



Paul R. LePage  
GOVERNOR

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
16 STATE HOUSE STATION  
AUGUSTA, MAINE 04333-0016

David Bernhardt  
COMMISSIONER

April 13, 2012  
Subject: **Caribou & Conner Twp.**  
Federal Project No's: AC-IB1922(700)X  
& AC-IB-1923(300)X  
State PIN's: 019227.00 & 019233.00  
**Amendment No. 1**

Dear Sir/Ms:

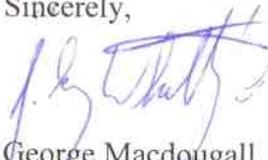
Make the following changes to the Bid Document:

In the Bid Book (page 121) **REMOVE** "SPECIAL PROVISION, SECTION 403, HOT MIX ASPHALT" 1 page dated April 4, 2012 (Titled "Caribou" in the upper right corner) and **REPLACE** with the attached new "SPECIAL PROVISION, SECTION 403, HOT MIX ASPHALT" 2 pages dated April 13, 2012.

In the Bid Book (pages 122 and 123) **REMOVE** "SPECIAL PROVISION, SECTION 403, HOT MIX ASPHALT" 2 pages dated April 4, 2012 (Titled "Caribou – Conner TWP" in the upper right corner) and **REPLACE** with the attached new "SPECIAL PROVISION, SECTION 403, HOT MIX ASPHALT" 2 pages dated April 13, 2012.

Consider these changes and information prior to submitting your bid on May 2, 2012.

Sincerely,



George Macdougall  
Acting Contracts & Specifications Engineer



MADE WITH RECYCLED PAPER

**SPECIAL PROVISION**  
**SECTION 403**  
**HOT MIX ASPHALT**

Desc. Of Course	Grad Design.	Item Number	Bit Cont. % of Mix	Total Thick	No. Of Layers	Comp. Notes
<b><u>1 ¾" Mill with 1 ¾" HMA Overlay Areas</u></b>						
<b><u>Mainline Travelway, Shoulders, and Approach Roads</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ½"	1	1,4,7,20
<b><u>Spot Shims – Delaminated Areas As Directed</u></b>						
Shim	9.5 mm	403.211	N/A	variable	1/more	1,2,4,7
<b><u>Shoulder Repair Areas</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ½"	1	1,4,7,20
Base	12.5 mm	403.213	N/A	1 ½"	1	1,4,10

**COMPLEMENTARY NOTES**

1. The required PGAB for this mixture will meet a **PG 58-28** grading. All asphalt grades utilized on the Project shall be treated with an approved liquid anti-strip. PG binders shall be unmodified and treated with 0.50 percent anti-strip by weight of asphalt binder used. The Contractor shall stipulate which PGAB grading will be used to construct the entire HMA pavement structure prior to starting work. Changes to the PGAB grading must be approved by the Department prior to the change in PGAB grading.
2. The density requirements are waived.
4. The design traffic level for mix placed shall be 0.3 to <3 million ESALS. The design, verification, Quality Control, and Acceptance tests for this mix will be performed at **50 gyrations**.
7. Section 106.6 Acceptance, (1) Method A.
10. Section 106.6 Acceptance, (2) Method D.
20. The Contractor may place the specified HMA pavement course, not to exceed 2" inch (50mm) compacted depth, over the full single travel lane width, for each production day. If this option is utilized the Contractor will be required to place a matching course of HMA over the adjacent section of travel lane before the end of the following calendar day. The Contractor will also be responsible for installing additional warning signage that clearly defines the centerline elevation differential hazard, as well as additional centerline delineation such as double RPM application, or temporary painted line. The Traffic Control Plan shall be amended to include this option and the additional requirements. All signs and traffic control devices will conform to Section 719.01, and Section 652, and will be installed to the work, at a maximum spacing of 0.50 mile [0.80 km] for the entire length of effected roadway section. On roadways with two-way traffic, the Contractor will be required to place the specified course over the full width of the mainline traveled way being paved prior to opening the sections to weekend or holiday traffic. If this option is utilized, all additional signing, labor, traffic control devices, or incidentals will not be paid for directly, will be considered incidental to the appropriate 652 items.

**Caribou  
AC-IB-1922(700)X  
U.S. Route 1 / Route 89  
1 ½' Mill & Fill  
April 13, 2012**

A tack coat of emulsified asphalt, RS-1, Item 409.15 shall be applied to any existing pavement at a rate of approximately 0.025 gal/yd<sup>2</sup>, and on milled pavement approximately 0.05 gal/yd<sup>2</sup> prior to placing a new course. A fog coat of emulsified asphalt shall be applied between shim /base courses and the surface course, at a rate not to exceed 0.025 gal/yd<sup>2</sup>. Tack used between layers of pavement will be paid for at the contract unit price for Item 409.15 Bituminous Tack Coat.

**SPECIAL PROVISION**  
**SECTION 403**  
**HOT MIX ASPHALT**

<b>Desc. Of Course</b>	<b>Grad Design.</b>	<b>Item Number</b>	<b>Bit Cont. % of Mix</b>	<b>Total Thick</b>	<b>No. Of Layers</b>	<b>Comp. Notes</b>
<b><u>1 ¼” HMA Overlay Areas</u></b>						
<b><u>Mainline Travelways, Shoulders, Trucklanes, and Approach Roads</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ¼”	1	1,4,7,20,25
Shim	9.5 mm	403.211	N/A	variable	1/more	1,2,4,7,20
<b><u>1 ¾” Mill &amp; 1 ¾” HMA Overlay Areas</u></b>						
<b><u>Mainline Travelway &amp; Shoulders</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ¾”	1	1,4,7,20
<b><u>Spot Shims – Delaminated Areas As Directed</u></b>						
Shim	9.5 mm	403.211	N/A	variable	1/more	1,2,4,7
<b><u>Shoulder Repair Areas</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ¼”	1	1,4,7,20
Base	12.5 mm	403.213	N/A	1 ¾”	1	1,4,10
<b><u>Pipe Trench Areas</u></b>						
Wearing	9.5 mm	403.210	N/A	1 ¼”	1	1,4,7,20
Base	12.5 mm	403.213	N/A	4”	2	1,4,10
<b><u>Crack Repair Areas</u></b>						
Wearing	9.5 mm	403.102	N/A	variable	1/more	23
<b><u>Drives, Misc.</u></b>						
Wearing	9.5 mm	403.209	N/A	1 ¼”	1/more	2,3,10,11,14

**COMPLEMENTARY NOTES**

1. The required PGAB for this mixture will meet a **PG 58-28** grading. All asphalt grades utilized on the Project shall be treated with an approved liquid anti-strip. PG binders shall be unmodified and treated with 0.50 percent anti-strip by weight of asphalt binder used. The Contractor shall stipulate which PGAB grading will be used to construct the entire HMA pavement structure prior to starting work. Changes to the PGAB grading must be approved by the Department prior to the change in PGAB grading.
2. The density requirements are waived. In addition, the use of an oscillating steel roller shall be required to compact all HMA pavements placed on bridge decks.
3. The design traffic level for mix placed shall be <0.3 million ESALS.
4. The design traffic level for mix placed shall be 0.3 to <3 million ESALS. The design, verification, Quality Control, and Acceptance tests for this mix will be performed at **50 gyrations**.
7. Section 106.6 Acceptance, (1) Method A.
10. Section 106.6 Acceptance, (2) Method D.
11. The combined aggregate gradation required for this item shall be classified as a 9.5mm “**fine graded**” mixture, (using the Primary Control Sieve control point) as defined in 703.09.
14. A mixture meeting the requirements of section 703.09 Grading ‘D’, with a minimum PGAB content of 6%, and the limits of Special Provision 401, Table 9 (Drives and Sidewalks) for PGAB content and gradation may be substituted for this item. A job mix formula shall be submitted to the department for approval.

20. The Contractor may place the specified HMA pavement course, not to exceed 2” inch (50mm) compacted depth, over the full single travel lane width, for each production day. If this option is utilized the Contractor will be required to place a matching course of HMA over the adjacent section of travel lane before the end of the following calendar day. The Contractor will also be responsible for installing additional warning signage that clearly defines the centerline elevation differential hazard, as well as additional centerline delineation such as double RPM application, or temporary painted line. The Traffic Control Plan shall be amended to include this option and the additional requirements. All signs and traffic control devices will conform to Section 719.01, and Section 652, and will be installed to the work, at a maximum spacing of 0.50 mile [0.80 km] for the entire length of effected roadway section. On roadways with two-way traffic, the Contractor will be required to place the specified course over the full width of the mainline traveled way being paved prior to opening the sections to weekend or holiday traffic. If this option is utilized, all additional signing, labor, traffic control devices, or incidentals will not be paid for directly, will be considered incidental to the appropriate 652 items.
23. Refer to Special Provision 401 –Hot Mix Asphalt for Special Areas for specifics.
25. The depth of the HMA 403.210 surface overlay on the mainline travelway will be 1 ¼”. The depth on the shoulders will be tapered so that the depth at the outside edge of the shoulder will be ¾”. See typical sections for more information.

#### Tack Coat

A tack coat of emulsified asphalt, RS-1, Item 409.15 shall be applied to any existing pavement at a rate of approximately 0.025 gal/yd<sup>2</sup>, and on milled pavement approximately 0.05 gal/yd<sup>2</sup> prior to placing a new course. A fog coat of emulsified asphalt shall be applied between shim /base courses and the surface course, at a rate not to exceed 0.025 gal/yd<sup>2</sup>.

Tack used between layers of pavement will be paid for at the contract unit price for Item 409.15 Bituminous Tack Coat.