



Paul R. LePage  
GOVERNOR

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
16 STATE HOUSE STATION  
AUGUSTA, MAINE 04333-0016

November 3, 2011  
Subject: **Westbrook**  
Federal Project No's: BH-1106(300)X &  
BH-1106(400)X  
State PINs: 011063.00 & 011064.00  
**Amendment No. 2**

David Bernhardt  
COMMISSIONER

Dear Sir/Ms:

The following questions have been received:

**Question:** SP 109.5 defines compensable delays and references a weather related uncontrollable event as a possibility for additional compensation should an event occur. SP 510.07 contradicts SP109.5 by stating the contractor is to repair or replace the Special Detour if it is damaged by high water or by any other cause without compensation. Does SP109.5 over ride SP510.07? If not, how can the contractor be expected to assume the risk associated with an uncontrollable event if they don't know if it is going to happen or not and to what magnitude?

**Response:** The Special Provision 109.5 takes precedence over the 510.07.

**Question:** Is the Maybe Bridge MDOT acquired on the Cribstone Bridge replacement project available for use on this project?

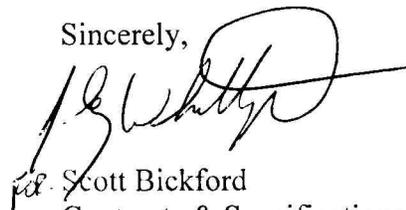
**Response:** No

**Question:** In reference to amendment #1, can the detour be constructed of fill adjacent to the island area if it is contained within a cofferdam and does not exceed the temporary impacts of 5,000 SF?

**Response:** Yes, so long as all contained fill material is removed by the end of the project and the temporary impact does not exceed 5000 s.f.

Consider this information prior to submitting your bid on November 9, 2011.

Sincerely,



for Scott Bickford  
Contracts & Specifications Engineer



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