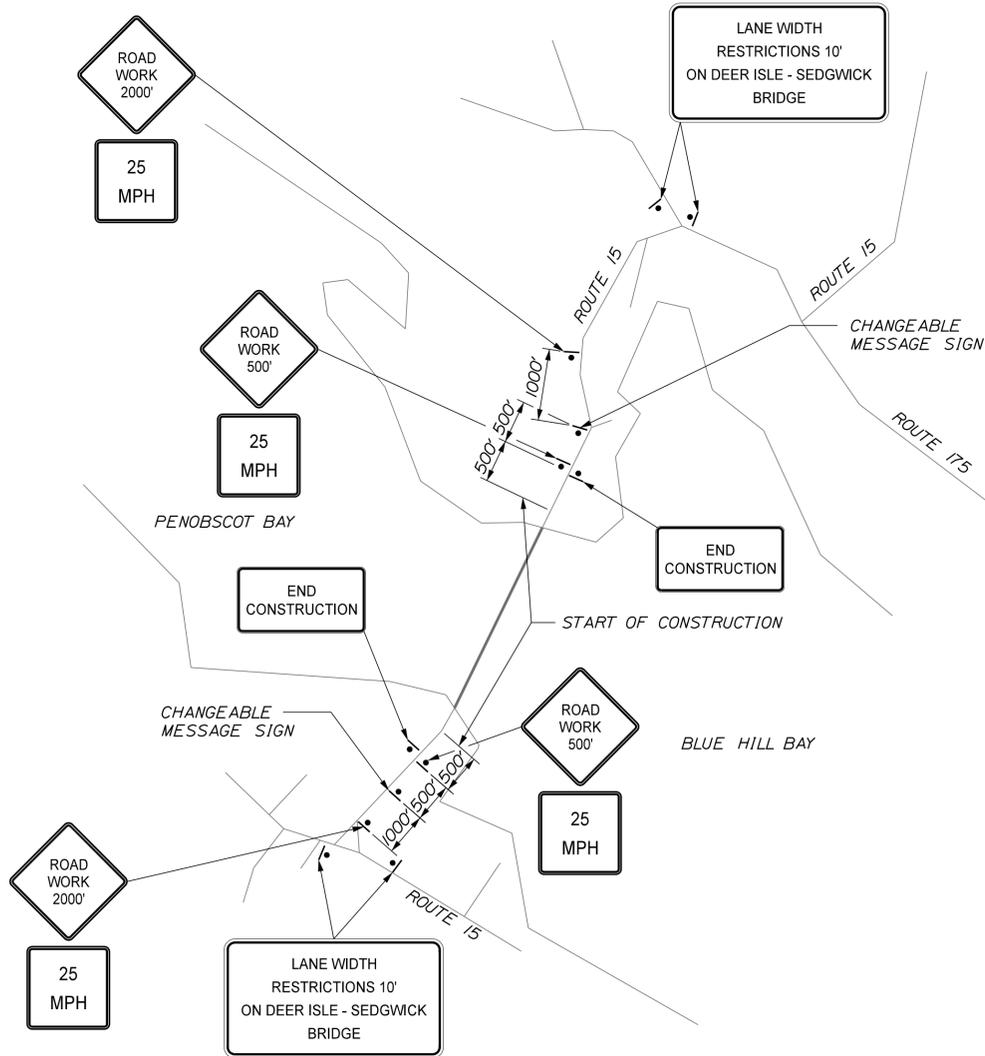
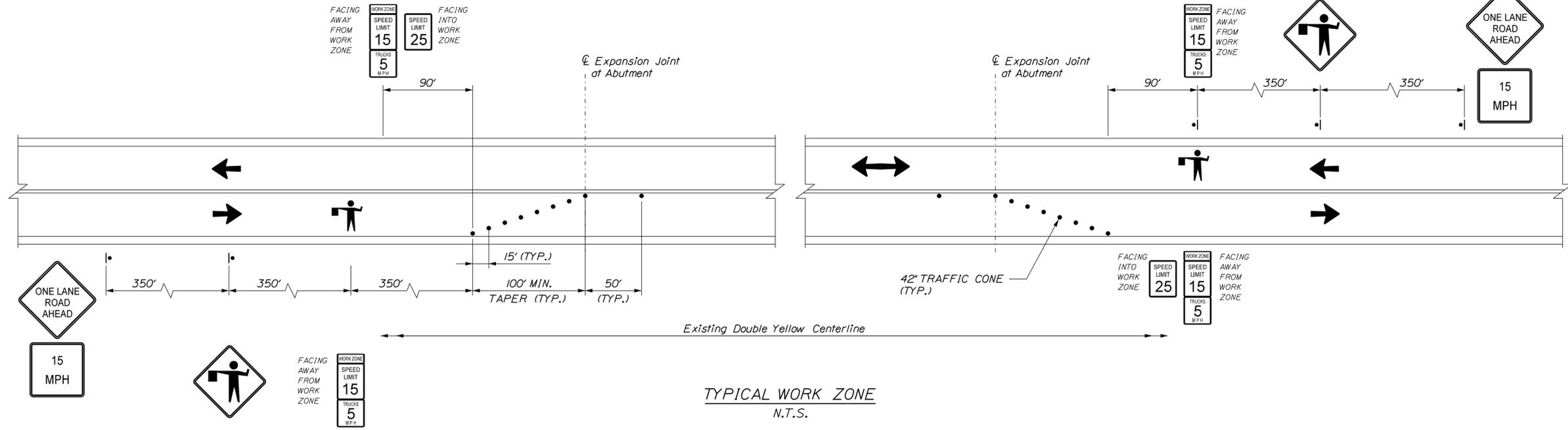
HALF SECTION D-D

SECTION C-C

CABLE BENT BOLT REPLACEMENT LOCATION		
PIER	SADDLE	BOLT LOCATION
PIER 5	WEST	#1
PIER 5	WEST	#2
PIER 5	EAST	#2
PIER 5	EAST	#3
PIER 5	EAST	#4
PIER 6	WEST	#5
PIER 6	WEST	#10

NOTES:

- FOR GENERAL NOTES SEE SHEET 2.
- FOR TOWER LOCATIONS SEE SHEET 3.
- BOLT REPLACEMENT AT LOCATIONS #1 THROUGH #4 SHALL BE 3/4" DIA. BOLT WITH SLEEVE AS SHOWN. ALL WORK ON THIS TASK TO BE PAID FOR UNDER ITEM 504.621.
- BOLT REPLACEMENT AT LOCATIONS #5 THROUGH #10 SHALL BE 1" DIA. HS BOLT WITH DOUBLE LOCKING NUTS. ALL WORK ON THIS TASK TO BE PAID FOR UNDER ITEM 504.622.
- FOR EACH REPAIR, ONLY ONE BOLT SHALL BE REMOVED AT A TIME.



NOTES:

1. FOR GENERAL NOTES SEE SHEET NO. 2.
2. ALL CONSTRUCTION SIGNING AND OTHER TRAFFIC MANAGEMENT DEVICES SHALL CONFORM WITH THE CURRENT MUTCD AND MAINE DOT SPECIFICATIONS.
3. ALL DISTANCES MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE RESIDENT.
4. MAXIMUM SPACING OF CHANNELIZING DEVICES IN A TAPER IS EQUAL IN FEET TO THE POSTED SPEED LIMIT (MPH).
5. MINIMUM TRAVEL LANE WIDTH SHALL BE 10 FEET.
6. FLAGGERS SHALL BE USED DURING ALL LANE CLOSURES. FLAGGER REGULATIONS SHALL CONFORM TO THE CURRENT MUTCD AND MAINE DOT SPECIFICATIONS.
7. SIGNS SHALL BE COVERED OR REMOVED WHEN THEY ARE NOT REQUIRED.
8. CONSTRUCTION SIGNS TO BE PAID FOR UNDER ITEM 652.35; FLAGGERS TO BE PAID FOR UNDER ITEM 652.38; 42\"/>

PROJ. MANAGER	E. LOWELL	DATE	4/24/13
DESIGNED/DETAILED	S. JAHNES	CHECKED/REVIEWED	E. LOWELL
DESIGNED/DETAILED	T. STLOPPS	CHECKED/REVIEWED	E. LOWELL
DESIGNED/DETAILED	R. BACK	CHECKED/REVIEWED	S. JAHNES
REVISIONS 1		DATE	
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REVISIONS 3		DATE	
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DEER ISLE-SEDGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY
TRAFFIC MANAGEMENT PLAN
1 OF 2

SHEET NUMBER

18

IDENTIFICATION NUMBER	SIZE OF SIGN (inch)		TEXT	NUMBER OF SIGNS REQUIRED	AREA IN SQUARE FEET
	WIDTH	HEIGHT			
G20-2	36	18		2	9.00
W20-1 (2000)	48	48		2	32.00
W20-1 (500)	48	48		2	32.00
W20-4	48	48		2	32.00
W20-7a	48	48		2	32.00
W13-1 (15)	30	30		2	12.50
W13-1 (25)	30	30		4	25.00
W-LR	72	48		4	96.00
R2-1	24	30		2	10.00
R2-1	24	30		4	20.00
R2-2	24	24		4	16.00
	24	12		4	8.00
TOTAL					324.50

NOTES:

- FOR GENERAL NOTES SEE SHEET NO. 2.
- THE SIGN SPACING FOR THE TRAFFIC MANAGEMENT SIGNING ARE MINIMUM DISTANCES. IF THERE IS A SIGHT DISTANCE PROBLEM FOR A SIGN, THE DISTANCES CAN BE INCREASED AS APPROVED BY THE ENGINEER.
- ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 652.361 UNLESS OTHERWISE NOTED.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
HP-1006(210)
WIN
10062.10
BRIDGE NO. 3257
BRIDGE PLANS

PROJ. MANAGER	E. LOWELL	BY	DATE
DESIGN-DETAILED	S. JAHNES	E. LOWELL	4/24/13
CHECKED-REVIEWED	T. STLOPPS	E. LOWELL	4/24/13
DESIGN-DETAILED	R. BACK	S. JAHNES	4/24/13
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REVISIONS 4			
FIELD CHANGES			

DEER ISLE-SEDGWICK BRIDGE
EGGEMOGGIN REACH
DEER ISLE-SEDGWICK HANCOCK COUNTY
TRAFFIC MANAGEMENT PLAN
2 OF 2

SHEET NUMBER

19
OF 19

