



Paul R. LePage
GOVERNOR

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE 04333-0016

David Bernhardt
COMMISSIONER

May 7, 2012
Subject: **Kittery**
Federal Project No: BH-1671(100)X
State WIN: 016710.10
Amendment No. 1

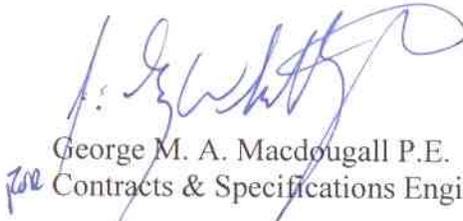
Dear Sir/Ms:

Make the following change to the Bid Document:

In the Bid Book **REMOVE** "SPECIAL PROVISION, SECTION 105, GENERAL SCOPE OF WORK, (Limitations of Operations)" 3 pages dated April 27, 2012 and **REPLACE** with the attached new "SPECIAL PROVISION, SECTION 105, GENERAL SCOPE OF WORK, (Limitations of Operations)" 4 pages dated May 4, 2012.

Consider this change and information prior to submitting your bid on May 16, 2012.

Sincerely,



George M. A. Macdougall P.E.
Contracts & Specifications Engineer



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SPECIAL PROVISION
SECTION 105
GENERAL SCOPE OF WORK
(Limitations of Operations)

The Limitations of Operations noted in this section are related to all construction activities occurring on the Sarah Mildred Long Bridge.

1. The Sarah Mildred Long Bridge shall remain open to two lanes of vehicular traffic at all times unless otherwise noted herein, or approved by the Resident.
2. All work related to the layout, installation and anchoring of the prefabricated bridge barrier system, bridge mounted thrie rail system and end treatments shall be completed as night work with the bridge closed to vehicular traffic. The signed detours noted in the traffic control plans shall be implemented during all bridge closures. The Contractor shall set up portable changeable message signs (PCMS) at least 2 days in advance of all planned bridge closures notifying traffic of the planned closures and detour routes. The Resident may request changes to the locations of the signs and the associated messages, as required for the project, at no additional cost to the Department. The PCMS shall remain in place for the duration of the project. The Contractor shall plan and sequence the work to minimize the number of bridge closures required to complete the work.
3. The Contractor will be permitted to work seven days per week. The bridge may be closed to vehicular traffic during the following hours:
 - a. Monday: Beginning at 10:00 p.m.; ending Tuesday at 5:00 a.m.
 - b. Tuesday: Beginning at 10:00 p.m.; ending Wednesday at 5:00 a.m.
 - c. Wednesday: Beginning at 10:00 p.m.; ending Thursday at 5:00 a.m.
 - d. Thursday: Beginning at 10:00 p.m.; ending Friday at 5:00 a.m.
 - e. Friday: Beginning at 10:00 p.m.; ending Saturday at 8:00 a.m.
 - f. Saturday: Beginning at 8:00 p.m.; ending Sunday at 9:00 a.m.
 - g. Sunday: Beginning at 8:00 p.m.; ending Monday at 5:00 a.m.

No bridge closures will be allowed during the Holiday periods defined in Special Provision 107, Contract Time.

4. The Sarah Mildred Long Bridge is currently posted for load at 20 tons. The Contractor will be required to adhere to this restriction at all times when the bridge is open to the general public.
5. During periods of bridge closure, the Contractor will be permitted to operate approved vehicles and equipment weighing up to 31 tons on the bridge provided the following requirements are met:
 - a. All equipment shall operate at 10 miles per hour or less while on the bridge.

- b. The Contractor will be limited to no more than one vehicle or piece of equipment per lane, per span.
 - c. All vehicles and equipment shall operate along the centerline of the bridge when possible, and always within the striped lanes.
 - d. All proposed vehicles and equipment with a loaded weight exceeding 20 tons shall be submitted to the Department for approval at least one week in advance of their planned use.
 - i. The submittal for each vehicle shall include the following:
 - 1. Vehicle description and proposed use.
 - 2. Number and spacing of axles.
 - 3. Total loaded weight and weight distribution.
 - 4. When vehicles will be used for hauling materials, the submittal shall clearly state the type(s) of material to be hauled and the maximum load proposed to be carried by the vehicle.
 - ii. The submittal for each piece of equipment shall include the following:
 - 1. Equipment description and proposed use
 - 2. Number and spacing/length of axles/tracks
 - 3. Proposed equipment configuration
 - 4. Equipment weight and load distribution
 - e. When vehicles or equipment will be used for lifting and/or hoisting equipment, materials, or personnel, the submittal shall provide the proposed location of the vehicle or equipment, maximum lifted load, maximum pick radius, and other relevant information necessary to determine the location and intensity of the resulting loads on the bridge deck.
 - f. The Department reserves the right to reject the use of any vehicle or piece of equipment, regardless of weight, if in the judgment of the Department, its use could have an unacceptable effect on the bridge structure.
- 6. The Contractor will not be permitted to stockpile or store materials on the bridge beyond those required to complete each night's work.
 - 7. The Contractor shall sequence the work on the lift span in a manner that meets the requirements of Special Provision 880, Span Balancing, at all times.
 - 8. The Contractor shall progress the barrier and thrie rail installation work such that an approved crashworthy end treatment is in place at the ends of all barrier and thrie rail runs prior to reopening the bridge to traffic. All permanent and temporary end treatments shall meet the requirements of Special Provision 606, Crash Cushions.

9. The Contractor shall note that all work will occur from the roadway or railroad decks, and in some cases will require the use of a snooper truck to access the underside of the upper deck. No barges or waterborne platforms will be allowed for this work. The work shall be completed in a manner that results in no vertical reduction of the navigational openings due to the presence of any construction equipment when marine traffic is present.
10. The Contractor will not be permitted to access or operate vehicles or equipment from the railroad deck located on the lower level of the truss, without prior coordination and approval by PanAm Railroad. All coordination efforts and associated costs with required flagger protection, railroad liability insurance and railroad safety training requirements shall be the sole responsibility of the Contractor. The Contractor shall plan the work accordingly.
11. The Contractor shall be aware that PanAm Railroad is planning a rail movement in the July or August time frame. During the rail movement, the retractable span will be closed and if the rail movement occurs during the construction of this project, the Contractor will be required to move personnel off the bridge along with any equipment or materials that may impede the rail movement. The Contractor shall plan the work accordingly.
12. The Contractor is required to complete the work in a manner that does not allow any construction debris, materials or equipment to fall into the waterway during times when marine traffic is present. Any costs associated with protection of the waterway from falling objects shall be considered incidental to pay item 659.10, Mobilization.
13. The Contractor shall complete the work in a manner that allows a vessel to pass through the retractable span (between bridge joints 20 and 21) upon request. All equipment, construction materials and personnel shall be moved out of the retractable span navigable opening within five minutes of a vessel requesting passage through the opening. No adjustments to the railroad retractable span (either opening or closing the span) will be made to accommodate work being completed as part of this project.
14. The Contractor shall complete the work in a manner that allows the lift span to be operated on an as-needed basis at all times. The Contractor will be required to remove all equipment, vehicles and personnel from the lift span within 15 minutes of being notified of a required lift span opening. In addition, all materials located on the lift span that have not been balanced in accordance with Special Provision 880 shall be removed from the lift span within 15 minutes of being notified of a required opening. The Contractor shall note that openings of the lift span will likely be required, even during nighttime hours.
15. The Contractor is required to maintain nightly communication with the Bridge Operator and Portsmouth Pilots, to understand nightly scheduled vessel passage and associated lift span operations. The contractor shall have a VHF-FM marine radio, set to the bridge communication channels 16/13 or the designated channel for the bridge.

16. The Contractor shall publish a Notice To Mariners which outlines the work schedule for the duration of the project. The notice shall be published in accordance with the USCG requirements. The Contractor shall contact the Coast Guard District Commander upon notice to proceed to publish the work schedule. Any updates to the work schedule shall be submitted to the Coast Guard District Commander.