

Calais Branch Corridor Rehabilitation

Downeast-Sunrise Trail Extension

Beal Avenue to Railroad Siding Road
City of Ellsworth to Town of Hancock

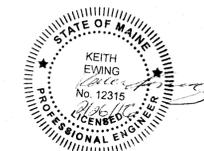
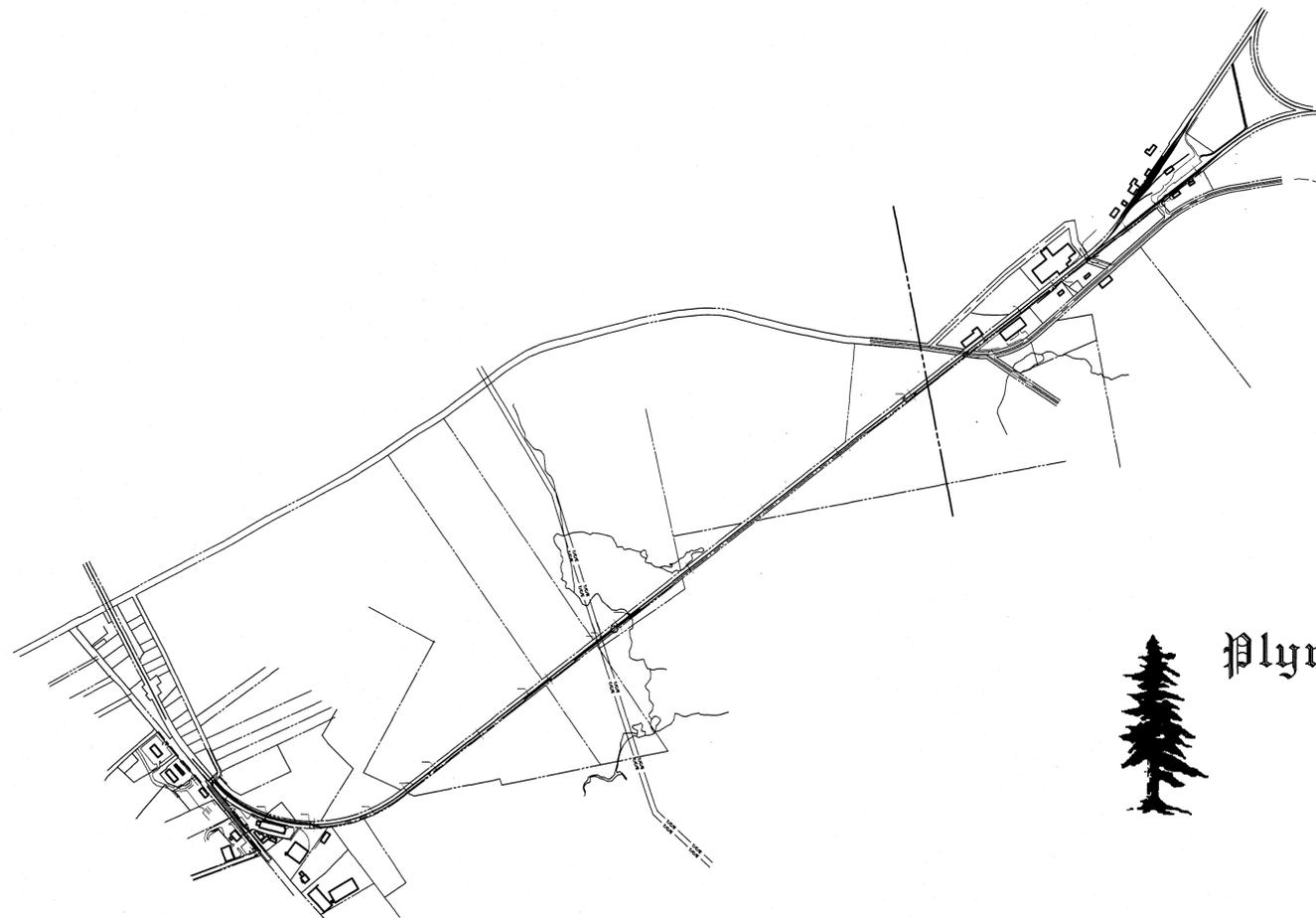
Bureau of Parks & Lands, Maine Department of Agriculture, Conservation & Forestry

APPROPRIATION No. CT-20141218-02185-014-01A-8463-84-7301

Bureau of General Services Project Number: PT 2492

Maine Department of Transportation Project Number: 0207-23058, PIN #014259.03

MARCH 1, 2016



Plymouth Engineering, Inc.

P.O. Box 46 30 Lower Deroit Road
Plymouth, Maine 04969

Fax: (207) 257-2130 Tel: (207) 257-2071

infor@plymouthengineering.com
www.plymouthengineering.com



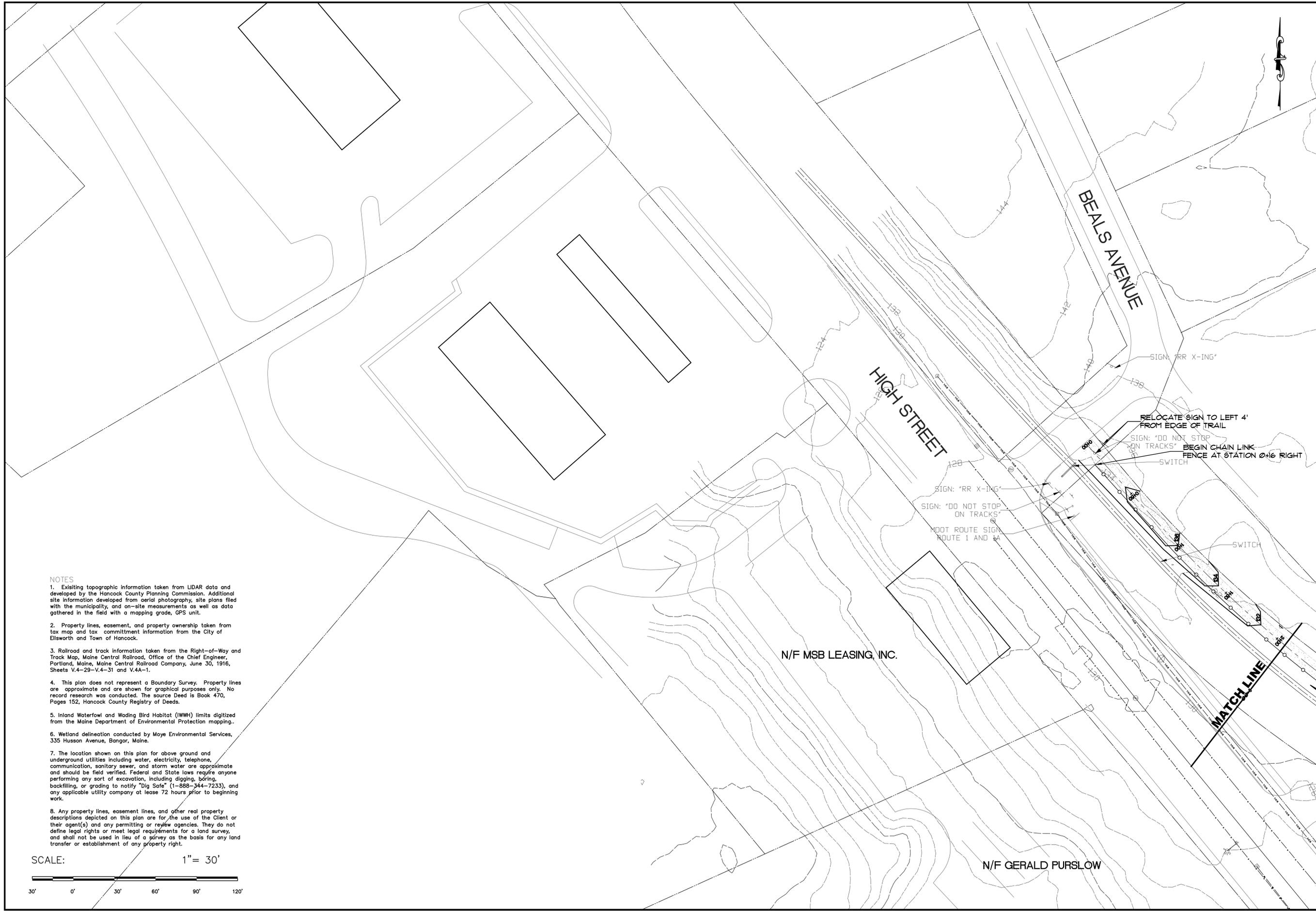
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- C2B - Station 2+50-11+00
- C2C - Station 11+00-21+00
- C2D - Station 21+00-30+00
- C2E - Station 30+00-38+50
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- C2J - Station 74+00-83+00
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- C12 - Trail Sections-Station 56+25-57+08
- C13 - Trail Sections-Station 57+08-112+89
- C14 - Typical Trail Sections-Non Wetland
- C15 - Typical Trail Sections-Wetland Areas

Issued for:
 Review
 Bid
 Construction
 Set No.:



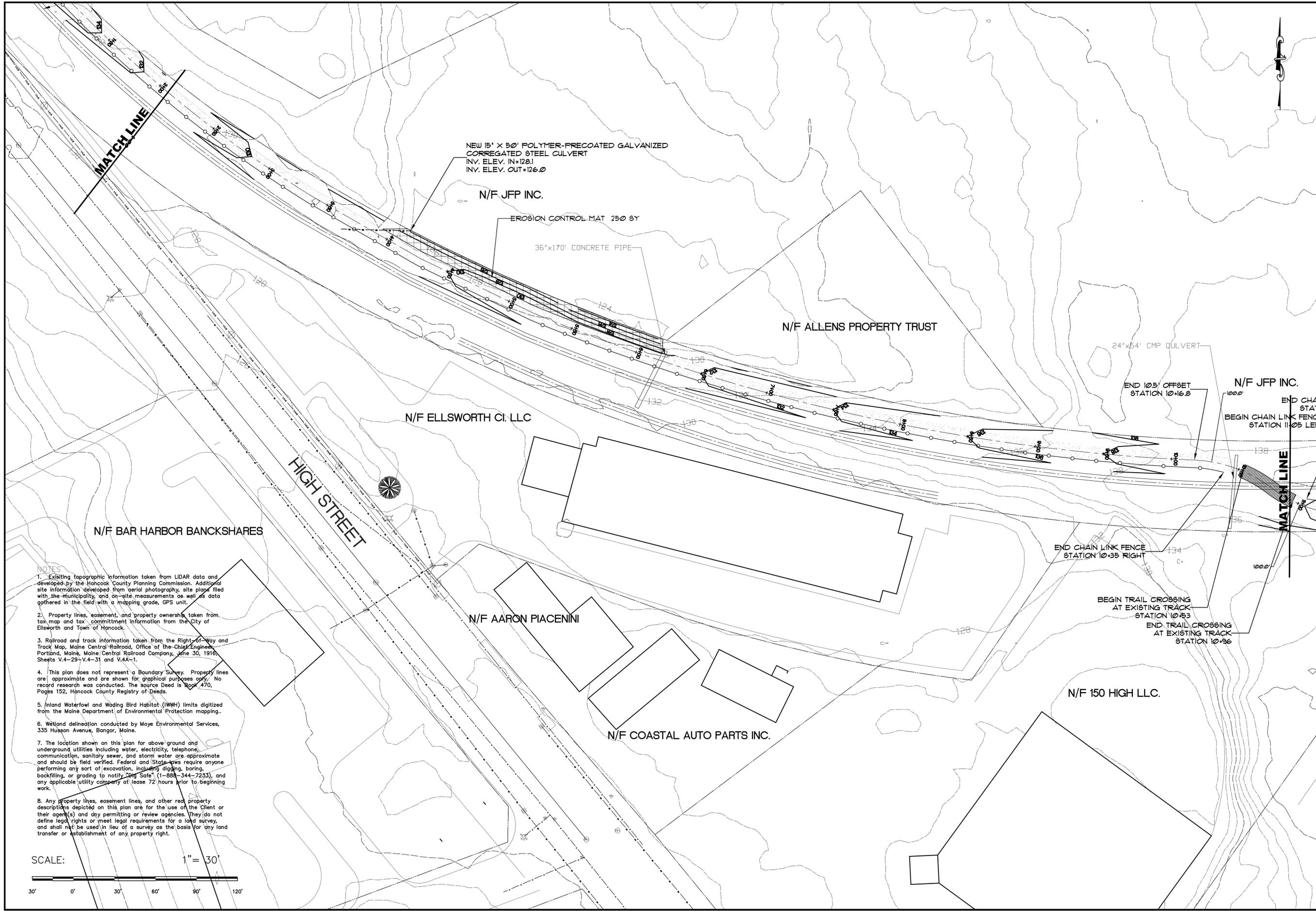
NOTES

1. Existing topographic information taken from LIDAR data and developed by the Hancock County Planning Commission. Additional site information developed from aerial photography, site plans filed with the municipality, and on-site measurements as well as data gathered in the field with a mapping grade, GPS unit.
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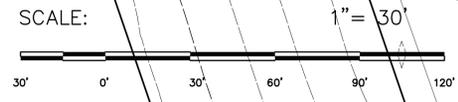


REVISIONS NO. DATE DESCRIPTION 	PROJECT NAME: Downeast/Sunrise Trail Extension Calais Branch-Maine Central Railroad Ellsworth-Hancock Maine Station 0+00 - 2+50	PROJECT NO. H204 DRAWING NO. H204Bae5.dwg FIELDBOOK: N/A SCALE: AS SHOWN DATE ISSUED: March 1, 2016	DESIGNED: FSM DRAWN: FSM CHECKED: SEB APPROVED: KGE PLAN DATE: March 1, 2016 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333	Plymouth Engineering, Inc. P.O. Box 48 30 Lower Detroit Road Plymouth, Maine 04969 Tel: (207) 287-2071 Fax: (207) 287-2190 info@plymouthengineering.com www.plymouthengineering.com	SHEET 2 OF 34 <h1 style="font-size: 2em; margin: 0;">C2A</h1>
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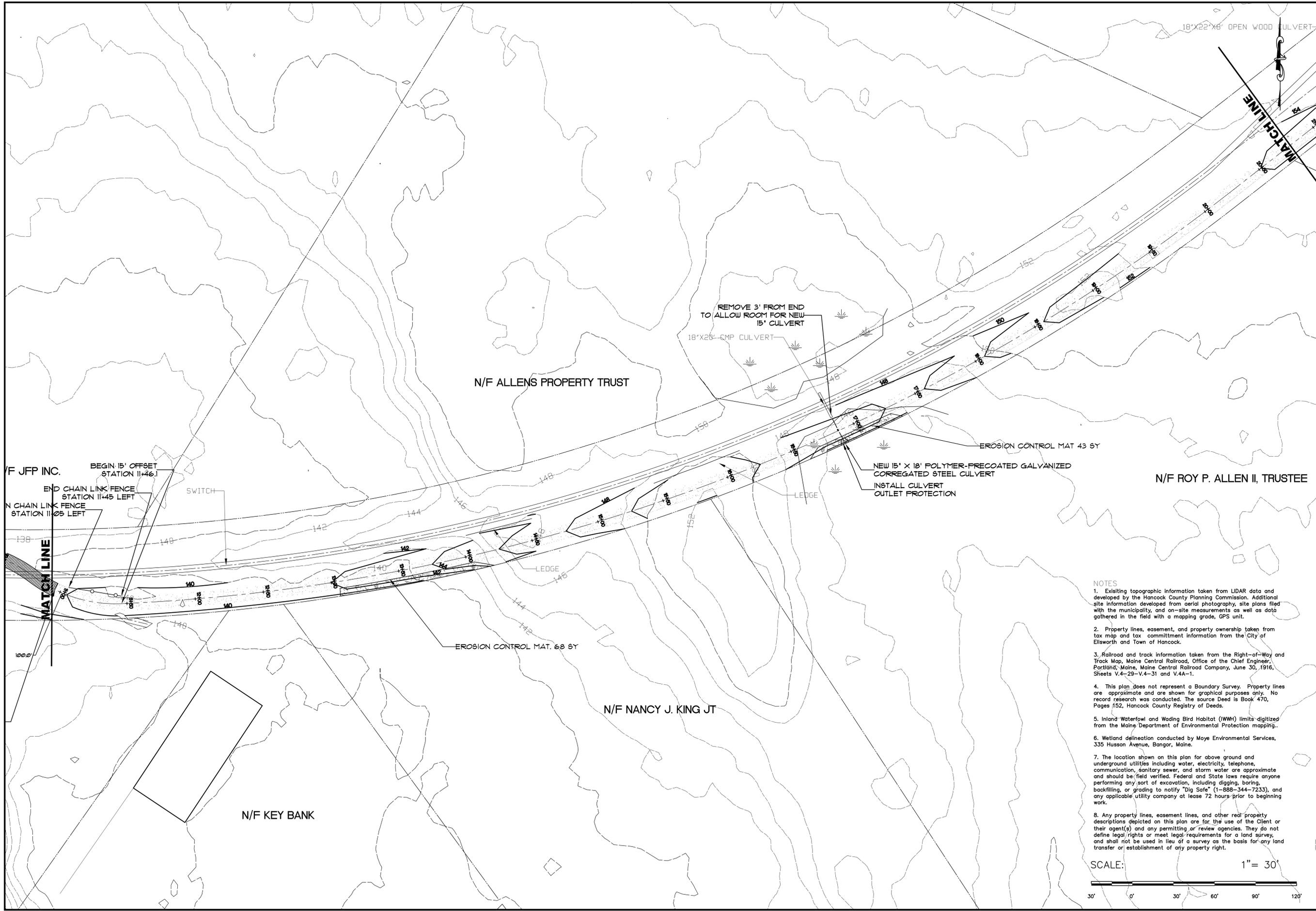


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REVISIONS NO. DATE DESCRIPTION _____ _____ _____	
PROJECT NAME: Downeast/Sunrise Trail Extension Calais Branch-Maine Central Railroad Station 2+50 - 11+00	
PROJECT NO. 14204 DRAWING NO. 14204Base5.dwg FIELDBOOK: N/A SCALE: AS SHOWN DATE ISSUED: March 1, 2016	DESIGNER: FSM DRAWN: FSM CHECKED: SEB APPROVED: KGE PLAN DATE: March 1, 2016 CLIENT: Plymouth Engineering, Inc. Department of Agriculture, Conservation and Forestry 10 Ells Lane Augusta, Maine 04333
Plymouth Engineering, Inc. P.O. Box 48 30 Lower Detroit Road Plymouth, Maine 04969 Tel: (207) 267-2071 Fax: (207) 267-2190 info@plymouthengineering.com www.plymouthengineering.com	
SHEET 3 OF 34 C2B	

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NO.	DATE	REVISIONS DESCRIPTION

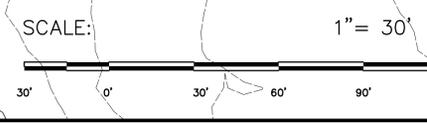
PROJECT NAME: Downeast/Sunrise Trail Extension
 Client: Calais Branch-Maine Central Railroad
 Location: Calais, Maine
 Station: 11+00 - 21+00

PROJECT NO. 14204
 DRAWING NO. 14204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

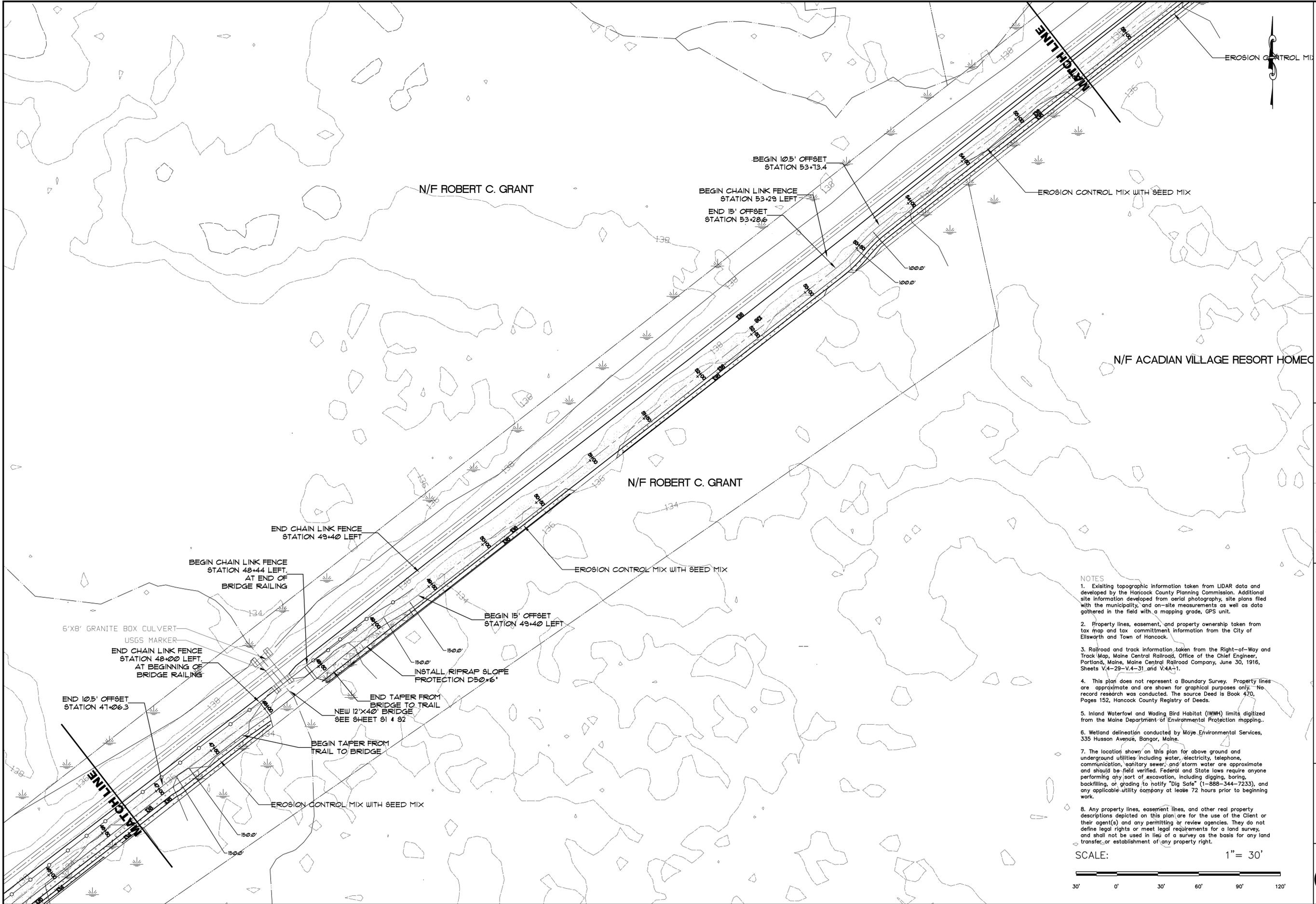
DESIGNED: FSM
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 CHECKED: SEB
 APPROVED: KGE
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REVISIONS
 SHEET 4 OF 34
C2C

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SCALE: 1" = 30'

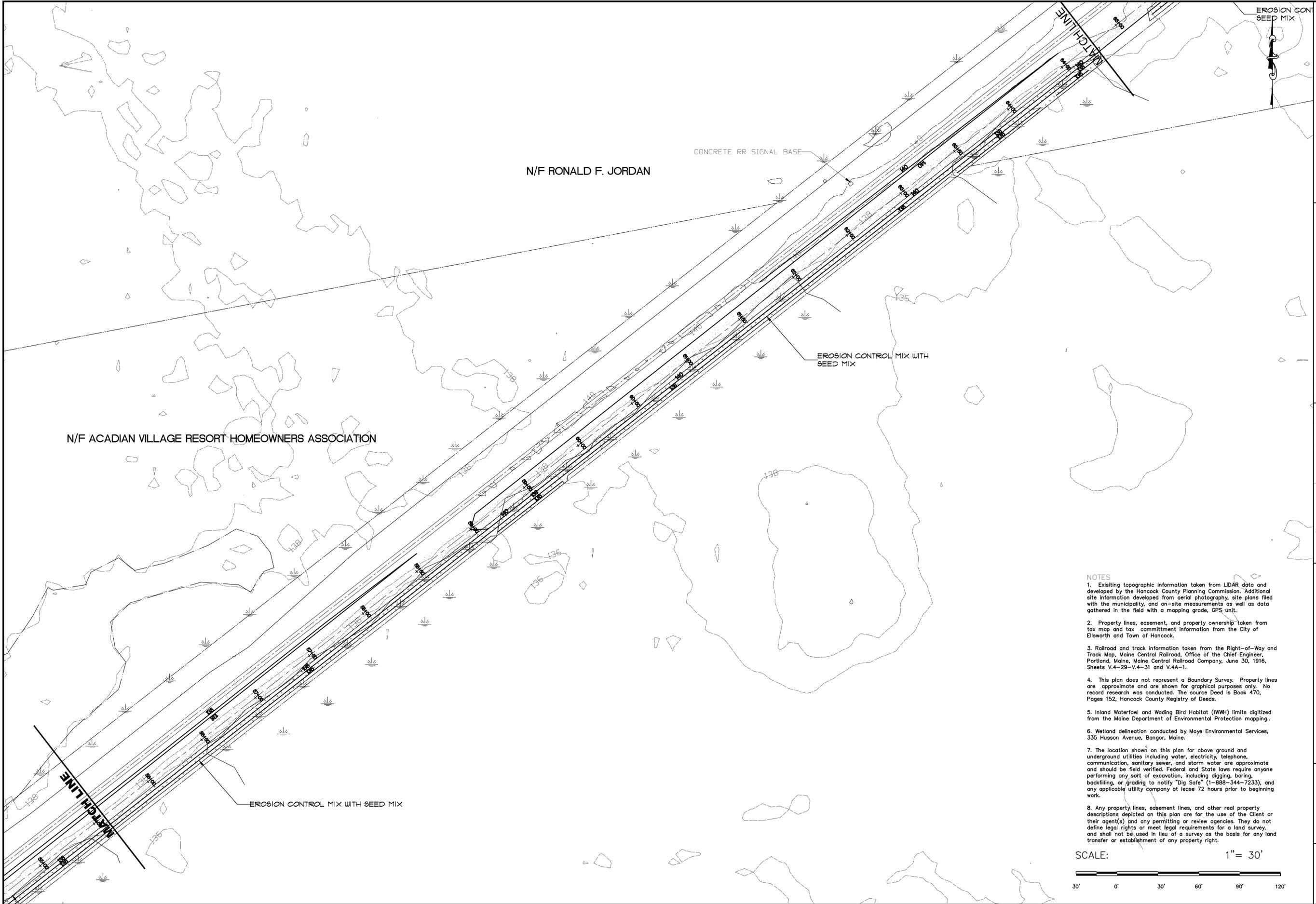
NO.	DATE	REVISIONS DESCRIPTION

PROJECT NAME: Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 PROJECT NO: 14204
 DRAWING NO: 14204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry
 August, Maine 04853

DESIGNED: FSM
 DRAWN: FSM
 CHECKED: SEB
 APPROVED: KGE
 PLAN DATE: March 1, 2016
 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry August, Maine 04853

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 APPROVED: KGE
 PLAN DATE: March 1, 2016
 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333

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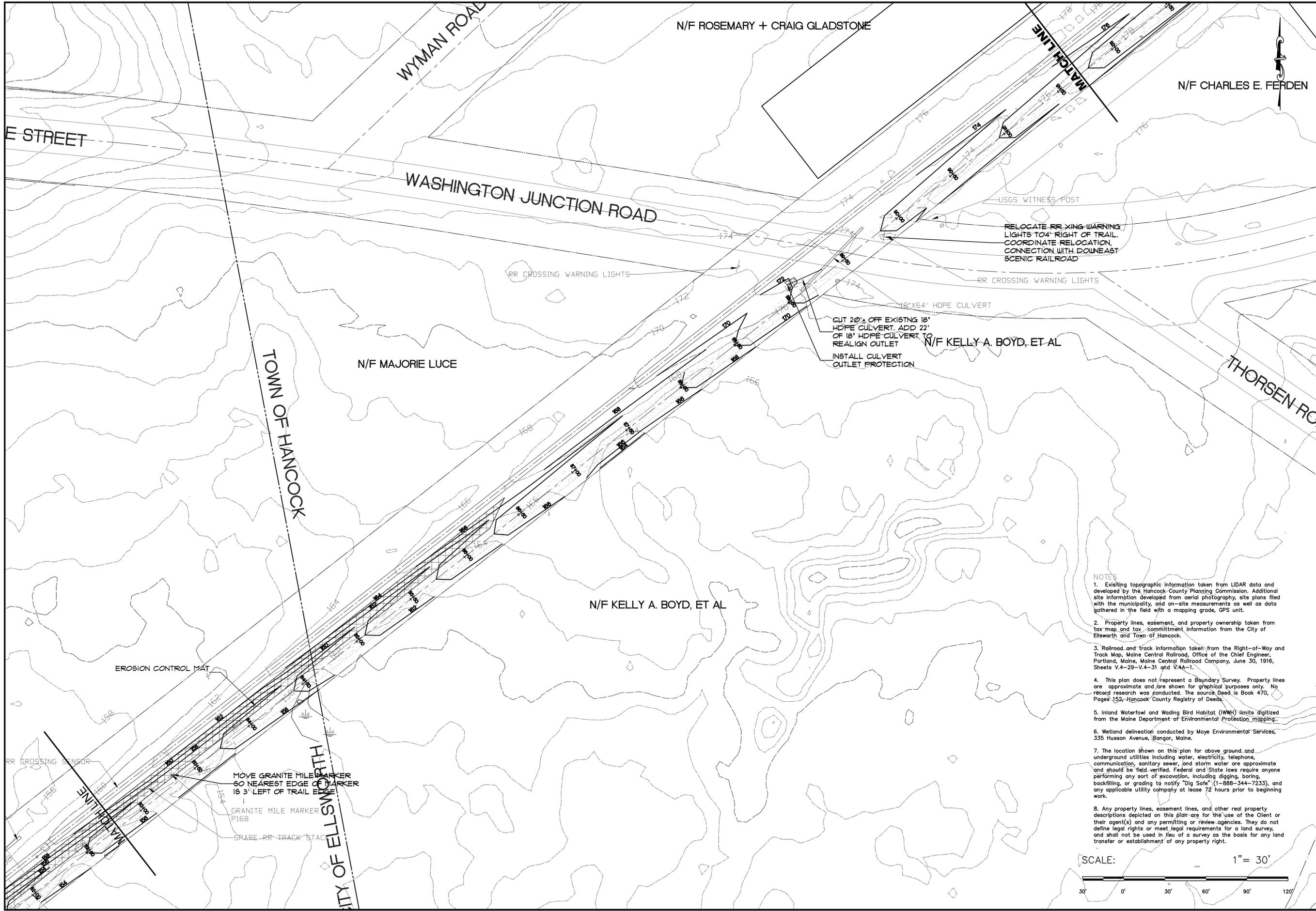
SHEET 9 OF 34
C2H

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SCALE: 1" = 30'

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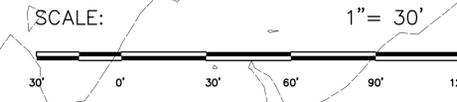
PROJECT NAME: Downeast/Sunrise Trail Extension
 Client: Calais Branch-Maine Central Railroad
 Station 88+00 - 92+00
 PROJECT NO. 14204
 DRAWING NO. 14204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

DESIGNED: FSM
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 CHECKED: SEB
 APPROVED: KGE
 PLAN DATE: March 1, 2016
 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333

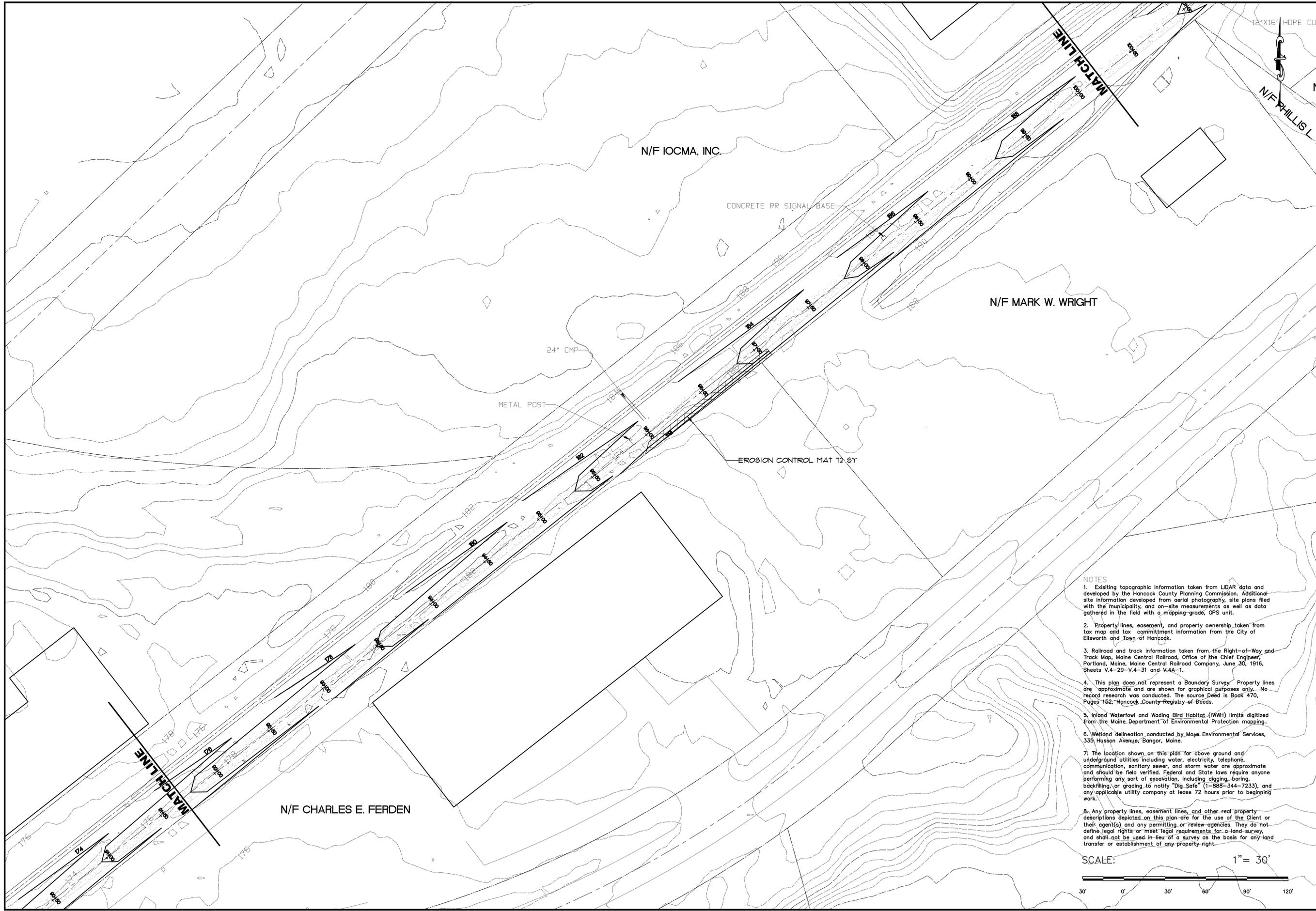
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SHEET 12 OF 34
C2K

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NO.	DATE	DESCRIPTION	APPD.

PROJECT NAME:
Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 Station 92+00 - 100+50

PROJECT NO. 14204
 DRAWING NO. 14204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
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REVISIONS

NO.	DATE	DESCRIPTION	APPD.

SHEET 13 OF 34
C2L

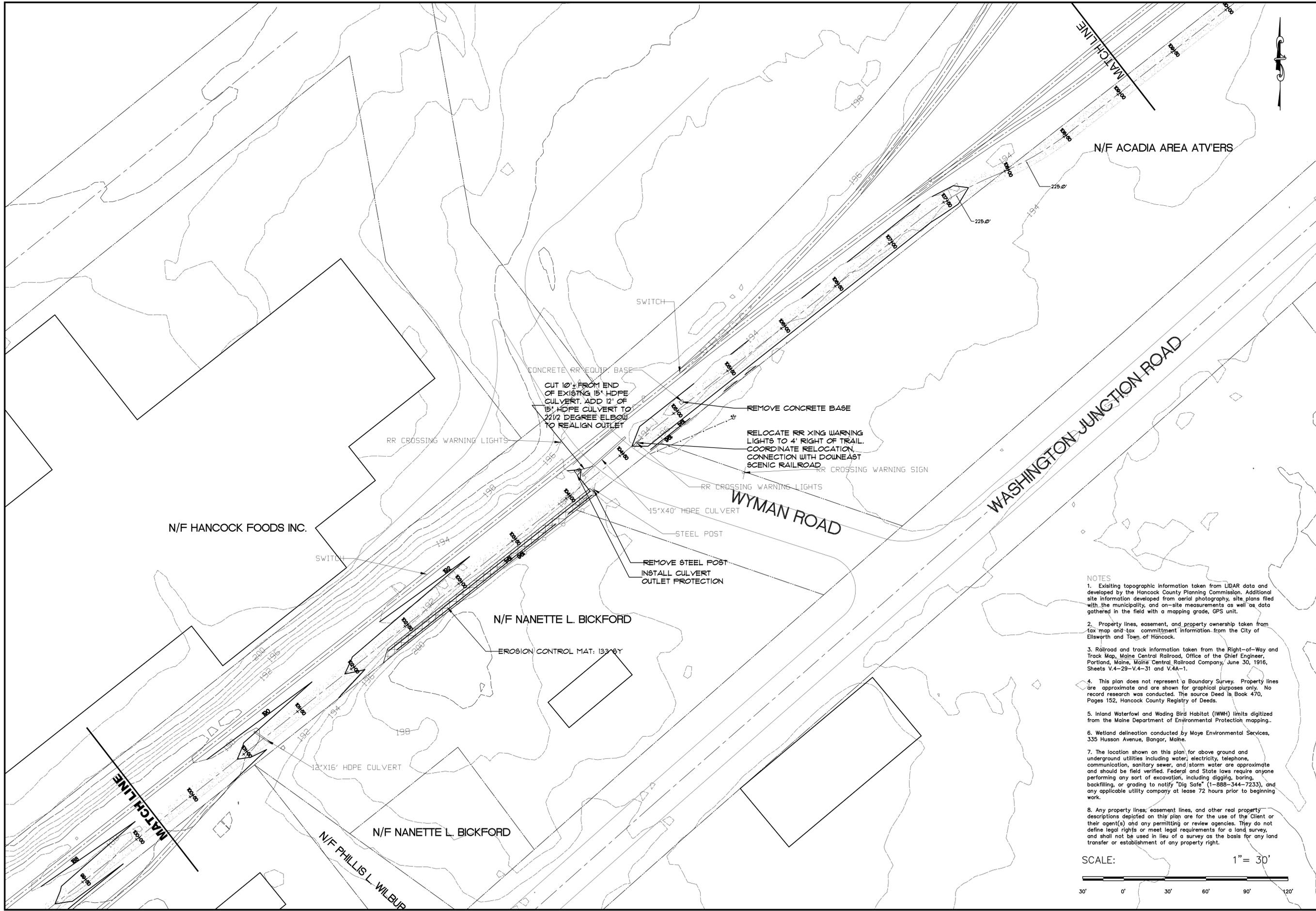
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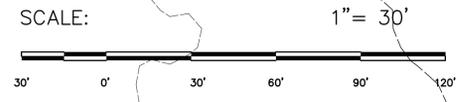
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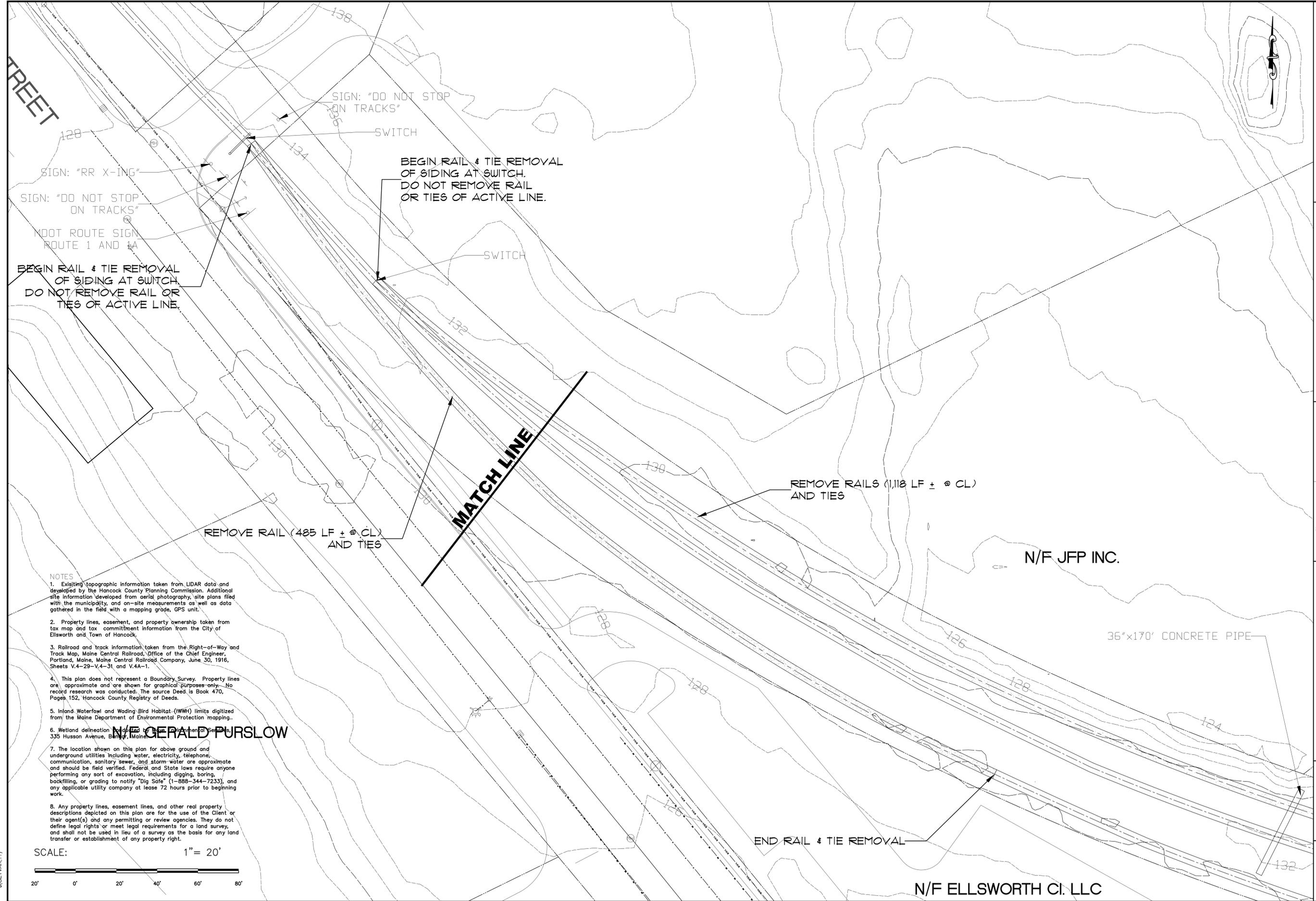
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SHEET 14 OF 34

C2M



SIGN: "RR X-ING"

SIGN: "DO NOT STOP ON TRACKS"

MDOT ROUTE SIGN ROUTE 1 AND 1A

BEGIN RAIL & TIE REMOVAL OF SIDING AT SWITCH. DO NOT REMOVE RAIL OR TIES OF ACTIVE LINE.

SIGN: "DO NOT STOP ON TRACKS"

SWITCH

BEGIN RAIL & TIE REMOVAL OF SIDING AT SWITCH. DO NOT REMOVE RAIL OR TIES OF ACTIVE LINE.

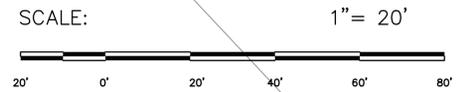
SWITCH

REMOVE RAIL (485 LF ± @ CL) AND TIES

REMOVE RAILS (1,118 LF ± @ CL) AND TIES

END RAIL & TIE REMOVAL

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 Removals: Station 0+00 - 9+00

PROJECT NO. 14204
 DRAWING NO. H204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

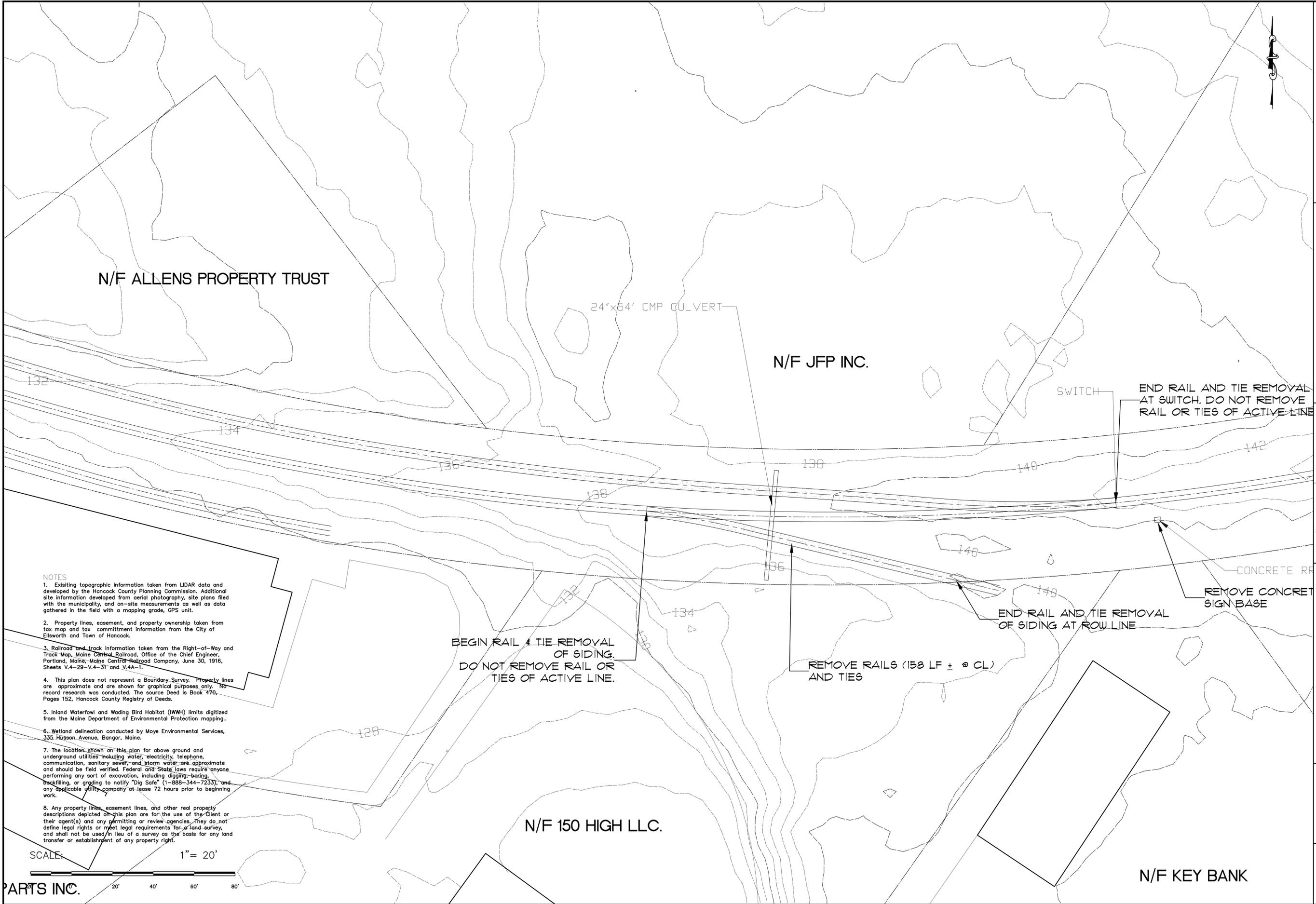
DESIGNED: FSM
 DRAWN: FSM
 CHECKED: SEB
 APPROVED: KGE

CLIENT: N/F JFP INC.
 N/F ELLSWORTH CI. LLC
 Department of Agriculture, Conservation and Forestry
 10 Elkins Lane
 Augusta, Maine 04333

Plymouth Engineering, Inc.
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 Ellsworth, Maine 04869
 Tel: (207) 287-2071 Fax: (207) 287-2190
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SHEET 16 OF 34

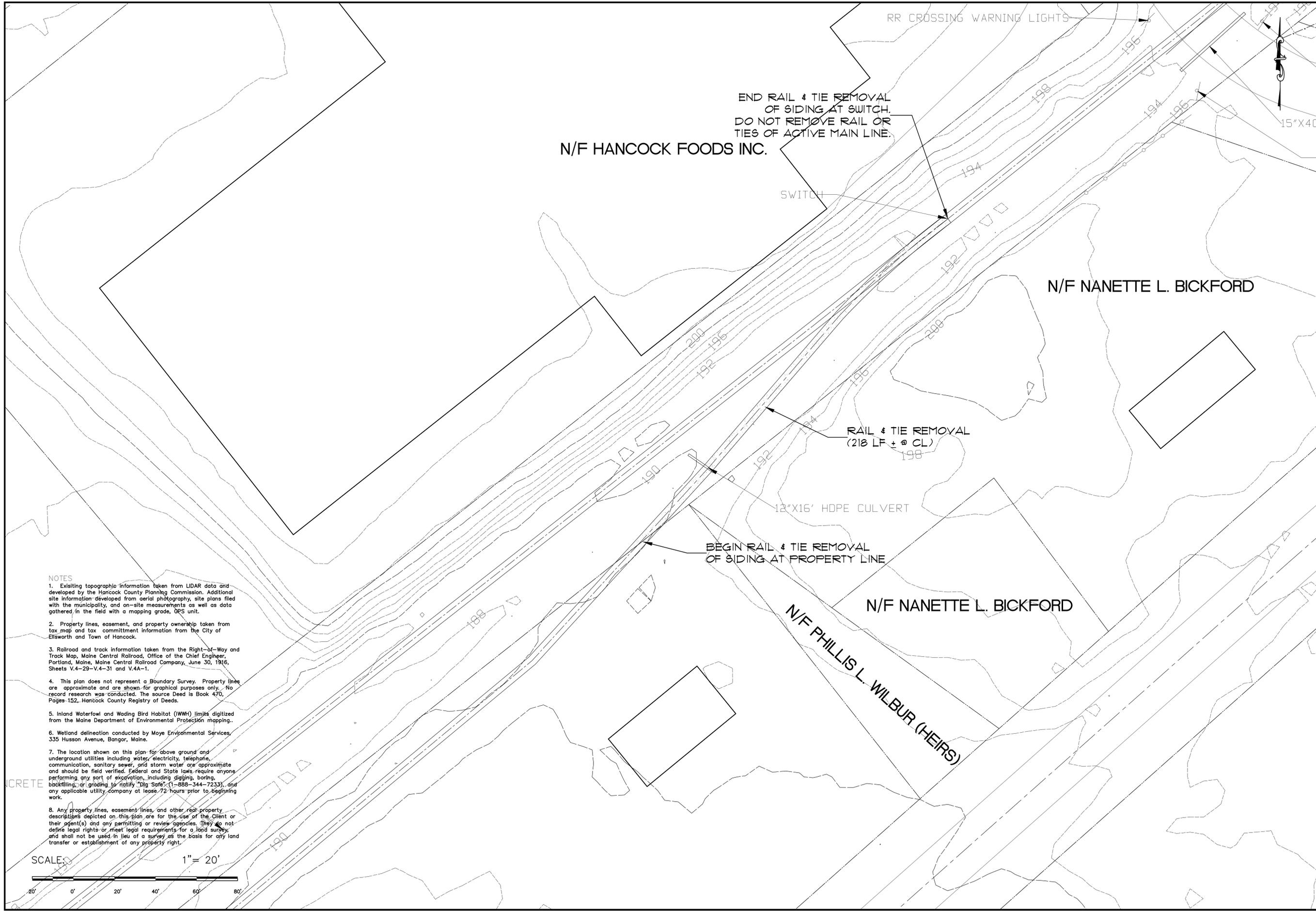
R1



- NOTES**
- Existing topographic information taken from LIDAR data and developed by the Hancock County Planning Commission. Additional site information developed from aerial photography, site plans filed with the municipality, and on-site measurements as well as data gathered in the field with a mapping grade, GPS unit.
 - Property lines, easement, and property ownership taken from tax map and tax commitment information from the City of Ellsworth and Town of Hancock.
 - Railroad and track information taken from the Right-of-Way and Track Map, Maine Central Railroad, Office of the Chief Engineer, Portland, Maine, Maine Central Railroad Company, June 30, 1916, Sheets V.4-29-V.4-31 and V.4A-1.
 - This plan does not represent a Boundary Survey. Property lines are approximate and are shown for graphical purposes only. No record research was conducted. The source Deed is Book 470, Pages 152, Hancock County Registry of Deeds.
 - Inland Waterfowl and Wading Bird Habitat (IWWH) limits digitized from the Maine Department of Environmental Protection mapping.
 - Wetland delineation conducted by Moyer Environmental Services, 335 Husson Avenue, Bangor, Maine.
 - The location shown on this plan for above ground and underground utilities including water, electricity, telephone, communication, sanitary sewer, and storm water are approximate and should be field verified. Federal and State laws require anyone performing any sort of excavation, including digging, boring, backfilling, or grading to notify "Dig Safe" (1-888-344-7233), and any applicable utility company at least 72 hours prior to beginning work.
 - Any property lines, easement lines, and other real property descriptions depicted on this plan are for the use of the Client or their agent(s) and any permitting or review agencies. They do not define legal rights or meet legal requirements for a land survey, and shall not be used in lieu of a survey as the basis for any land transfer or establishment of any property right.

REVISIONS NO. DATE DESCRIPTION _____ _____ _____	
PROJECT NAME: Downeast/Sunrise Trail Extension Calais Branch-Maine Central Railroad	PROJECT NO.: H204 DRAWING NO.: H204Base5.dwg FIELDBOOK: N/A SCALE: AS SHOWN DATE ISSUED: March 1, 2016
DESIGNED: FSM DRAWN: FSM CHECKED: SEB APPROVED: KGE	CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333
SHEET 17 OF 34 R2	

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 www.plymouthengineering.com



N/F HANCOCK FOODS INC.

N/F NANETTE L. BICKFORD

N/F NANETTE L. BICKFORD

N/F PHILLIS L. WILBUR (HEIRS)

END RAIL & TIE REMOVAL
OF SIDING AT SWITCH.
DO NOT REMOVE RAIL OR
TIES OF ACTIVE MAIN LINE.

SWITCH

RAIL & TIE REMOVAL
(218 LF ± @ CL)

BEGIN RAIL & TIE REMOVAL
OF SIDING AT PROPERTY LINE

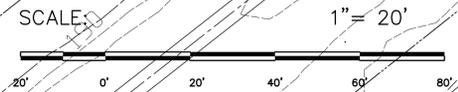
12'X16' HDPE CULVERT

RR CROSSING WARNING LIGHTS

15"X40"

NOTES

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NO.	DATE	DESCRIPTION	REVISIONS

PROJECT NAME: Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 PROJECT NO.: 14204
 DRAWING NO.: H204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

DESIGNED: FSM
 DRAWN: FSM
 CHECKED: SEB
 APPROVED: KGE
 PLAN DATE: March 1, 2016
 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333

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SHEET 18 OF 34
R3

GENERAL NOTES

- 1. CONTRACTOR SHALL NOTIFY THE OWNER AND ENGINEER OF ALL DISCREPANCIES BETWEEN THE DRAWINGS AND FIELD CONDITIONS PRIOR TO AND DURING CONSTRUCTION. ALL FIELD ADJUSTMENTS ARE TO BE APPROVED BY THE OWNER PRIOR TO IMPLEMENTATION.
- 2. PRIOR TO STARTING CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS BY ALL OF THE PERMITTING AUTHORITIES. PARTICULAR ATTENTION SHOULD BE PAID TO CONDITIONS OUTLINED IN THE NATURAL RESOURCES PROTECTION ACT PERMIT DEP #L-26520-IW-A-N/L-26520-TC-B-N.
- 3. THE CONTRACTOR SHALL BE FAMILIAR WITH AND RESPONSIBLE FOR ANY/ALL CERTIFICATIONS, INSPECTIONS, ETC. REQUIRED BY ALL GOVERNING JURISDICTIONAL AGENCIES DURING AND AFTER CONSTRUCTION.
- 4. ALL WORK SHALL BE PERFORMED IN ACCORANCE WITH THESE PLANS, SPECIFICATIONS, AND STANDARDS OF THE LOCAL GOVERNING AUTHORITY.
- 5. THESE PLANS ARE BASED ON INFORMATION PROVIDED TO PLYMOUTH ENGINEERING AT THE TIME OF PLAN PREPARATION. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND NOTIFY PLYMOUTH ENGINEERING IF ACTUAL SITE CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLAN, OR IF THE PROPOSED WORK WOULD BE INHIBITED BY ANY OTHER SITE FEATURES.
- 6. ALL DIMENSIONS SHOWN ON THE PLANS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING IF ANY DISCREPANCIES EXIST PRIOR TO PROCEEDING WITH CONSTRUCTION. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
- 7. CONTRACTOR IS TO EXERCISE EXTREME CARE WHEN PERFORMING ANY WORK ADJACENT TO PAVEMENT, RAILROAD TRACK AND EQUIPMENT, AND STRUCTURES TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING THE APPROPRIATE MEASURES AS NECESSARY TO ENSURE THE STRUCTURAL STABILITY OF ACTIVE RAIL LINE AND PAVEMENT TO REMAIN AND TO PROVIDE A SAFE WORK AREA.
- 8. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING DAMAGE DONE TO ANY EXISTING ITEM DURING CONSTRUCTION SUCH AS BUT NOT LIMITED TO DRAINAGE, UTILITIES, RAILROAD TRACK, RAILROAD SWITCHES, RAILROAD SIGNS, ETC. REPAIR SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITIONS. THE CONTRACTOR IS RESPONSIBLE TO DOCUMENT ALL EXISTING DAMAGE AND NOTIFY THE OWNER PRIOR TO CONSTRUCTION START.
- 9. CONTRACTOR IS RESPONSIBLE FOR ALL SHORING REQUIRED DURING EXCAVATION AND ANY ADDITIONAL PROVISIONS TO ASSURE STABILITY OF CONTIGUOUS STRUCTURES, AS FIELD CONDITIONS DICTATE.
- 10. LAYDOWN AREAS, STOCKPILES, AND STORAGE AREAS WHEN NO LONGER NEEDED FOR THE CONSTRUCTION SHALL BE RETURNED TO THEIR ORIGINAL OR BETTER CONDITION.
- 11. THE ENGINEER IS NOT RESPONSIBLE FOR CONSTRUCTION METHODS/MEANS FOR COMPLETION OF THE WORK DEPICTED ON THESE PLANS NOR ANY CONFLICTS/SCOPE REVISIONS WHICH RESULT FROM SAME. CONTRACTOR IS RESPONSIBLE FOR DETERMINING METHODS/MEANS FOR COMPLETION OF THE WORK PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFICATION OF OWNER AND ENGINEERING WHEN A CONFLICT IS IDENTIFIED.
- 12. NEITHER THE PROFESSIONAL ACTIVITIES OF PLYMOUTH ENGINEERING, INC., NOR THE PRESENCE OF ITS EMPLOYEES AND SUBCONSUTANTS AT A CONSTRUCTION/PROJECT SITE SHALL RELIEVE THE GENERAL CONTRACTOR OF ITS OBLIGATIONS, DUTIES AND RESPONSIBILITIES INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION MEANS, METHODS, SEQUENCE, TECHNIQUES OR PROCEDURES NECESSARY FOR PERFORMING, SUPERINTENDING, AND COORDINATING THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND ANY HEALTH OR SAFETY PRECAUTIONS REQUIRED BY ANY REGULATORY AGENCIES. PLYMOUTH ENGINEERING, INC. AND ITS PERSONNEL HAVE NO AUTHORITY TO EXERCISE ANY CONTROL OVER ANY CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES IN CONNECTION WITH THEIR WORK OR ANY HEALTH OR SAFETY PROGRAMS OR PROCEDURES. THE GENERAL CONTRACTOR SHALL BE SOLEY RESPONSIBLE FOR JOBSITE SAFETY.

GENERAL NOTES

THE FOLLOWING NOTES ARE PROVIDED AS GUIDANCE IN THE INTERPRETATION AND UNDERSTANDING OF THE DRAWING SET. THESE NOTES DO NOT SUPERCEDE THE PROJECT MANUAL OR SPECIFICATIONS.

DEMOLITION NOTES

- 1. PRIOR TO STARTING ANY DEMOLITION, THE CONTRACTOR IS RESPONSIBLE FOR/TO:
 - A. ENSURING COPIES OF ALL PERMITS AND APPROVALS ARE ACQUIRED AND COPIES ARE MAINTAINED ON SITE AND AVAILABLE FOR REVIEW.
 - B. USE AND COMPLY WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY NOTIFICATION SYSTEM TO LOCATE ALL THE UNDERGROUND UTILITIES. ALL UTILITIES INCLUDE BUT ARE NOT LIMITED TO GAS, WATER, ELECTIC, SANITARY SEWER, STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC.
 - C. PROTECT AND MAINTAIN IN OPERATION ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED DURING ALL DEMOLITION ACTIVITIES. THIS SPECIFICALLY INCLUDES OPERATION OF THE ACTIVE RAIL LINE.
 - D. FAMILIARIZING THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY DEMOLITION REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY REQUIREMENTS.
- 2. REMOVE ONLY THE ITEMS NOTED ON THESE DRAWINGS OR AS DIRECTED BY THE OWNER OR OWNER'S REPRESENTATIVE.
- 3. STOCKPILE MATERIAL ONLY IN LOCATIONS APPROVED BY THE OWNER.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR JOB SITE SAFETY AND SUPERVISION. CONTRACTOR IS TO PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL OSHA REQUIREMENTS, TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
- 5. EXPLOSIVES SHALL NOT BE USED WITHOUT WRITTEN CONSENT OF THE OWNER OR OWNER'S REPRESENTATIVE AND APPLICABLE REGULATORY ENTITIES. ALL THE REQUIRED PERMITS AND EXPLOSIVE CONTROL MEASURES THAT ARE REQUIRED BY THE FEDERAL, STATE, AND LOCAL GOVERNMENTS SHALL BE IN PLACE PRIOR TO STARTING AN EXPLOSIVE PROGRAM. THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL INSPECTION AND SEISMIC VIBRATION TESTING THAT IS REQUIRED TO MONITOR THE EFFECTS ON ALL LOCAL STRUCTURES.
- 6. CONDUCT DEMOLITION ACTIVIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, RAILROAD AND OTHER ADJACENT FACILITIES.
- 7. DEMOLITION ACTIVITES AND EQUIPMENT SHALL NOT USE AREAS OUTSIDE THE RIGHT-OF-WAY WITHOUT WRITTEN PERMISSION OF THE OWNER.
- 8. THE CONTRACTOR IS RESPONSIBLE TO SAFEGUARD THE SITE AS NECESSARY TO PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE ENTRY OF UNAUTHORIZED PERSONS AT ANY TIME.
- 9. DEMOLITION AND CONSTRUCTION DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL DEMOLITION WASTES AND DEBRIS (SOLID WASTE) SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL MUNICIPAL, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.

SIGNAGE:

- 1. INSTALLATION OF TEMPORARY WARNING AND GUIDANCE SIGNAGE IS THE RESPONSIBILITY OF THE CONTRACTOR.
- 2. INSTALLATION OF ALL PERMANENT REGULATORY, WARNING AND GUIDANCE SIGNAGE WILL BE PERFORMED BY THE OWNER OR THE OWNER'S REPRESENTATIVE.
- 3. DESIGN, SELECTION, AND LOCATION OF ALL PERMANENT REGULATORY, WARNING, AND GUIDANCE SIGNAGE WILL BE PERFORMED BY THE BUREAU OF PARKS AND LANDS, DEPARTMENT OF AGRICULTURE, CONSERVATION AND FORESTRY, STATE OF MAINE. DESIGN SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES-2009 EDITION AS AMENDED AND OFF ROAD RULES AND REGULATIONS OF THE DEPARTMENT OF CONSERVATION.

SITE PREPARATION NOTES:

- 1. SITE PREPARATION AS SHOWN, IS TO BE PERFORMED PRIOR TO COMMENCEMENT OF REMOVALS.
 - 2. CONTRACTOR SHALL NOTIFY THE DEPARTMENT OF ALL DISCREPANCIES BETWEEN THE DRAWINGS AND FIELD CONDITIONS PRIOR TO AND DURING WORK. ALL FIELD ADJUSTMENTS ARE TO BE APPROVED BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.
 - 3. ALL DIMENSION LINES ARE PERPENDICULAR OR PARALLEL TO THE LINE FROM WHICH THEY ARE MEASURED WITH THE EXCEPTION OR RADII OR UNLESS OTHERWISE NOTED.
- SEDIMENT AND EROSION CONTROL
- 1. EROSION AND SEDIMENTATION CONTROL PRACTICES SHOWN ON THESE PLANS ARE FOR GUIDANCE ONLY.
 - 2. ALL ACTIVITIES SHALL CONFORM TO THE MAINE DEPARTMENT OF TRANSPORTATION'S "BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL", LATEST EDITION.
 - 3. THE CONTRACTOR SHALL PREPARE A SOIL EROSION AND WATER POLLUTION CONTROL PLAN (SEWPCP) IN ACCORDANCE WITH SECTION656, MAINE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS" REVISION OF NOVEMBER, 2014, AS AMENDED.
- ADDITIONAL REMOVAL/DEMOLITION NOTES
- 1. REMOVE ONLY THE AREAS AND ITEMS NOTED ON THIS DRAWING OR AS DIRECTED BY THE ENGINEER OR ITS DESIGNATED REPRESENTATIVE. REPORT ANY DISCREPANCIES TO THE ENGINEER.
 - 2. STOCKPILE MATERIAL ONLY IN LOCATIONS WITHIN THE RIGHT-OF-WAY OF THE RAIL CORRIDOR.
 - 3. ALL RAIL, TIES, AND OTHER ITEMS NOTED ON THE REMOVALS PLANS ARE TO BE REMOVED FROM THE PROPERTY. TEMPORARY STORAGE OF SALVAGE MATERIAL MAY BE GRANTED BY THE OWNER..
 - 4. EXISTING RAILROAD WARNING SIGNS, SIGNALS, ETC. WITHIN THE RAILROAD RIGHT-OF-WAY ARE TO REMAIN AND PROTECTED FROM DAMAGE.
 - 5. MILEAGE SIGNS AND POST SHALL REMAIN IN PLACE UNLESS NOTED FOR RELOCATION..
 - 6. ALL MONUMENTS WITHIN THE RAILROAD RIGHT-OF-WAY SHALL BE PROTECTED FROM DAMAGE AND/OR REMOVAL.
 - 7. ALL RAILROAD TRACK, TIES, SWITCHES, SIGNAGE, WARNING SIGNALS, ETC. WITHIN THE RIGHT-OF-WAY NOT SHOWN FOR REMOVAL SHALL BE PROTECTED FROM DAMAGE.
 - 8. ALL DEMOLITION MATERIALS ARE TO BE REMOVED FROM THE SITE, UNLESS NOTED OTHERWISE, AND DISPOSED OF IN A PROPER MANNER. THE CONTRACTOR SHALL NOTIFY, IN WRITING PRIOR TO DISPOSAL, THE DEPARTMENT OF THE DISPOSAL SITE.
 - 9. NO REMOVAL OR DEMOLITION ACTIVITY MAY TAKE PLACE OUTSIDE THE LIMITS OF THE RAILROAD RIGHT-OF-WAY WITHOUT THE EXPRESSED APPROVAL OF THE DEPARTMENT.
 - 10. CONTRACTOR SHALL CONTACT DIG SAFE, IN ACCORDANCE WITH DIG SAFE'S PROTOCOL, AT 1-800-225-4977 PRIOR TO COMMENCING WORK. THE CONTRACTOR MAY CHOOSE TO DIG SAFE SECTIONS OF THE CORRIDOR IN ORDER TO ALLOW DIG SAFE SUFFICIENT TIME TO REVIEW EACH SECTION.
 - 11. THE CONTRACTOR SHALL CONTACT OTHER APPROPRIATE UTILITIES THAT MAY HAVE SERVICES WITHIN THE RAILROAD CORRIDOR.
 - 12. THE CONTRACTOR SHALL COORDINATE ANY ACTIVITY OUTSIDE THE LIMITS OF CONSTRUCTION BUT WITHIN PUBLIC RIGHTS OF WAY WITH THE MAINE DEPARTMENT OF TRANSPORTATION OR THE MUNICIPAL PUBLIC WORKS DEPARTMENT AND TAKE ALL NECESSARY PRECAUTIONS FOR TEMPORARY TRAFFIC CONTROL IN ACCORDANCE WITH "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" 2009 EDITION BY THE FEDERAL HIGHWAY ADMINISTRATION.

NO.	DATE	DESCRIPTION	REVISIONS

PROJECT NAME: Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 PROJECT NO.: 14204
 DRAWING NO.: 14204Bae5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane Augusta, Maine 04333

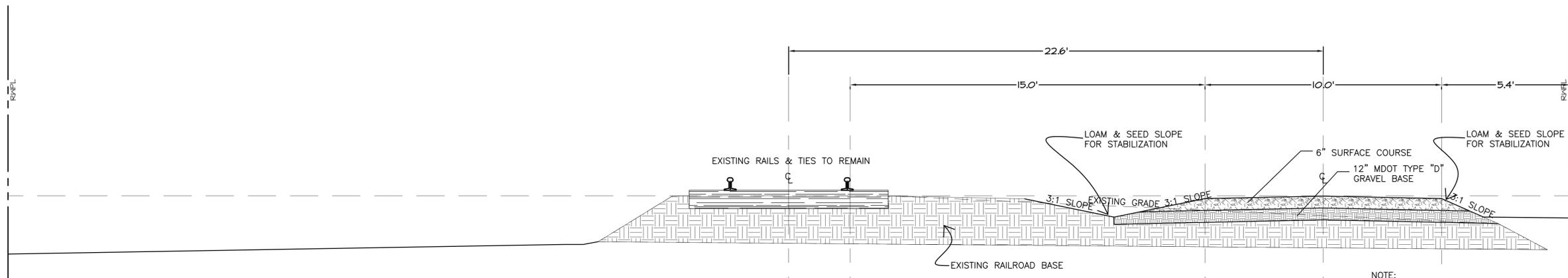
DESIGNED: FSM	FSM
DRAWN: FSM	FSM
CHECKED: SEB	SEB
APPROVED: KGE	KGE
PLAN DATE: March 1, 2016	DATE ISSUED: March 1, 2016

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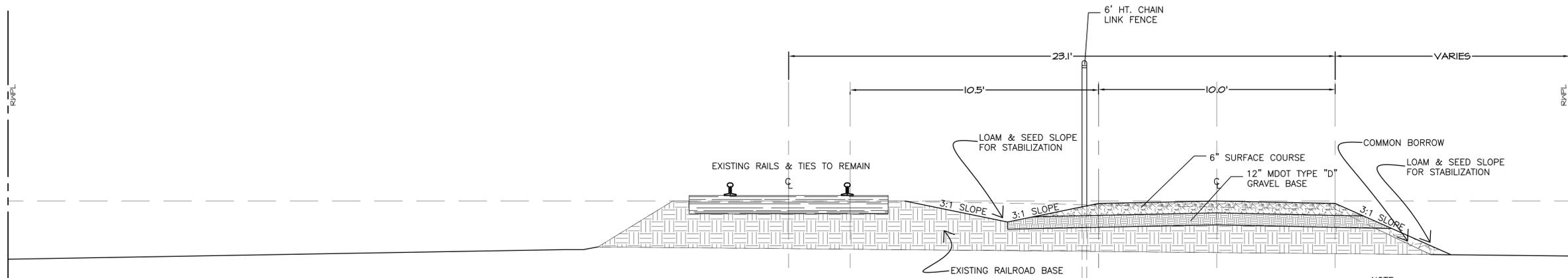


CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane Augusta, Maine 04333

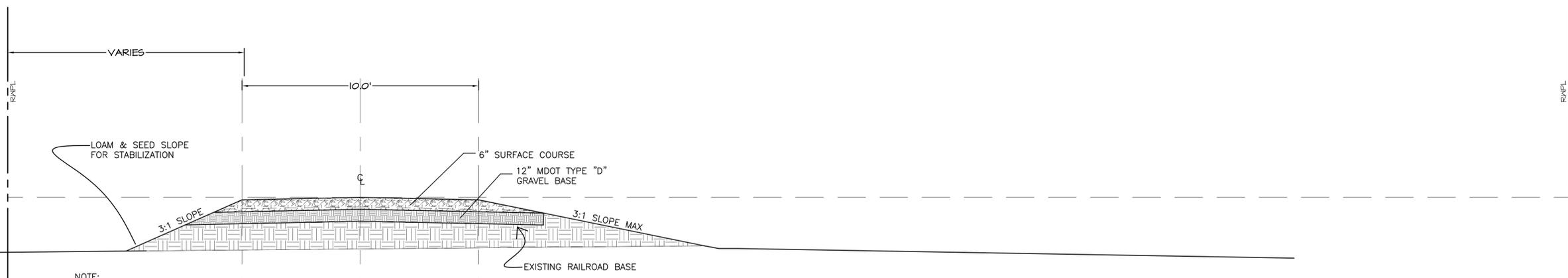




TRAIL/RAIL SECTION DETAIL
STATION 11+46-13+05



TRAIL/RAIL SECTION DETAIL
STATION 11+05-11+46



TRAIL/RAIL SECTION DETAIL
STATION 10+35-11+05

REV.	NO.	DATE	DESCRIPTION

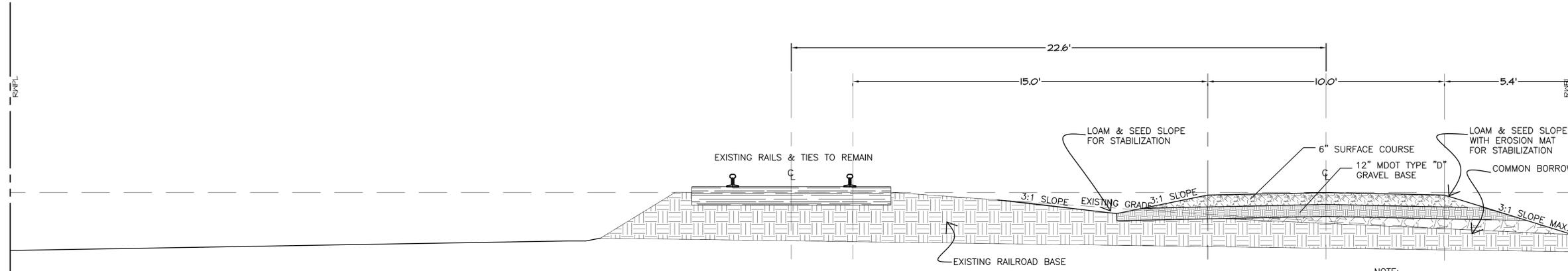
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Calais Branch-Maine Central Railroad
PROJECT NO. 14204
DRAWING NO. H204Base&Zing
FIELDBOOK: N/A
SCALE: AS SHOWN
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CLIENT: Maine Department of Agriculture, Conservation and Forestry
10 Elkins Lane Augusta, Maine 04333

DESIGNED: FSN
DRAWN: KGE
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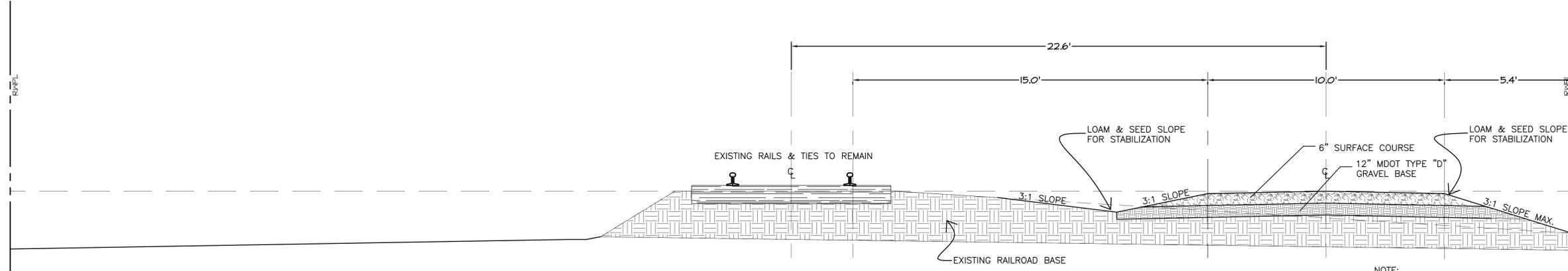
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SHEET 21 OF 34

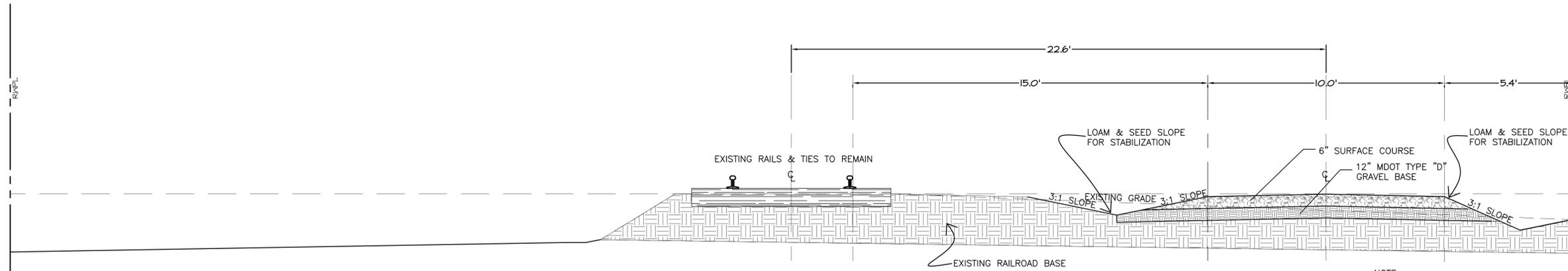
C5



TRAIL/RAIL SECTION DETAIL
STATION 17+30-22+75



TRAIL/RAIL SECTION DETAIL
STATION 14+75-16+68



TRAIL/RAIL SECTION DETAIL
STATION 14+16-14+75

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.

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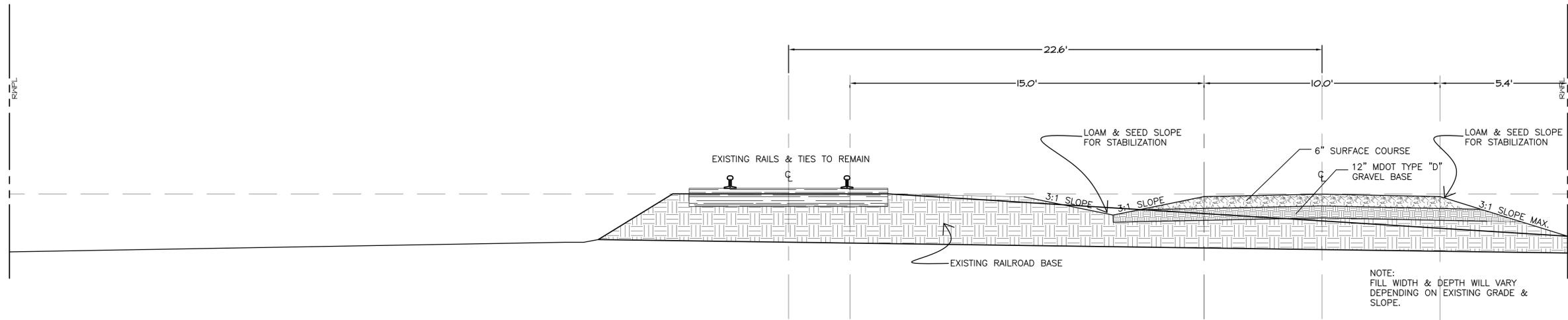
NOTE:
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NO.	DATE	DESCRIPTION	REVISIONS

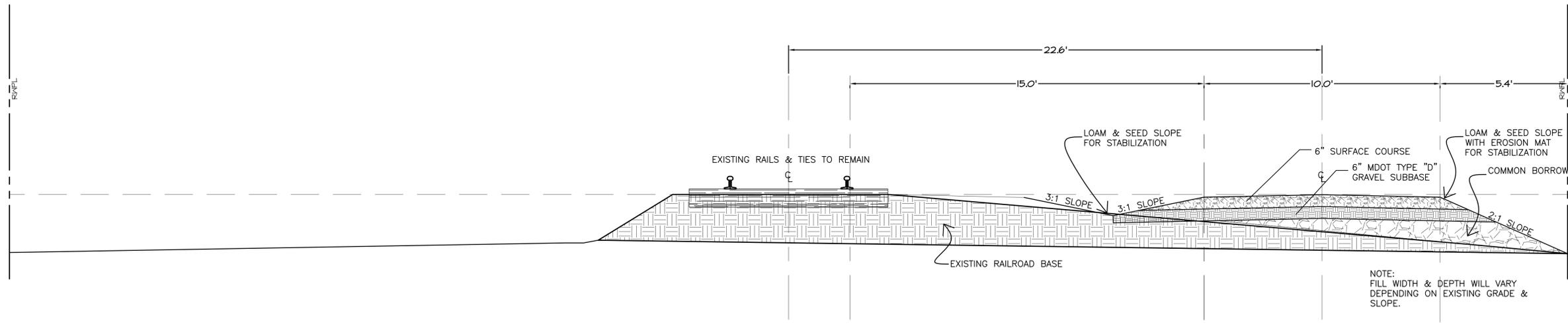
PROJECT NAME: **Downeast/Sunrise Trail Extension**
Calais Branch-Maine Central Railroad
PROJECT NO. 14204
DRAWING NO. H204Base5.dwg
FIELDBOOK: N/A
SCALE: AS SHOWN
DATE ISSUED: March 1, 2016
SHEET NAME: **TRAIL SECTIONS-STATION 4+16-22+75**

DESIGNED: FSN
DRAWN: KGE
CHECKED: KGE
APPROVED: AS SHOWN
CLIENT: Maine Department of Agriculture, Conservation and Forestry
10 Elkins Lane
Augusta, Maine 04333

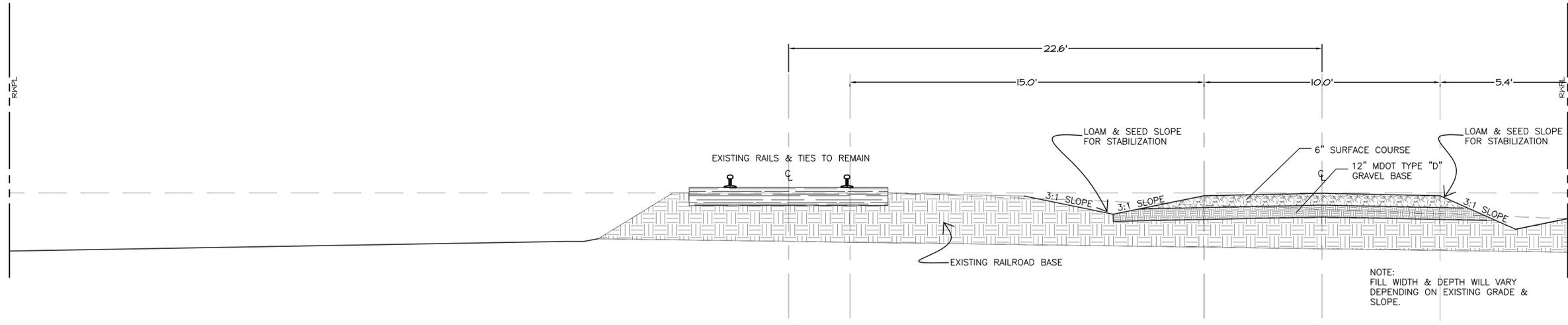
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TRAIL/RAIL SECTION DETAIL
STATION 30+75-31+48



TRAIL/RAIL SECTION DETAIL
STATION 30+26-30+75



TRAIL/RAIL SECTION DETAIL
STATION 22+75-30+26

NO.	DATE	DESCRIPTION	BY	APPD.

PROJECT NAME:
Downeast/Sunrise Trail Extension
Calais Branch-Maine Central Railroad

PROJECT NO. H204

DRAWING NO. H204Base&Zoning

FIELDBOOK: N/A

SCALE: AS SHOWN

DATE ISSUED: March 1, 2016

CLIENT: Maine
Elsworth-Horcock

SHEET NAME:
TRAIL SECTIONS-STATION 22+75-31+48

DESIGNED: FSN

DRAWN: KGE

CHECKED: KGE

APPROVED: KGE

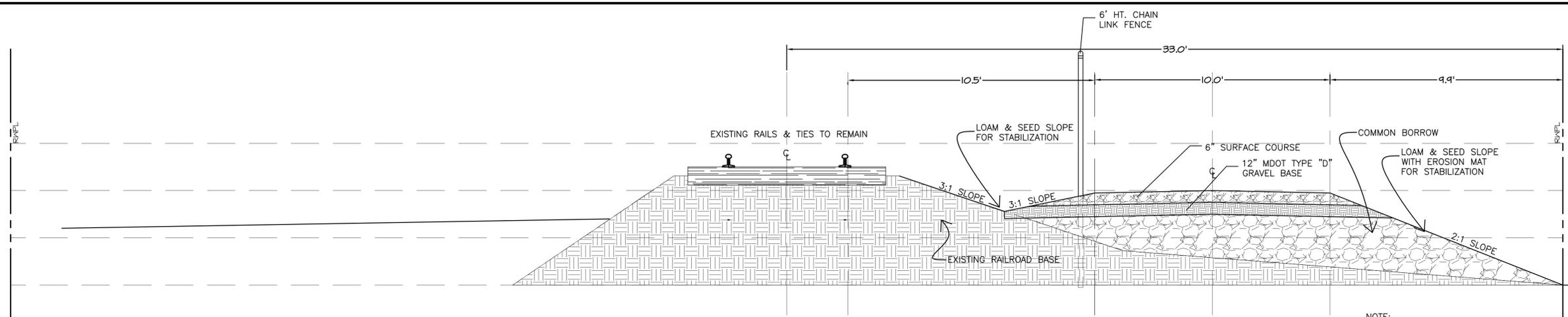
PLAN DATE: March 1, 2016

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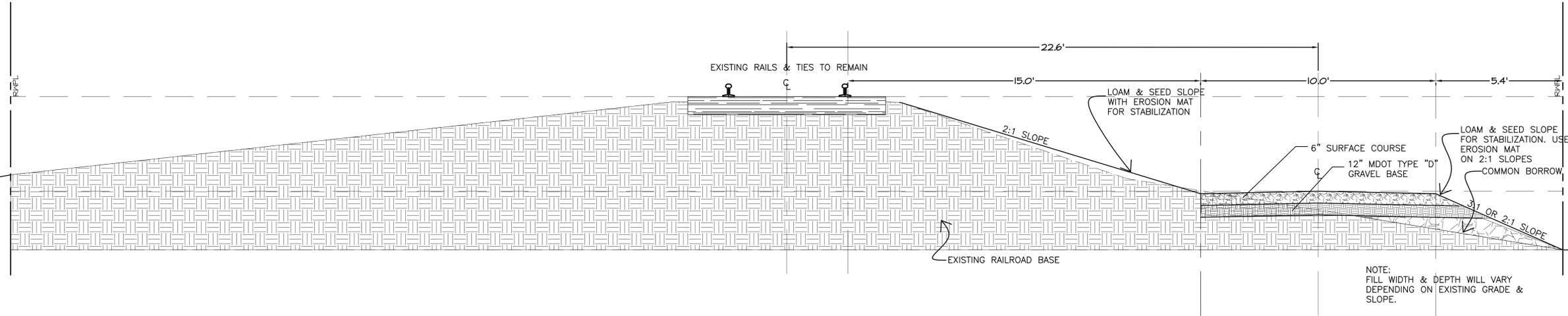
CLIENT: Maine
Elsworth-Horcock

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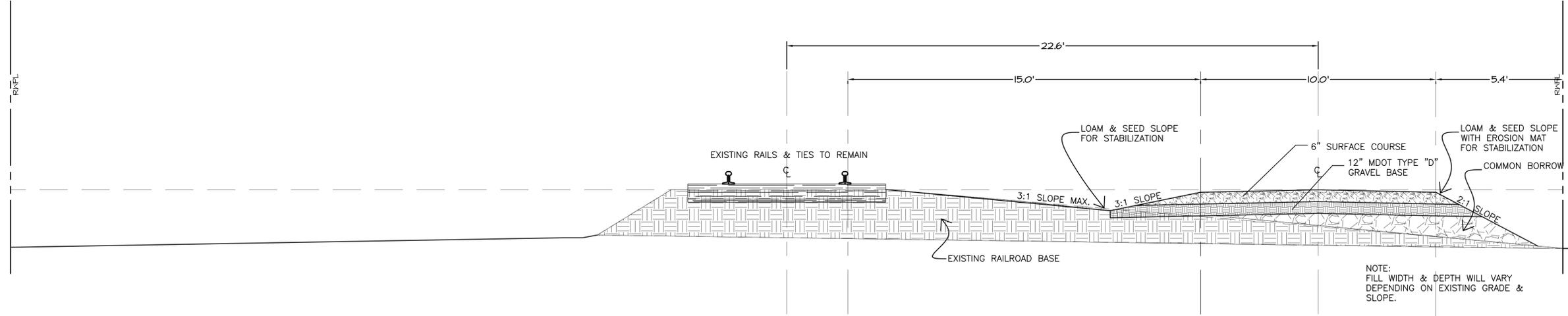
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TRAIL/RAIL SECTION DETAIL
STATION 35+18-38+43



TRAIL/RAIL SECTION DETAIL
STATION 32+24-35+18



TRAIL/RAIL SECTION DETAIL
STATION 31+48-32+24

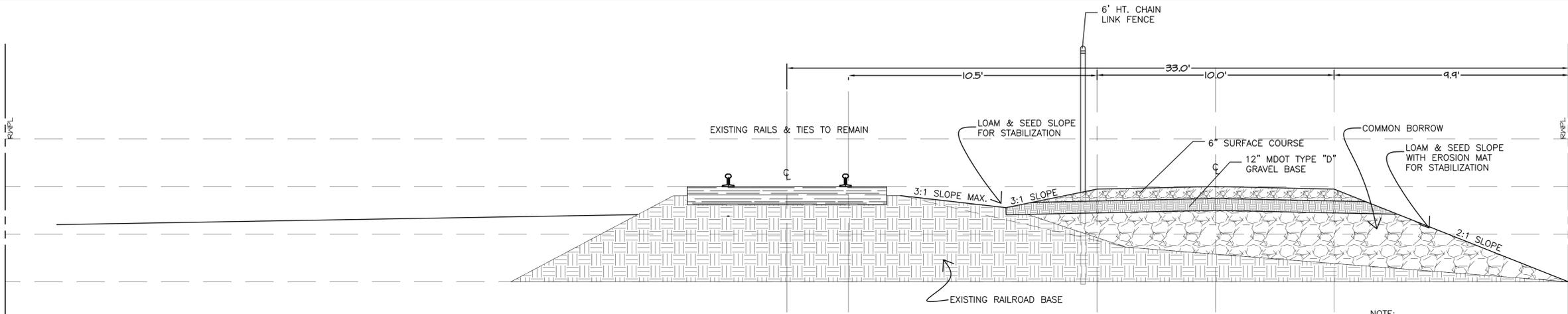
NO.	DATE	DESCRIPTION	BY	APPD.

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Catais Branch-Maine Central Railroad
 PROJECT NO: 14204
 DRAWING NO: 14204Base&Zing
 FIELDBOOK: N/A
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 DATE ISSUED: March 1, 2016
 CLIENT: **Maine Department of Agriculture, Conservation and Forestry**
 10 Elkins Lane
 Augusta, Maine 04333

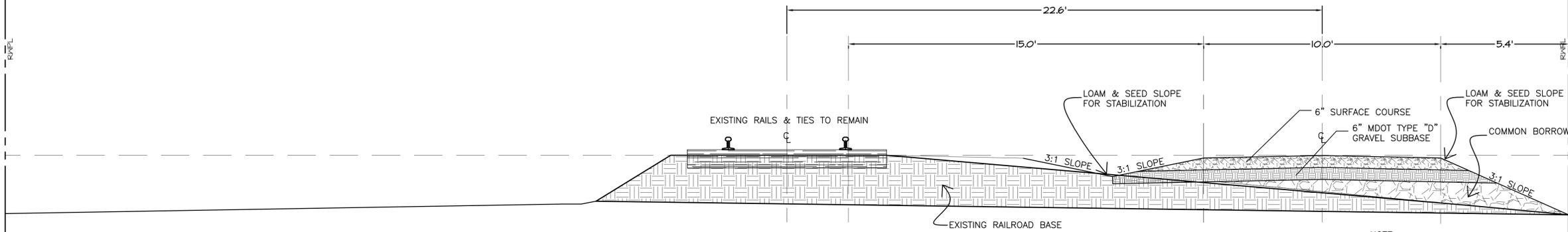
DESIGNED: FSH
 DRAWN: KGE
 CHECKED: KGE
 APPROVED: KGE
 PLAN DATE: March 1, 2016
 CLIENT: **Maine Department of Agriculture, Conservation and Forestry**
 10 Elkins Lane
 Augusta, Maine 04333

Plymouth Engineering, Inc.
 P.O. Box 48 30 Lower Detroit Road
 Plymouth, Maine 04969
 Tel: (207) 287-2071 Fax: (207) 287-2130
 info@plymouthengineering.com
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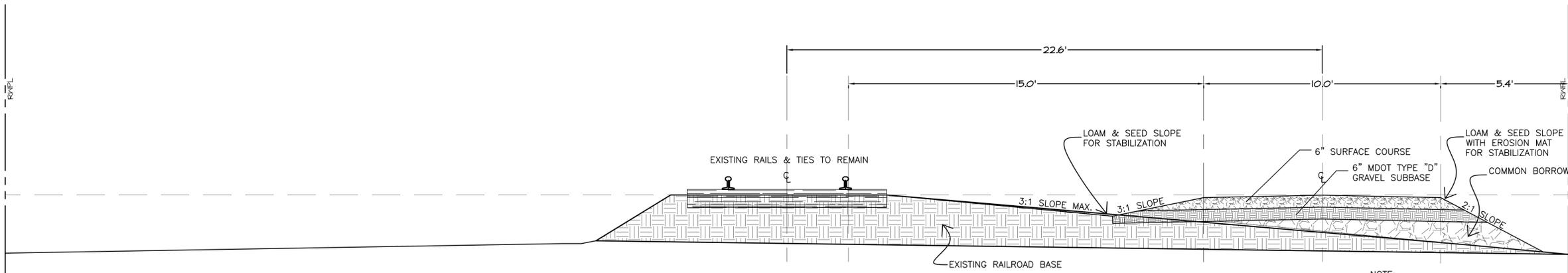
NOTE: FILL WIDTH & DEPTH WILL VARY DEPENDING ON EXISTING GRADE & SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 41+32-48+00 & 48+40-49+40



TRAIL/RAIL SECTION DETAIL
STATION 39+42-41+32



TRAIL/RAIL SECTION DETAIL
STATION 38+43-39+42

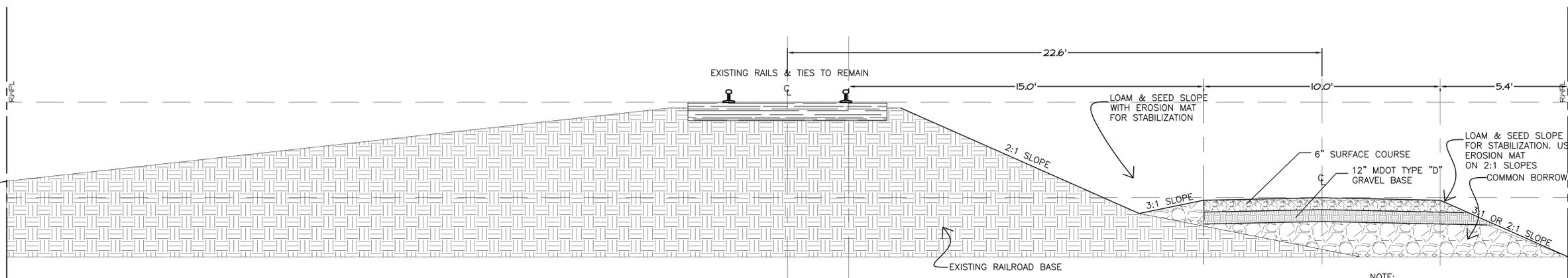
NO.	DATE	DESCRIPTION	BY	APPD.

PROJECT NAME: Downeast/Sunrise Trail Extension
Catais Branch-Maine Central Railroad
PROJECT NO. H204
DRAWING NO. H204Base5.dwg
FIELDBOOK: N/A
SCALE: AS SHOWN
DATE ISSUED: March 1, 2016
CLIENT: Maine Department of Agriculture, Conservation and Forestry
10 Elkins Lane Augusta, Maine 04333

DESIGNED: FSH
DRAWN: KGE
CHECKED: KGE
APPROVED: AS SHOWN
DATE: March 1, 2016
CLIENT: Maine Department of Agriculture, Conservation and Forestry
10 Elkins Lane Augusta, Maine 04333

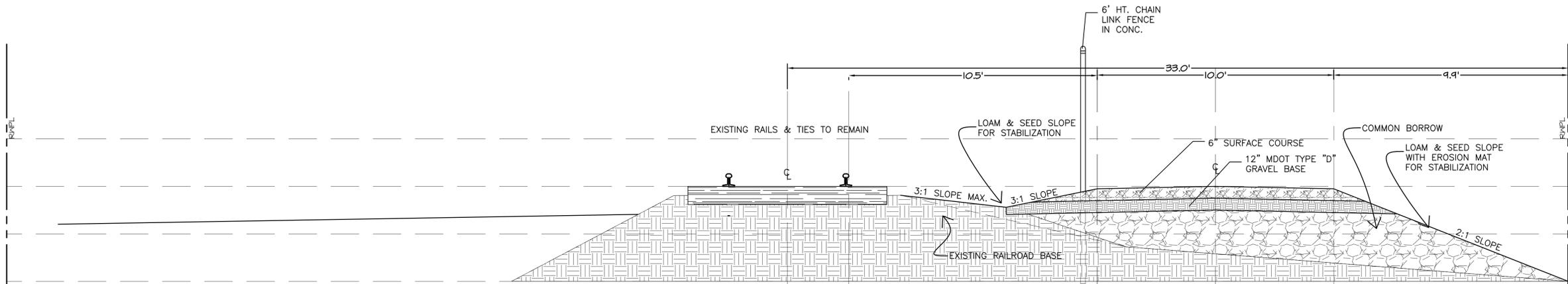
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REVISIONS



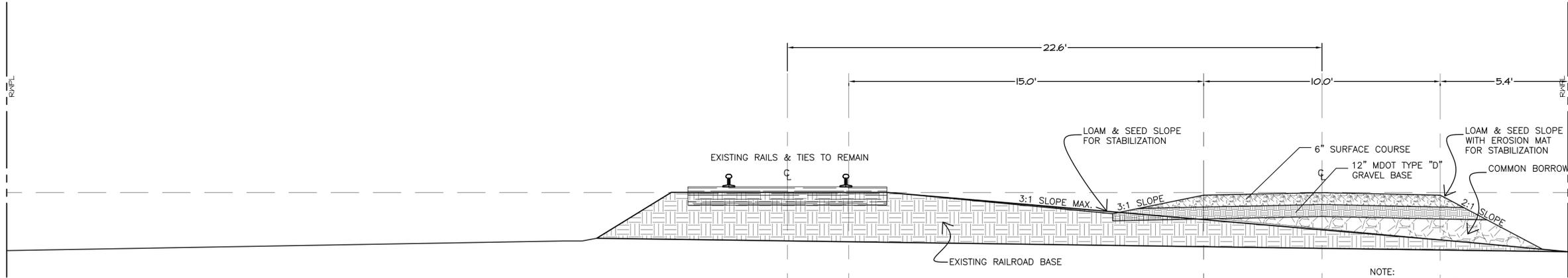
TRAIL/RAIL SECTION DETAIL
STATION 77+39-86+25

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 58+29-77+39

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 49+40-58+29

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.

NO.	DATE	DESCRIPTION

PROJECT NAME:
Downeast/Sunrise Trail Extension
Calais Branch-Maine Central Railroad

PROJECT NO.: H204
DRAWING NO.: H204Base5.dwg
FIELDBOOK: N/A
SCALE: AS SHOWN
DATE ISSUED: March 1, 2016

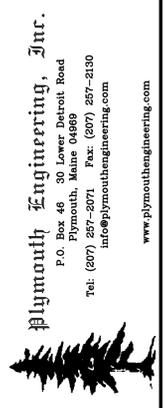
DESIGNED: FSH
DRAWN: KGE
CHECKED: KGE
APPROVED: AS SHOWN

CLIENT:
Maine and Public Lands
Department of Agriculture, Conservation and Forestry
10 Elkins Lane
Augusta, Maine 04333

PROJECT LOCATION:
Elsworth-Hancock
Maine

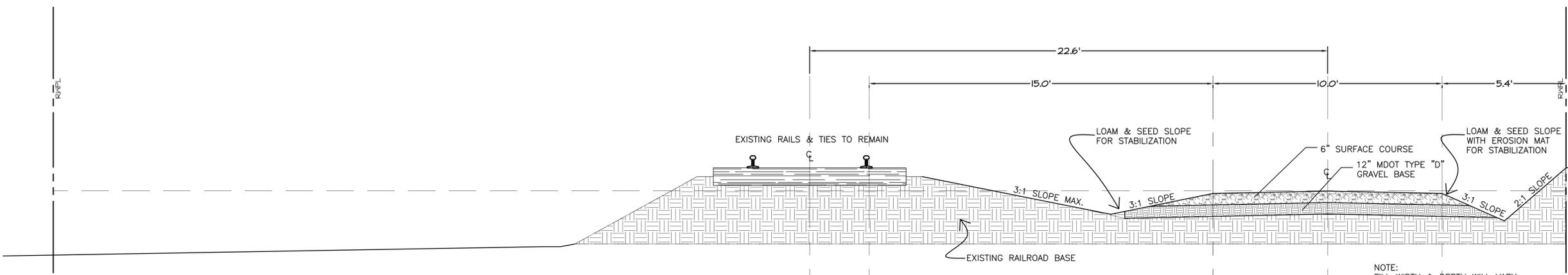
SHEET NAME:
TRAIL SECTIONS-STATION 44+40-86+25

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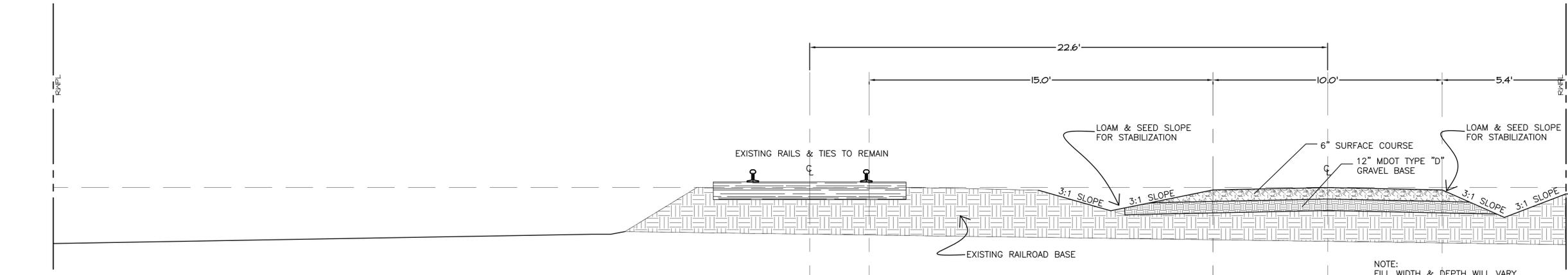
SHEET 27 OF 34

C11



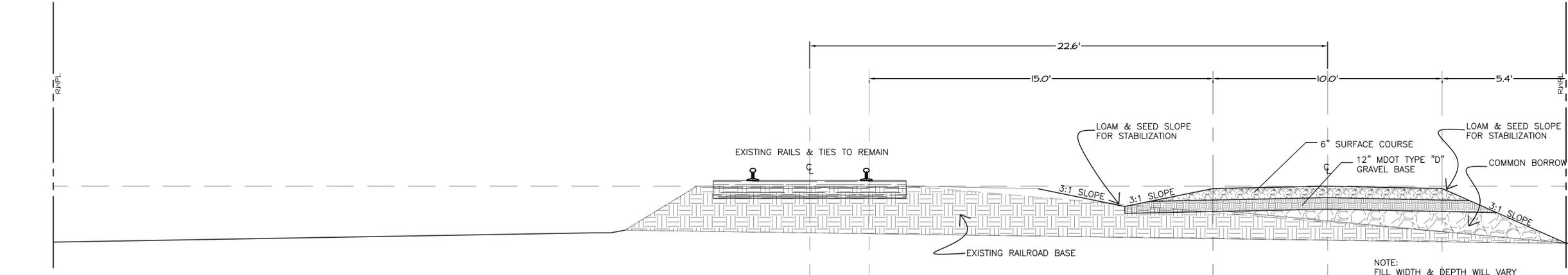
TRAIL/RAIL SECTION DETAIL
STATION 95+75-97+08

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 89+75-95+75

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 86+25-89+43

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.

NO.	DATE	DESCRIPTION	REVISIONS

PROJECT NAME:
Downeast/Sunrise Trail Extension
Calais Branch-Maine Central Railroad

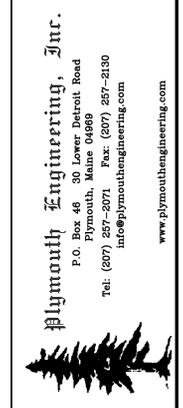
PROJECT NO. 14204
DRAWING NO. H204Base5.dwg
FIELDBOOK: N/A
SCALE: AS SHOWN
DATE ISSUED: March 1, 2016

DESIGNED: FSH
DRAWN: KGE
CHECKED: KGE
APPROVED: [Signature]

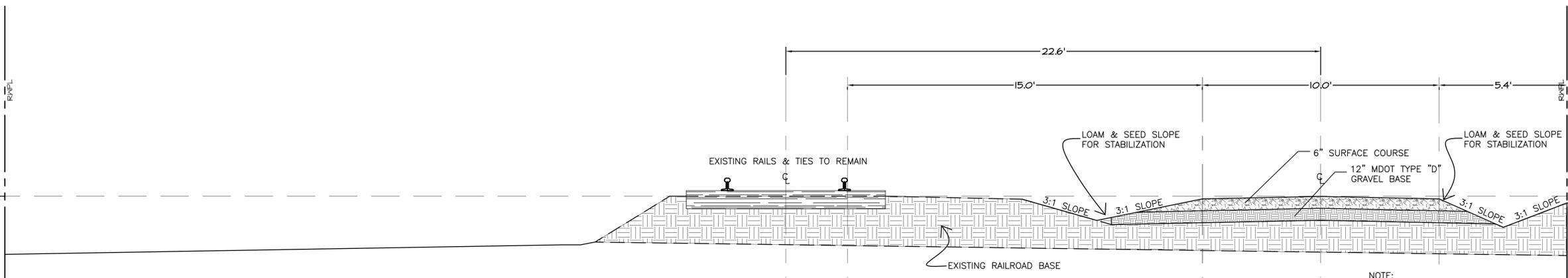
CLIENT:
Maine and Public Lands
Department of Agriculture, Conservation and Forestry
10 Elkins Lane
Augusta, Maine 04333

SHEET NAME:
TRAIL SECTIONS-STATION 86+25-97+08

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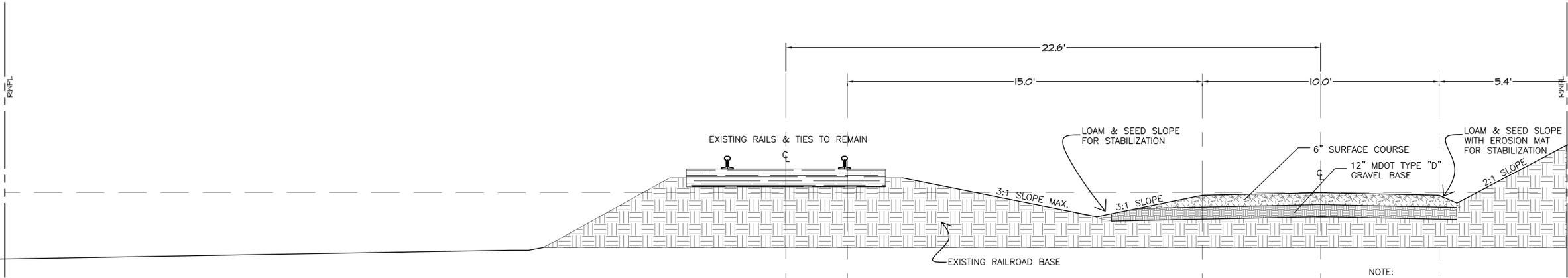


Feb 26, 2016 - 12:37pm
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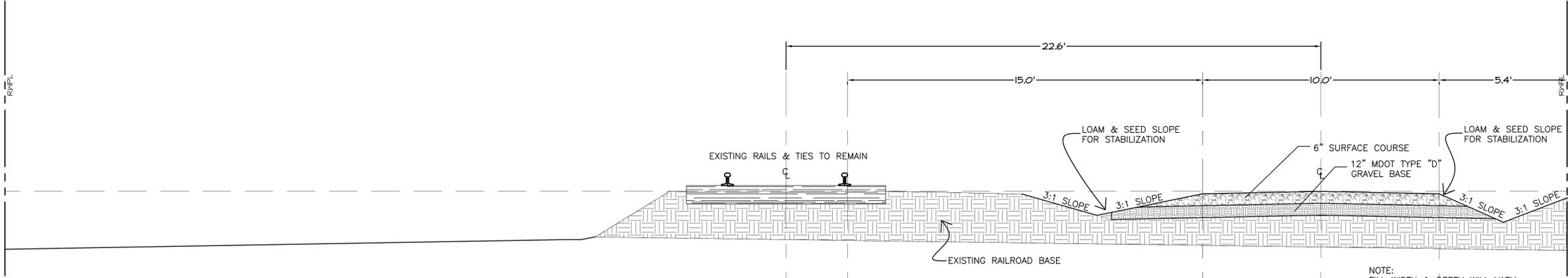
TRAIL/RAIL SECTION DETAIL
STATION 104+33-112+89

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 102+30-104+33

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.



TRAIL/RAIL SECTION DETAIL
STATION 97+08-102+30

NOTE:
FILL WIDTH & DEPTH WILL VARY
DEPENDING ON EXISTING GRADE &
SLOPE.

NO.	DATE	REVISIONS

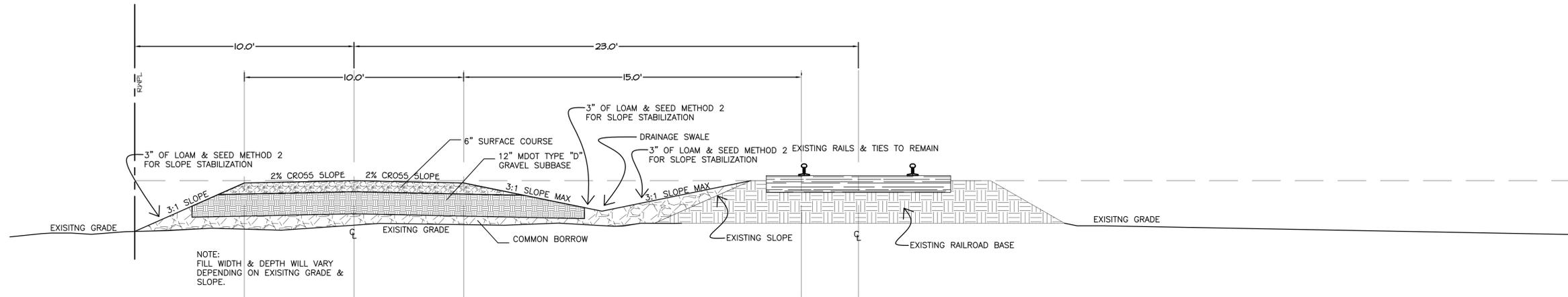
PROJECT NAME: **Downeast/Sunrise Trail Extension**
Catais Branch-Maine Central Railroad
 PROJECT NO. 14204
 DRAWING NO. H204Base&Jdg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane Augusta, Maine 04333

DESIGNED: FSN
 DRAWN: KGE
 CHECKED: KGE
 APPROVED: [Signature]
 PLAN DATE: March 1, 2016
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane Augusta, Maine 04333

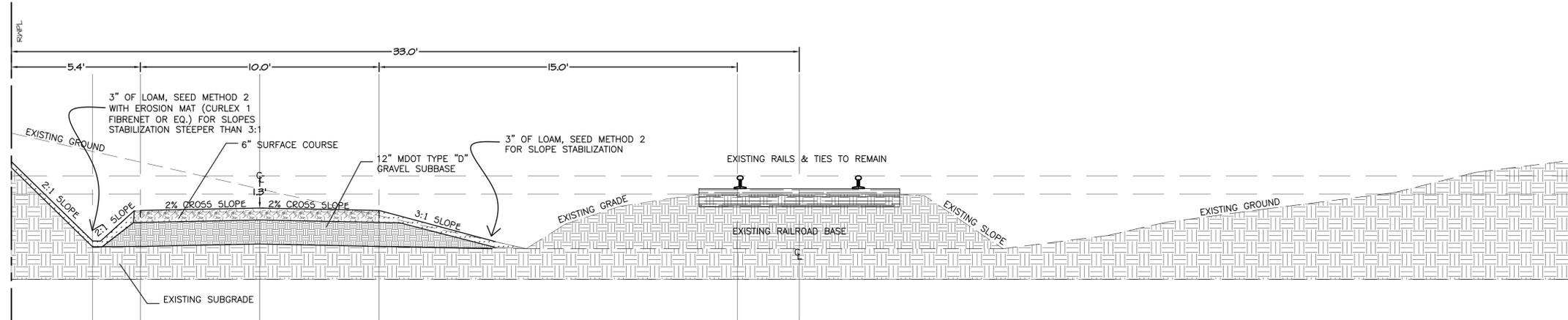
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SHEET 24 OF 34

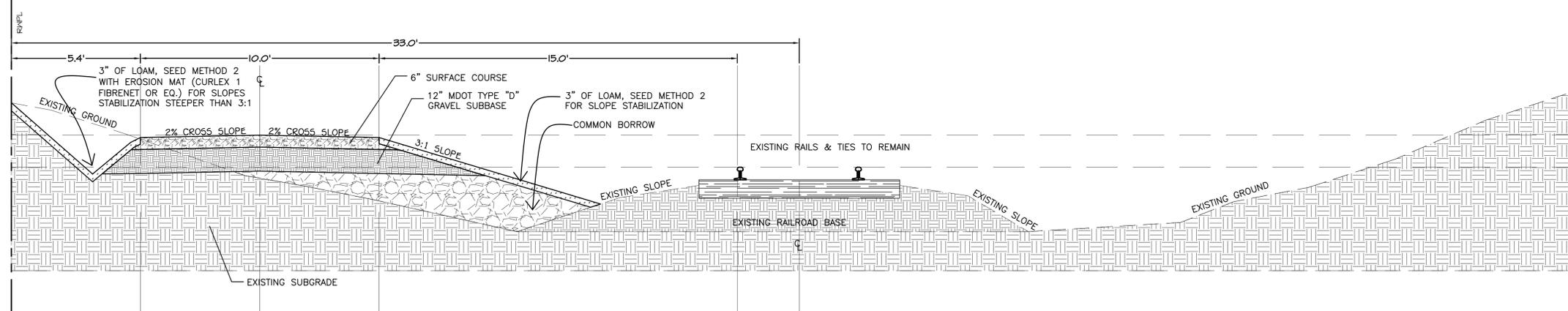
C13



TRAIL/RAIL TYPICAL SECTION WITH FILL-NON-WETLAND



TRAIL/RAIL TYPICAL SECTION IN CUT-NON-WETLAND



TRAIL/RAIL TYPICAL SECTION IN CUT & FILL-NON-WETLAND

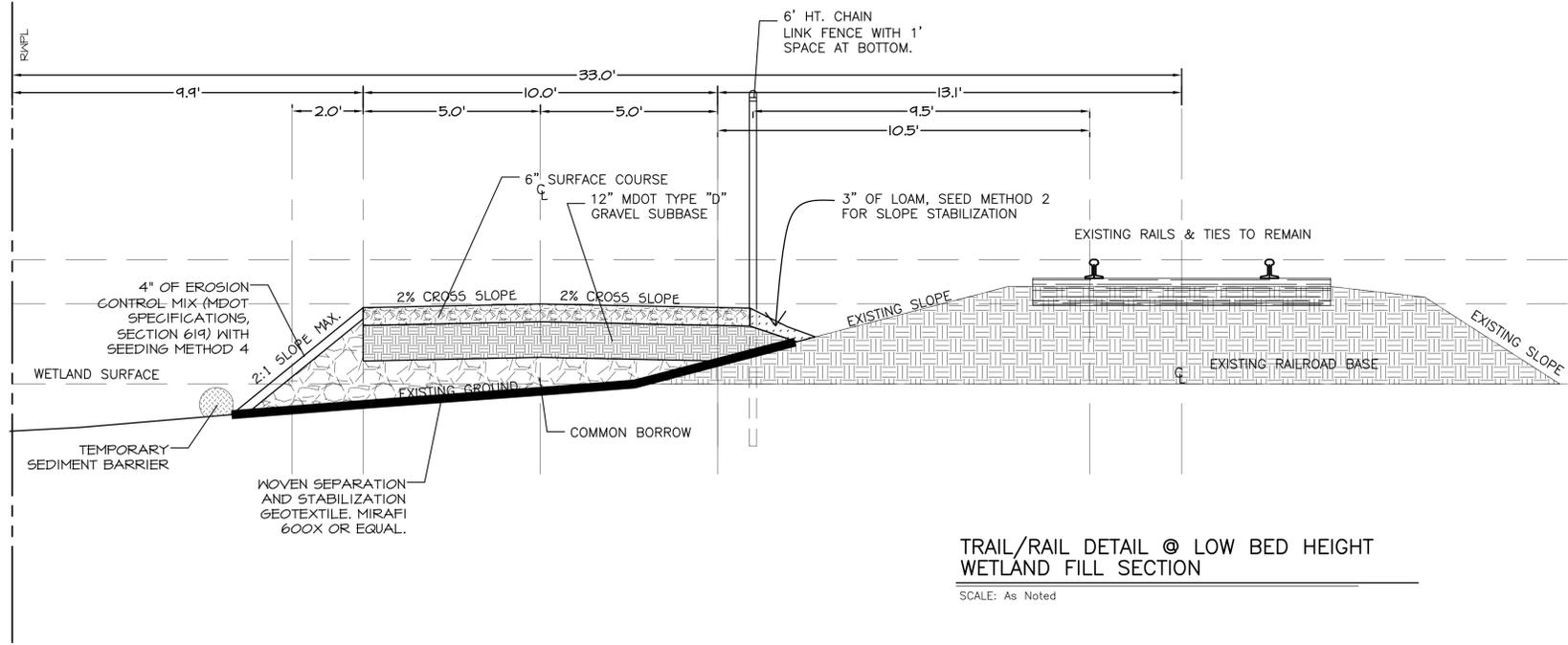
NO.	DATE	DESCRIPTION	REVISIONS

PROJECT NAME: Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 PROJECT NO. 14204
 DRAWING NO. H204Base&Sdg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane Augusta, Maine 04333

DESIGNED: FSN
 DRAWN: KGE
 CHECKED: KGE
 APPROVED: AS SHOWN
 PLAN DATE: March 1, 2016
 DATE ISSUED: March 1, 2016

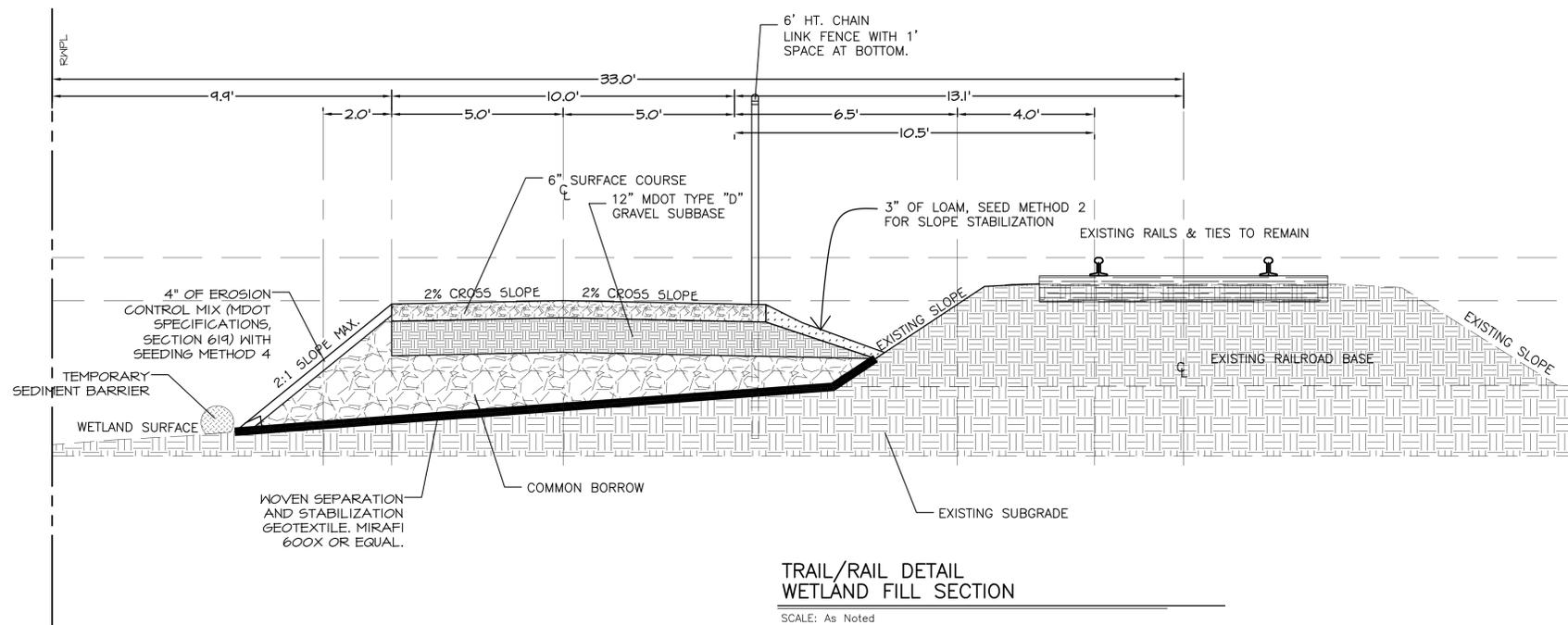
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TRAIL/RAIL DETAIL @ LOW BED HEIGHT
WETLAND FILL SECTION

SCALE: As Noted



TRAIL/RAIL DETAIL
WETLAND FILL SECTION

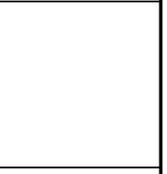
SCALE: As Noted

NO.	DATE	DESCRIPTION	REVISIONS

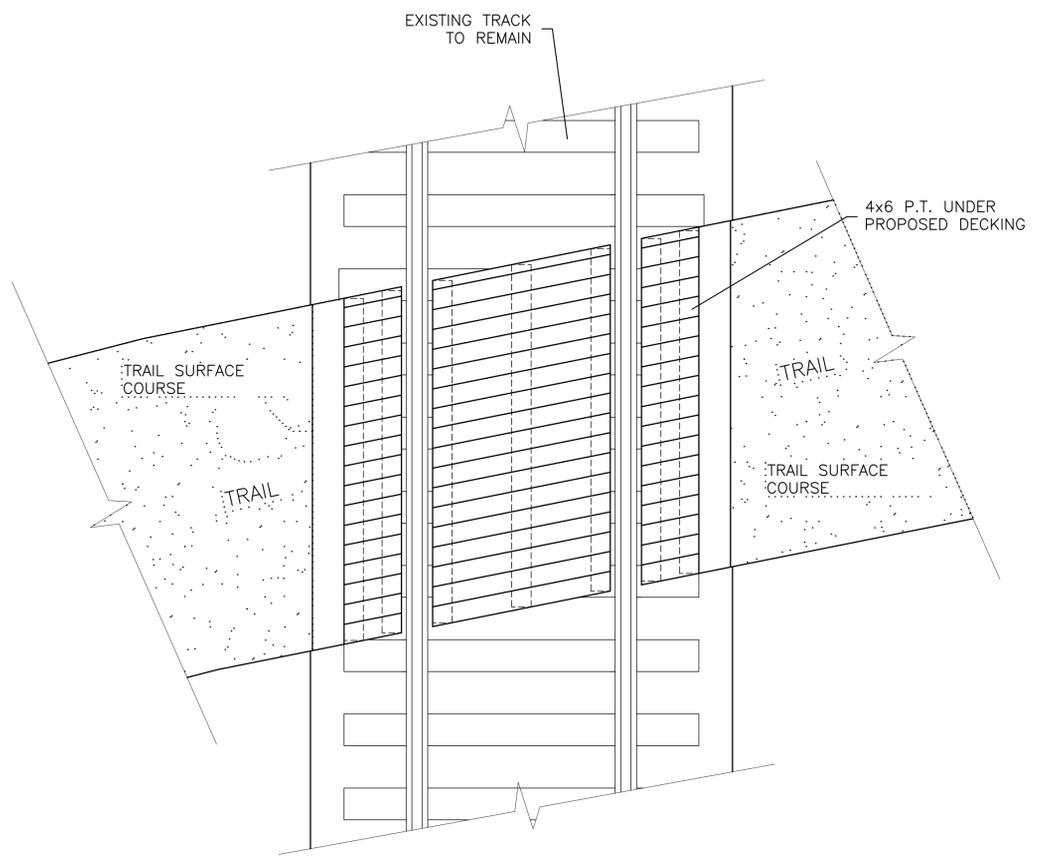
PROJECT NAME: Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 PROJECT NO: 14204
 DRAWING NO: H204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

DESIGNED: FSN
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 APPROVED: [Signature]
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
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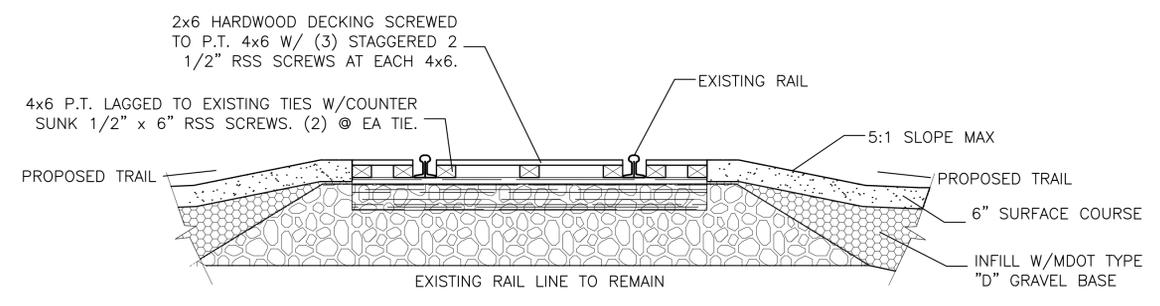


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TRAIL CROSSING OF EXISTING RAILS

SCALE: NTS



TRAIL CROSSING OF EXISTING RAILS SECTION

SCALE: NTS

NO.	DATE	REVISIONS

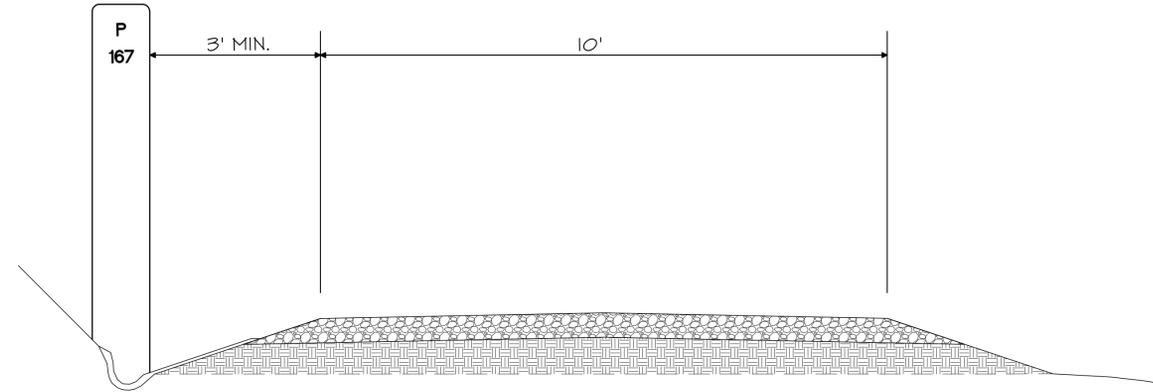
PROJECT NAME:
Downeast/Sunrise Trail Extension
 Calais Branch-Maine Central Railroad
 Ellsworth-Hancock
 Maine

PROJECT NO. 14204
 DRAWING NO. H204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016

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SHEET 33 OF 34

C17

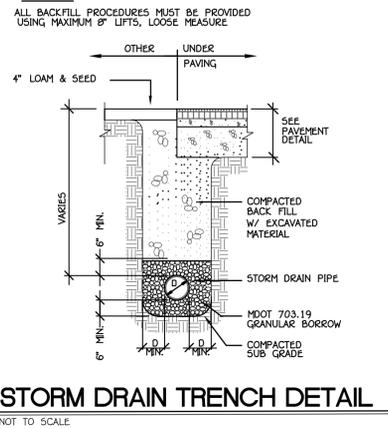


GRANITE MILE MARKER RELOCATION

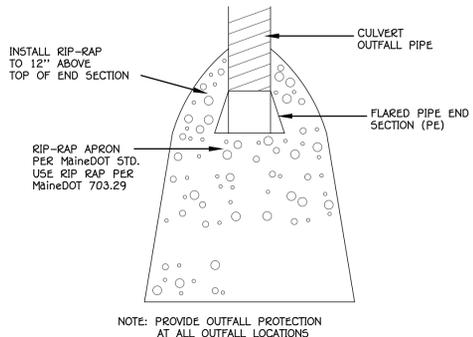
NO SCALE

NOTE: LATERAL SIGN CLEARANCE SHALL BE A MINIMUM OF 3 FEET AND A MAXIMUM OF 6 FEET FROM THE NEAR EDGE OF THE SIGN TO THE NEAR EDGE OF THE TRAIL. RELOCATED SIGN SHOULD BE PLACED SO THE AMOUNT OF EXPOSURE OF THE SIGN POST FACE IS THE SAME IN THE NEW PLACEMENT AS IN THE EXISTING CONDITION.

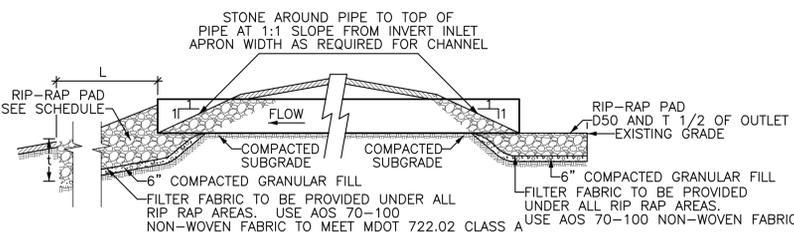
DESIGNED: FSN DRAWN: - CHECKED: KGE APPROVED: - PLAN DATE: March 1, 2016 CLIENT: Parks and Public Lands Department of Agriculture, Conservation and Forestry 10 Elkins Lane Augusta, Maine 04333		PROJECT NO. 14204 DRAWING NO. H204Base5.dwg FIELDBOOK: N/A SCALE: AS SHOWN DATE ISSUED: March 1, 2016		PROJECT NAME: Downeast/Sunrise Trail Extension Calais Branch-Maine Central Railroad Ellsworth-Hancock Maine		SHEET NAME: Granite Mile Marker Relocation	
NO.	DATE	DESCRIPTION	REVISIONS	DRAWN	APPROVED		



STORM DRAIN TRENCH DETAIL
NOT TO SCALE



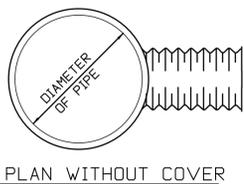
CULVERT OUTLET DETAIL
NOT TO SCALE



RIP RAP SCHEDULE			
DIA.	d50	t	L
15"	8"	21"	6'
18"	10"	24"	8'
24"	12"	27"	10'
36"	12"	27"	15'
48"	13"	30"	20'

- NOTE
1. RIP RAP PROTECTION TO BE ANGULAR STONE
 2. RIP RAP NOT TO EXTEND BEYOND ROW LINE

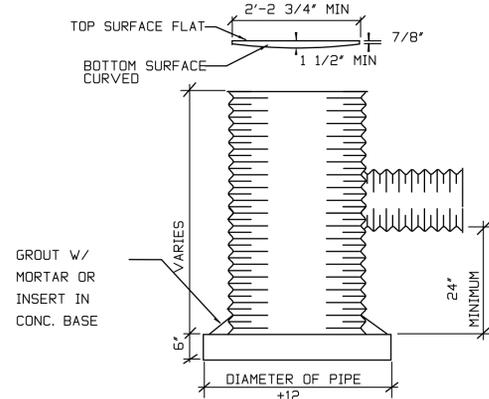
CULVERT INLET/OUTLET DETAIL
NOT TO SCALE



PLAN WITHOUT COVER



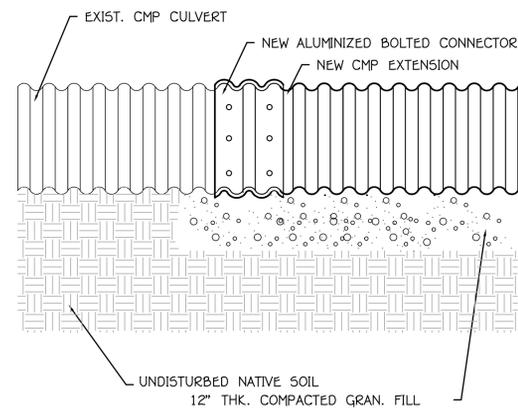
COVER PLAN
(MIN. 150#)



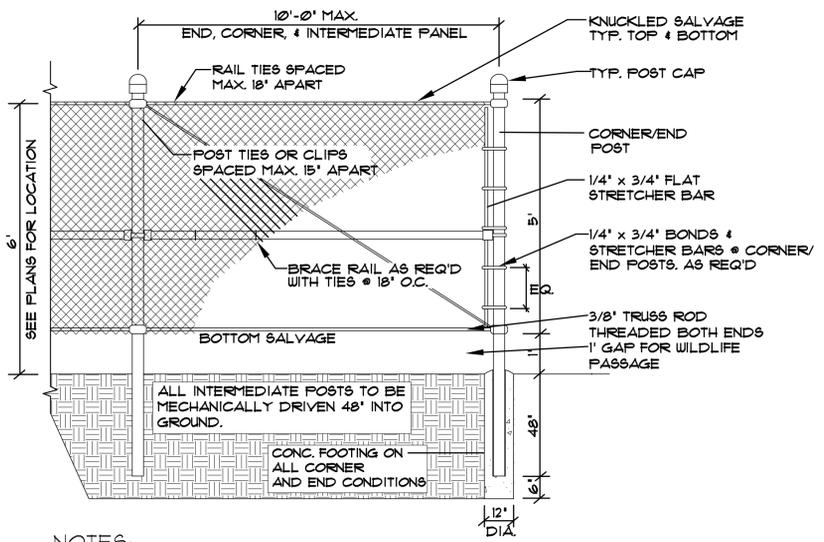
ELEVATION

TYPE "E" CATCH BASIN DETAIL
NO SCALE

1. EXCESS PIPE TO BE CUT AND FOLDED AGAINST THE INSIDE WALL OF CATCH BASIN. JOINTS TO BE CAULKED WITH FLEXIBLE SEALANT. JOINT MAY BE SHOP WELDED
2. STUBS TO BE MADE FROM 2' LENGTH OF PIPE.
3. GRATE TO BE OF GRAY CAST IRON
4. BARS OF GRATE TO BE PLACED PARALLEL WITH FLOW.



CULVERT EXTENSION DETAIL
NOT TO SCALE



- NOTES:
1. CORNER OR END POST: NOMINAL 3" O.D. GALVANIZED STEEL PIPE, MIN. 5.19 lbs./LF.
 2. INTERMEDIATE POST: NOMINAL 2-1/2" O.D. GALVANIZED STEEL PIPE, MIN. 3.65 lbs./LF.
 3. BRACES (TOP & BOTTOM): NOMINAL 1-5/8" O.D. GALVANIZED STEEL PIPE, MIN. 2.21 lbs./LF.
 4. THE OUTSIDE OF THE FENCE FABRIC SHALL BE 3' INSIDE THE EDGE OF PAVEMENT.
 5. BRACE RAIL AND DIAGONAL BRACE ROD SHALL BE INSTALLED AT EACH 10' CORNER SECTION OF ENCLOSURE.
 6. CONCRETE SHALL HAVE MINIMUM COMPRESSIVE STRENGTH (f_c) OF 3000 psi WITH 6% ± AIR ENTRAINMENT.
 7. SUBMIT SHOP DRAWINGS FOR OWNER'S/ENGINEER'S APPROVAL.
 8. FENCE FABRIC, POST, RAILS AND APPURTENANCES SHALL BE VINYL CLAD ± COLOR: BLACK

CHAIN LINK FENCE DETAIL
NOT TO SCALE

NO.	DATE	REVISIONS

PROJECT NAME: **Downeast/Sunrise Trail Extension**
Calais Branch-Maine Central Railroad
 PROJECT NO. 14204
 DRAWING NO. H204Base5.dwg
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: Maine Department of Agriculture, Conservation and Forestry
 10 Elkins Lane
 Augusta, Maine 04333

DESIGNED: FSH
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 CHECKED: KGE
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EROSION & SEDIMENTATION CONTROL NOTES

Introduction
This Erosion and Sedimentation Control Plan has been prepared to aid in the prevention of erosion and subsequent sedimentation of the downstream storm water structures during and after construction of the proposed site. Actual prevention is the responsibility of those involved in site construction. Construction personnel must be observant and prepared to take immediate action to prevent erosion at all times. This plan should be used as a guide, however unforeseen site conditions and storm events may require additional or alternative preventative measures. This plan has been prepared in accordance with the Maine Erosion and Sediment Control BMPs: MOST CURRENT VERSION

Administration
This Plan has been prepared by Plymouth Engineering, Inc. for use by those involved in the development of the site. Temporary measures and permanent measures are the responsibility of the Contractor until final acceptance by the Owner. The owner or their designated representative shall inspect work for compliance with project plans and specifications.

Key Areas of Concern During Construction
During the development of this site the following areas shall be watched on a continual basis for signs of erosion, and will be areas of preventative measures. Efforts must be made to minimize the disturbed area.

Area	Temp. Measures	Perm. Measures
Natural Slopes Disturbed <3:1	Mulch, Grass Cover, Silt Barrier	Grass Cover
Man Made Slopes >3:1	Mulch, Grass Cover, Silt Barrier	Grass Cover
Man Made Swales <3:1	Mulch, Grass Cover, Stone Check Dams	Erosion Control Blanket, Grass Cover
Man Made Swales >3:1	Mulch, Grass Cover, Stone Check Dams	Geotextile Fabric, Rip Rap
Site Improvements	Mulch, Grass Cover, Silt Barrier	Grass Cover
Drainage Structures	Hay Bale Inlet Protection	

Key Areas of Concern After Construction
Within this development the following areas should be of special concern on an ongoing basis.

Area	Temp. Measures	Perm. Measures
Project Slopes <3:1	Mulch, Grass Cover	Grass Cover
Project Slopes >3:1	Mulch, Grass Cover	Erosion Control Blanket, Grass Cover
Drainage Swales	As Required by Plans	

Temporary Control Measures

Housekeeping
Contractor shall provide pavement sweeping as needed to control dust and sediment. Daily sweeping may be necessary to prevent or mitigate tracking of sediment onto public roads.

Temporary Mulch
-Purpose: To prevent erosion by protecting exposed soil surfaces and to aid in the growth of vegetation by conserving available moisture, controlling weeds, and providing protection against extreme heat and cold.
-Product: Organic Mulches: straw mulch, free of weed seeds.
-Required Usage: Stabilize with mulch or other non-erodible cover any exposed soils that will not be worked for more than 75 feet of a wetland or waterbody within 48 hours of the initial disturbance of the soil or prior to any storm event, whichever comes first.
-Execution: Application shall be at a rate of 2 bales (70-90 lb.) per 1,000 sq. ft. and lightly cover 75-90% of the surface.
-Maintenance: All mulches shall be inspected weekly and after every storm event, to check for erosion. Remulching shall be required if less than 90% of soil surface is covered.
-Removal: Temporary Mulch shall be removed once vegetative cover has been established or regrading is to be done.

Winter Mulching
Winter mulching shall take place following disturbance of all areas. Stockpiles not scheduled for use within seven (7) days shall be winter mulched. In no case shall ground be left exposed and inactive in freezing conditions. All exposed areas shall be mulched prior to freezing.

Temporary Grass Cover-Non-Wetland Areas
-Purpose: To reduce erosion by stabilization of disturbed areas which have not been brought to final grade.
-Product: Lime-50% calcium plus magnesium oxide, Fertilizer-10:10:10 (N-P205-K20), Seed-Creeping Red Fescue, Redtop, Tall Fescue.
-Required Usage: Use winter rye between August 15th and October 1st at a rate of 112 lbs/1 acre and to a depth of 1-1.5 inches. Use annual rye between April 1st and July 1st at a rate of 40lb/1 acre and to a depth of 0.5-1 inch. Use Sudangrass between May 15th and August 15th at a rate of 40 lbs/1 acre and to a depth of 0.5-1 inch.
-Execution: Loosen topsoil to a depth of 2". Apply 13.8 lb. of 10-10-10 (N-P205-K20) or equivalent fertilizer per 1000 sq. ft. and lime at rate of 138 lb. per 1,000 sq. ft. Apply seed uniformly and apply temporary mulch.
-Maintenance: All temporary covers shall be inspected weekly and after every storm event, to check for erosion. Reseeding shall be required if less than 95% of soil surface is covered.

Temporary Grass Cover-Wetland Areas
-Purpose: To reduce erosion by stabilization of disturbed areas which have not been brought to final grade.
-Product: Seed-Creeping Red Fescue, Redtop, Tall Fescue.
-Required Usage: Use winter rye between August 15th and October 1st at a rate of 112 lbs/1 acre and to a depth of 1-1.5 inches. Use annual rye between April 1st and July 1st at a rate of 40lb/1 acre and to a depth of .25 inches. Use Sudangrass between May 15th and August 15th at a rate of 40 lbs/1 acre and to a depth of 0.5-1 inch.
-Execution: Loosen topsoil to a depth of 2". Apply 13.8 lb. of 10-10-10 (N-P205-K20) or equivalent fertilizer per 1000 sq. ft. and lime at rate of 138 lb. per 1,000 sq. ft. Apply seed uniformly and apply temporary mulch.
-Maintenance: All temporary covers shall be inspected weekly and after every storm event, to check for erosion. Reseeding shall be required if less than 95% of soil surface is covered. Provide for other preventative measures in the interim.

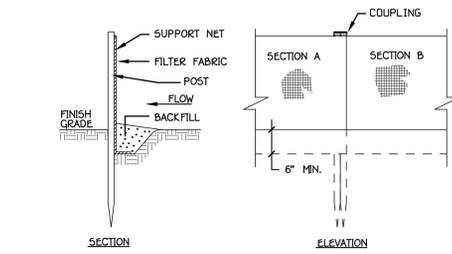
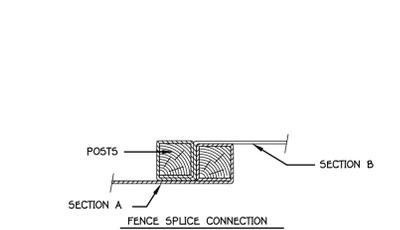
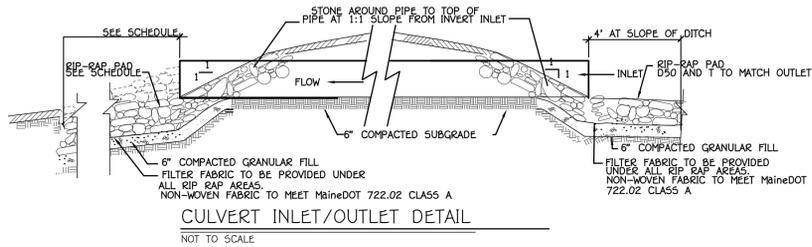
Temporary Sedimentation Barriers
-Purpose: To intercept and retain small amounts of sediment from disturbed or unprotected areas of limited extent.
-Product: Well graded mixture of particle sizes and may contain rocks <4" diameter. Mix must be free of refuse, physical contaminants, and material toxic to plant growth. Organic matter shall be between 50-10% dry weight, fibrous and elongated. The pH between 5.0-8.0. Particle size by weight shall be between 70-100% passing a 6" screen; max. of 85% passing a 0.75" screen.
-Required Usage: Install along uniform grade of disturbed downhill slope.
-Installation: Install at the beginning of the project prior to any soil disturbance.
-Execution: Install barrier in accordance with detail.
-Maintenance: All sediment barriers shall be inspected weekly and after every storm event, to check for sediment build up or failure. Sediments shall be removed after each storm event.
-REMOVE SEDIMENT PRIOR TO REACHING 1/2 OF CONTAINMENT HEIGHT
-Sediment barriers shall be maintained in effective condition until all upgradient locations are in finished condition including the vegetation being established.
-Removal: mulch shall be removed once sedimentation of the runoff is no longer a threat. Regrade, reseed and mulch affected areas.

Temporary Check Dams
-Purpose: To reduce the velocity of concentrated storm water flows to prevent erosion.
-Products: Stone of 2" to 3" in diameter.
-Required Usage: As shown on Drainage Plan
-Installation: Install immediately after ditch development rough grading.
-Execution: Install in accordance with enclosed details.
-Maintenance: All check dams shall be inspected weekly and after every storm event, to check for sediment build up or failure. Sediments shall be removed after each storm event. Check dams shall be maintained in effective condition until permanent channel stabilization has been established.
-REMOVE SEDIMENT PRIOR TO REACHING 1/2 OF CONTAINMENT HEIGHT
-Removal: Remove Check dams when channel stabilization has been established. Finish area as specified. Areas of removal shall be inspected until stabilization of these areas is reached.

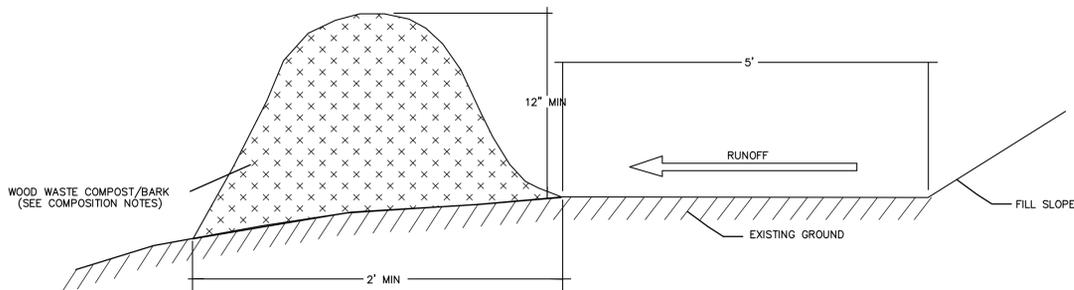
Permanent Control Measures
Permanent Grass Cover
-Purpose: To permanently stabilize the soil to reduce erosion of soils.
-Products: Lime-50% calcium plus magnesium oxide, Fertilizer-10:20:20 (N-P205-K20), Seed-
-Required Usage: MDOT Park mixture shall be used for loamed areas which are expected to be maintained by frequent mowing; i.e. private lawns. MDOT roadside mixture #2 shall be used on loamed or existing soil areas which are expected to be maintained by infrequent mowing; i.e. inslopes, ditches, and rural roads. MDOT roadside mixture #3 inclusions and time eroding soils, erosion control mix or rip rap areas which are not expected to be moving; i.e. backslopes, guardrail areas.
-Execution: Apply top soil to a level of 4". Work limestone (138 lbs/1000 SF) and fertilizer (18.4 lbs/1000 SF) into soil. Remove debris, stones 2" or larger. Apply seed mixture (0.97 lbs/1000 SF). Then apply mulch (2 bales/1000 SF). After September 1 provide temp. cover.
-Maintenance: Establish a grass cover and remove mulch. Reseed areas not attaining a cover of 90%.

Culvert Inlet & Outlet Protection
-Purpose: To permanently stabilize soil around culvert inlets and outlets to prevent scouring, undercutting, and erosion of soils.
Products: Clean stones sized in accordance with enclosed details.
Required Usage: Locate at all culvert entrances and discharges.
Installation - Install concurrently with culvert installation.
Execution: Install in accordance with enclosed details.
Maintenance: Inspect periodically to determine if high flows have caused scour beneath the rip rap protection or dislodged any stone. If repairs are needed, they should be accomplished immediately.

RIP RAP SCHEDULE			
DIA.	d50	T	L
15"	9"	21"	10"
18"	10"	24"	10"
24"	12"	27"	10"
36"	12"	27"	10"
48"	13"	30"	12"



SEDIMENTATION CONTROL FENCE-FABRIC
NOT TO SCALE



COMPOSITION
EROSION CONTROL MIX SHALL CONTAIN A WELL-GRADED MIXTURE OF PARTICLE SIZES AND MAY CONTAIN ROCKS LESS THAN 4" IN DIAMETER. EROSION CONTROL MIX MUST BE FREE OF REFUSE, PHYSICAL CONTAMINANTS, AND MATERIAL TOXIC TO PLANT GROWTH. THE MIX COMPOSITION SHALL MEET THE FOLLOWING STANDARDS.

- THE ORGANIC MATTER CONTENT SHALL BE BETWEEN 80 AND 100% DRY WEIGHT BASIS. PARTICLE SIZE BY WEIGHT SHALL BE BETWEEN 70 AND 100% PASSING AT A 6" SCREEN, MAXIMUM OF 85% PASSING A 0.75" SCREEN.
- THE ORGANIC PORTION NEEDS TO BE FIBROUS AND ELONGATED.
- LARGE PORTIONS OF SILTS, CLAYS OR FINE SANDS ARE NOT ACCEPTABLE IN THE MIX.
- SOLUBLE SALTS CONTENT SHALL BE <4.0 MMHOS/CM.
- THE PH SHOULD FALL BETWEEN 5.0 AND 8.0.

INSTALLATION
THE BARRIER MUST BE PLACED ALONG A RELATIVELY LEVEL CONTOUR. IT MAY BE NECESSARY TO CUT TALL GRASSES OR WOODY VEGETATION TO AVOID CREATING VOIDS AND BRIDGES THAT WOULD ENABLE FINES TO WASH UNDER THE BARRIER THROUGH THE GRASS BLADES OR PLANT STEMS. ON SLOPES LESS THAN 5% OR AT THE BOTTOM OF STEEPER SLOPES (<2:1) UP TO 20 FEET LONG, THE BARRIER MUST BE A MINIMUM OF 12" HIGH, AS MEASURED ON THE UPHILL SIDE OF THE BARRIER, AND A MINIMUM OF TWO FEET WIDE. ON LONGER OR STEEPER SLOPES, THE BARRIER SHOULD BE WIDER TO ACCOMMODATE THE ADDITIONAL RUNOFF.
FROZEN GROUND, OUTCROPS OF BEDROCK AND VERY ROOTED FORESTED AREAS ARE LOCATIONS WHERE BERMS OF EROSION CONTROL MIX ARE MOST PRACTICAL AND EFFECTIVE.
OTHER BMPs SHOULD BE USED AT LOW POINTS OF CONCENTRATED RUNOFF, BELOW CULVERT OUTLET APRONS, AROUND CATCH BASINS AND CLOSED STORM SYSTEMS, AND AT THE BOTTOM OF STEEP PERIMETER SLOPES THAT ARE MORE THAN 50 FEET FROM TOP TO BOTTOM (I.E., A LARGE UP GRADIENT CONTRIBUTING WATERSHED).

TEMPORARY MULCH SEDIMENT BARRIER
NOT TO SCALE

OVERWINTER CONSTRUCTION AND STABILIZATION

PURPOSE & APPLICATIONS

If a construction site is not stabilized with pavement, a road gravel base, 75% mature vegetation cover or riprap by November 15 then the site needs to be protected with over-winter stabilization. An area considered open is any area not stabilized with pavement; vegetation, mulching, erosion control mix, erosion control mats, riprap or gravel base on a road. The winter construction period is from November 1 through April 15.

CONSIDERATIONS

Winter excavation and earthwork shall be completed such that no more than 1 acre of the site is without stabilization at any one time. Limit the exposed area to those areas in which work is to occur during the following 15 days and that can be mulched in one day prior to any snow event. All area shall be considered denuded until the subbase gravel is installed in roadway areas or the areas of future loam and seed have been loamed, seeded and mulched. A cover of erosion control mix performs the best. Refer to the TEMPORARY MULCHING BMP. Any added measures, which may be necessary to control erosion/sedimentation, must be installed. These may be dependent upon site conditions, the actual site size and weather conditions. To minimize areas without erosion control protection, continuation of earthwork operations on additional areas shall not begin until the exposed soil surface on the area being worked has been stabilized.

SPECIFICATIONS

Natural Resource Protection

Any areas within 100 feet from any natural resources, if not stabilized with a minimum of 75% mature vegetation catch, shall be mulched by December 1 and anchored with plastic netting or protected with an erosion control cover. During winter construction, a double row of sediment barriers (i.e. silt fence backed with hay bales or erosion control mix) will be placed between any natural resource and the disturbed area. Projects crossing the natural resource shall be protected a minimum distance of 100 feet on either side from the resource. Existing projects not stabilized by December 1 shall be protected with the second line of sediment barrier to ensure functionality during the spring thaw and rains.

Sediment Barriers

During frozen conditions, sediment barriers may consist of erosion control mix berms or any other recognized sediment barriers as frozen soil prevents the proper installation of hay bales or silt fences.

Mulching

All area shall be considered to be denuded until seeded and mulched. Straw mulch shall be applied at a rate of 150 lb. per 1,000 square feet or 3 tons/acre (twice the normal accepted rate of 75-lbs./1,000 s.f. or 1.5 tons/acre) and shall be properly anchored. Erosion control mix must be applied with a minimum 4 inch thickness. Mulch shall not be spread on top of snow. The snow will be removed down to a one-inch depth or less prior to application. After each day of final grading, the area will be properly stabilized with anchored straw or erosion control matting. An area shall be considered to have been stabilized when exposed surfaces have been either mulched or adequately anchored so that ground surface is not visible through the mulch. Between the dates of November 1 and April 15, all mulch shall be anchored by either mulch netting, asphalt emulsion chemical, tracking or wood cellulose fiber. The cover will be considered sufficient when the ground surface is not visible through the mulch. After November 1st, mulch and anchoring of all exposed soil shall occur at the end of each final grading workday.

Soil Stockpiling

Stockpiles of soil or subsoil will be mulched for over winter protection with straw at twice the normal rate or with a four-inch layer of erosion control mix. This will be done within 24 hours of stocking and re-established prior to any rainfall or snowfall. Any soil stockpile will not be placed (even covered with mulched) within 100 feet from any natural resources.

Seeding

Between the dates of October 15 and April 1st, loam or seed will not be required. During periods of above freezing temperatures finished areas shall be fine graded and either protected with mulch or temporarily seeded and mulched until such time as the final treatment can be applied. If the date is after November 1st and if the exposed area has been loamed, final graded with a uniform surface, then the area may be dormant seeded at a rate of 3 times higher than specified for permanent seed and then mulched. Dormant seeding may be placed prior to the placement of mulch or erosion control blankets. If dormant seeding is used for the site, all disturbed areas shall receive 4" of loam and seed at an application rate of 5lbs/1000 s.f. All areas seeded during the winter will be inspected in the spring for adequate catch. All areas insufficiently vegetated (less than 75 % catch) shall be revegetated by replacing loam, seed and mulch. If dormant seeding is not used for the site, all disturbed areas shall be revegetated in the spring.

Stabilization of ditches and channels

All grass-lined ditches and channels must be constructed and stabilized by September 1. If a ditch or channel is not grass-lined by September 1, then one of the following actions must be taken to stabilize the ditch for late fall and winter. Install a sod lining in the ditch. A ditch must be lined with properly installed sod by October 1. Proper installation includes: pinning the sod onto the soil with wire pins, rolling the sod to guarantee contact between the sod and underlying soil, watering the sod to promote root growth into the disturbed soil, and anchoring sod at the base of the ditch with jute or plastic mesh to prevent the sod from sloughing during flow conditions. See the PERMANENT VEGETATION BMP section. Install a stone lining in the ditch. A ditch must be lined with stone riprap by November 15. A Maine Licensed Engineer or Maine Licensed Landscape Architect must be hired to determine the stone size and lining thickness needed to withstand the anticipated flow velocities and flow depths within the ditch. If necessary, the contractor will regrade the ditch prior to placing the stone lining so to prevent the stone lining from reducing the ditch's cross-sectional area.

Stabilization of disturbed soils

All slopes to be vegetated must be seeded and mulched by September 1. The department will consider any area having a grade greater than 15% to be a slope. If a slope to be vegetated is not stabilized by September 1, then one of the following actions must be taken to stabilize the slope for late fall and winter. Stabilize the soil with temporary vegetation and erosion control mats -- By October 1 the disturbed slope must be seeded with winter rye at a seeding rate of 3 pounds per 1000 square feet and then install erosion control mats or anchored mulch over the seeding. If the rye fails to grow at least three inches or fails to cover at least 75% of the slope by November 1, then the contractor will cover the slope with a layer of erosion control mix or with stone riprap as described in the following standards.

Stabilize the soil with sod -- The disturbed slope must be stabilized with properly installed sod by October 1. Proper installation includes the contractor pinning the sod onto the slope with wire pins, rolling the sod to guarantee contact between the sod and underlying soil, and watering the sod to promote root growth into the disturbed soil. The contractor will not use late-season sod installation to stabilize slopes having a grade greater than 33% (3H:1V) or having groundwater seeps on the slope face.
Stabilize the soil with erosion control mix -- Erosion control mix must be properly installed by November 15. The contractor will not use erosion control mix to stabilize slopes having grades greater than 50% (2H:1V) or having groundwater seeps on the slope face. See the TEMPORARY MULCHING BMP section.
Stabilize the soil with stone riprap -- Place a layer of stone riprap on the slope by November 15. The development's owner will hire a registered professional engineer to determine the stone size needed for stability on the slope and to design a filter layer for underneath the riprap. See the RIPRAP SLOPE STABILIZATION BMP section.

Stabilization of disturbed soils

By September 15, all disturbed soils on areas having a slope less than 15% must be seeded and mulched. If the disturbed areas are not stabilized by this date, then one of the following actions must be taken to stabilize the soil for late fall and winter. Stabilize the soil with temporary vegetation -- By October 1, seed the disturbed soil with winter rye at a seeding rate of 3 pounds per 1000 square feet, lightly mulch the seeded soil with hay or straw at 75 pounds per 1000 square feet, and anchor the mulch with plastic netting. Monitor growth of the rye over the next 30 days. If the rye fails to grow to at least three inches or fails to cover at least 75% of the disturbed soil before November 1, then mulch the area for over-winter protection as described below. Stabilize the soil with mulch -- By November 15, mulch the disturbed soil by spreading hay or straw at a rate of at least 150 pounds per 1000 square feet on the area so that no soil is visible through the mulch. Immediately after applying the mulch, anchor the mulch with plastic netting to prevent wind from moving the mulch off the disturbed soil.

MAINTENANCE

Maintenance measures shall be applied as needed during the entire construction season. After each rainfall, snow storm or period of thawing and runoff, the site contractor shall perform a visual inspection of all installed erosion control measures and perform repairs as needed to insure their continuous function. Following the temporary and/or final seeding and mulching, the contractor shall, in the spring, inspect and repair any damages and/or bare spots. An established vegetative cover means a minimum of 85 to 90 % of areas vegetated with vigorous growth.

STABILIZATION SCHEDULE BEFORE WINTER

September 15 -- All disturbed areas must be seeded and mulched. All slopes must be stabilized, seeded and mulched.
November 15 -- All stone-lined ditches and channels must be stabilized with mulch or an erosion control blanket.
October 1 -- If the slope is stabilized with an erosion control blanket and seeded.
All disturbed areas to be protected with an annual grass must be seeded at a seeding rate of 3 pounds per 1000 square feet and mulched.
November 15 -- All stone-lined ditches and channels must be constructed and stabilized.
Slopes that are covered with riprap must be constructed by that date.
December 1 -- All disturbed areas where the growth of vegetation fails to be at least three inches tall or at least 75% of the disturbed soil is covered by vegetation, must be protected for over-winter.

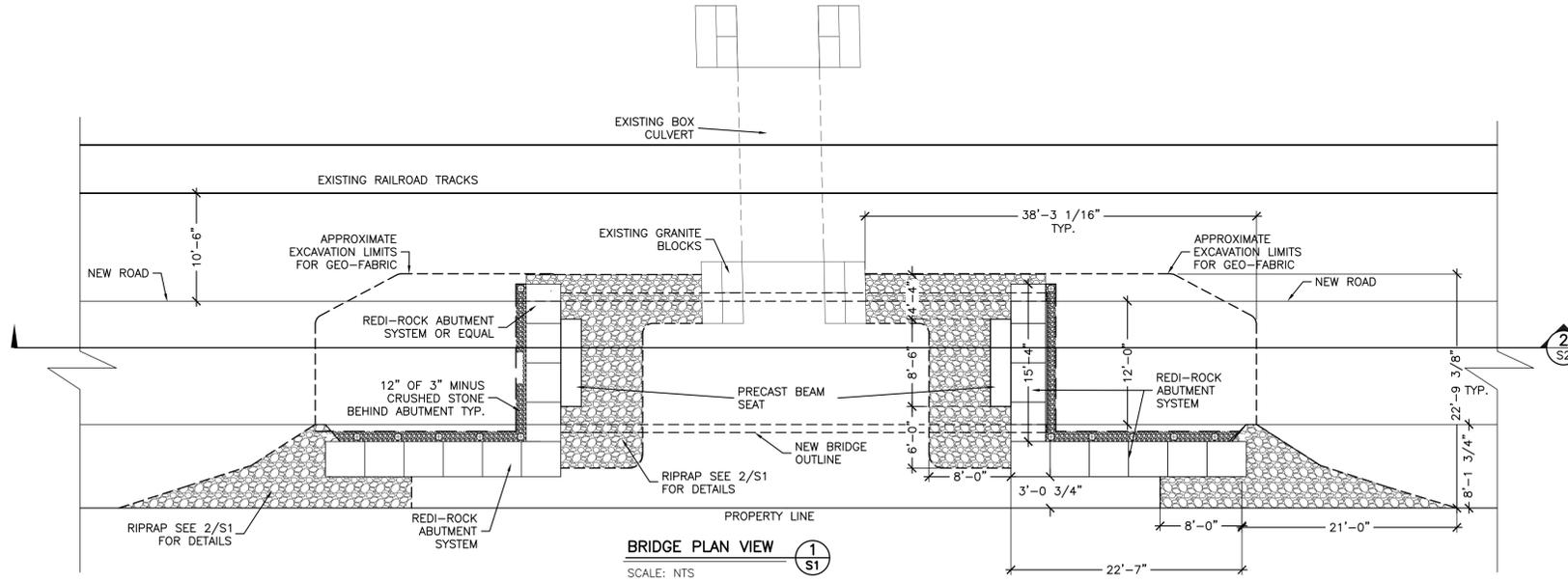
NOTE: The dates given are for projects in South-Central Maine. Adjust the dates given based on the project's location within the state -- reducing times up to three weeks for projects in Northern Maine and extending times up to two weeks for projects on the coast in extreme Southern Maine.

REVISIONS	DESCRIPTION	DATE	BY	APP'D.

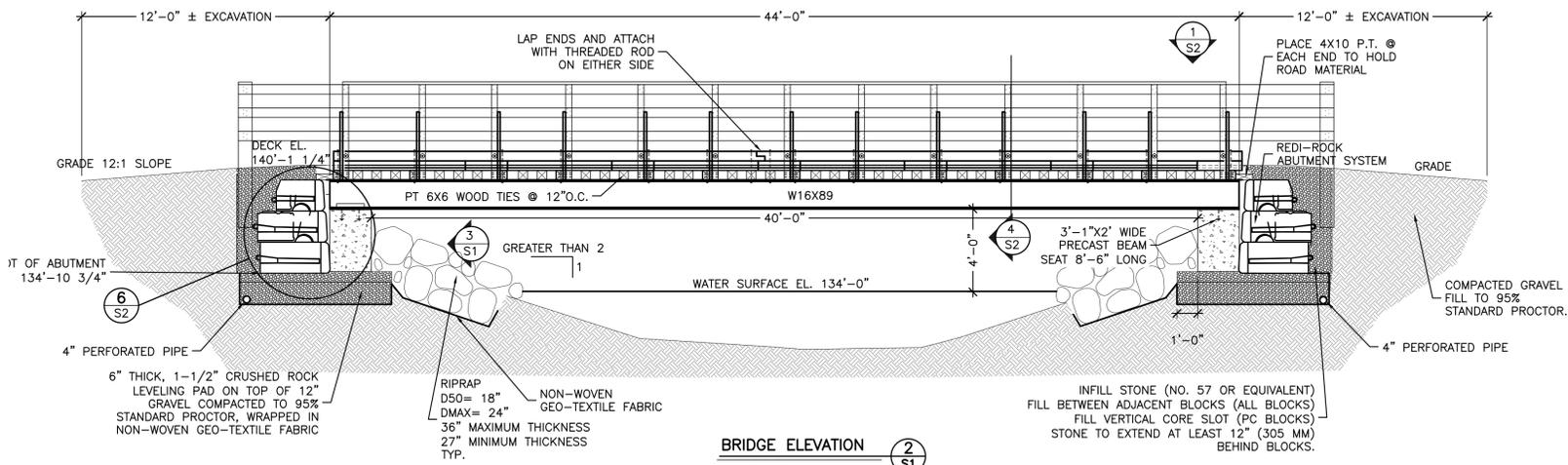
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 PROJECT NO.: H204
 DRAWING NO.: H204BBaseJing
 FIELDBOOK: N/A
 SCALE: AS SHOWN
 DATE ISSUED: March 1, 2016
 CLIENT: U.S. Forest Service, Conservation and Forestry
 SHEET NAME: DETAILS
 SHEET NO.: 37 OF 39

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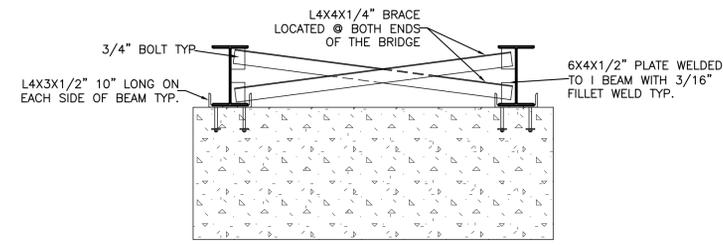
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BRIDGE PLAN VIEW (1)
SCALE: NTS



BRIDGE ELEVATION (2)
SCALE: NTS



BEAM CONNECTION DETAIL (3)
SCALE: NTS

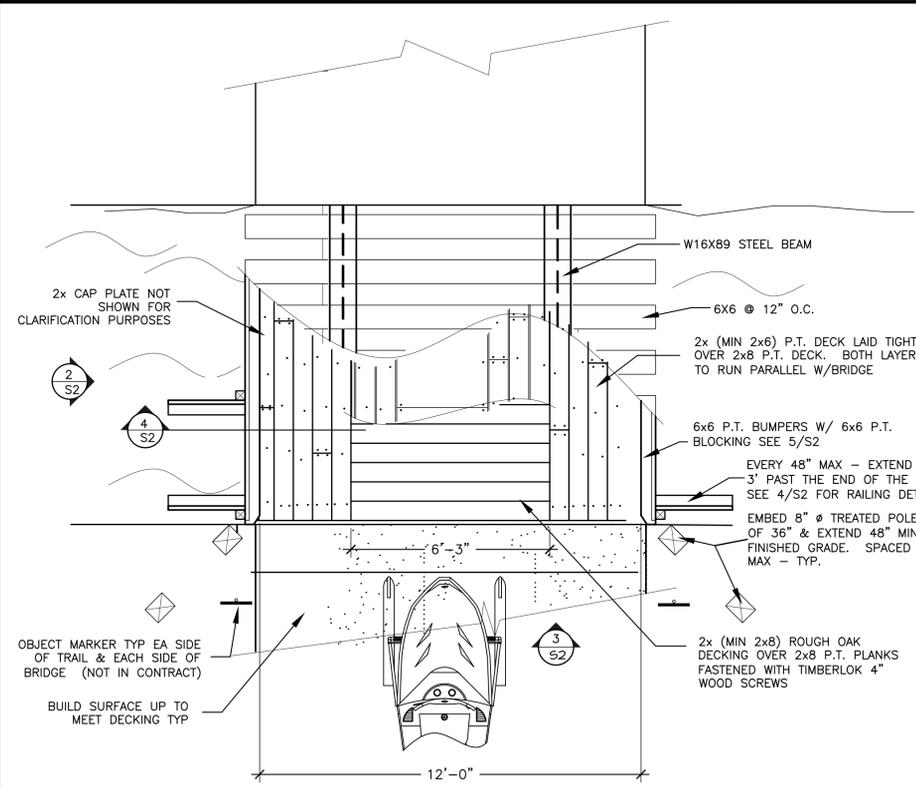
NO.	DATE	REVISIONS

PROJECT NAME:
Downeast/Sunrise Trail Extension
Calais Branch-Maine Central Railroad
Maine
Ellsworth-Hancock
SHEET NAME:

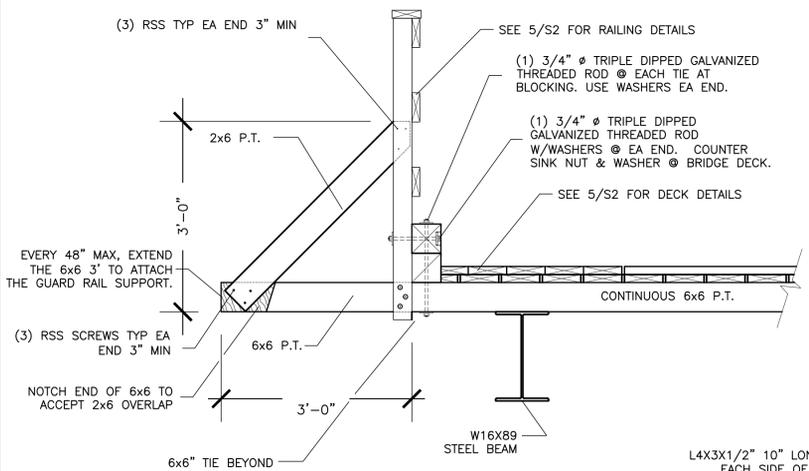
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DRAWING NO. H204Base5.dwg
FIELD NO. N/A
SCALE:
DATE ISSUED: March 1, 2016
DATE REVISION: March 1, 2016
CLIENT: Parks and Public Lands, Department of Agriculture, Conservation and Forestry, 10 Elkins Lane, Augusta, Maine 04333

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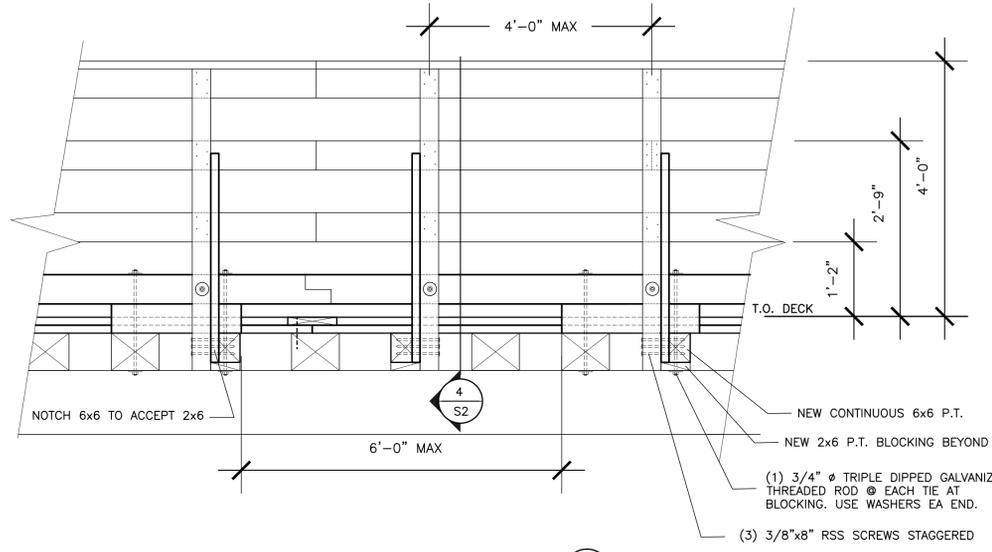
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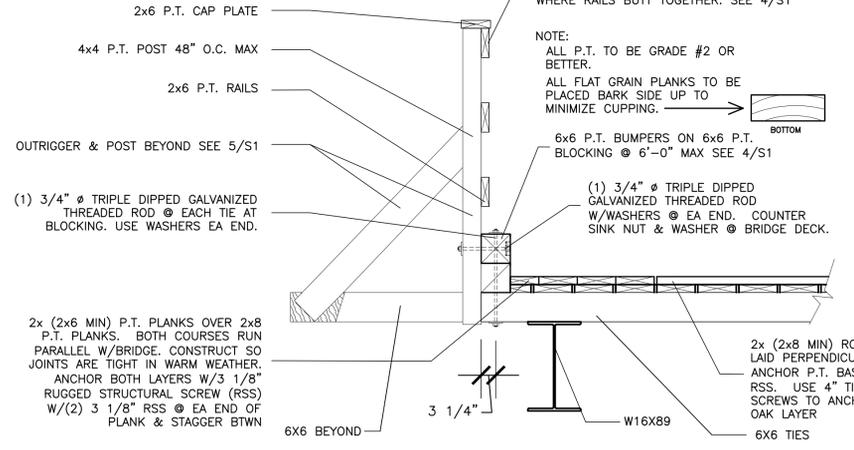
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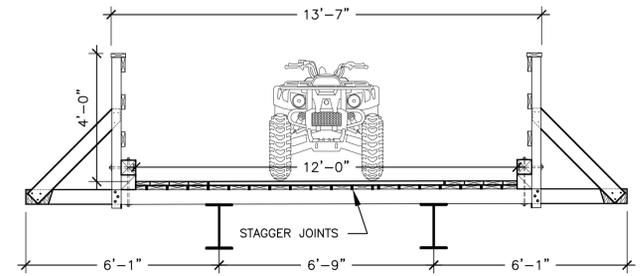
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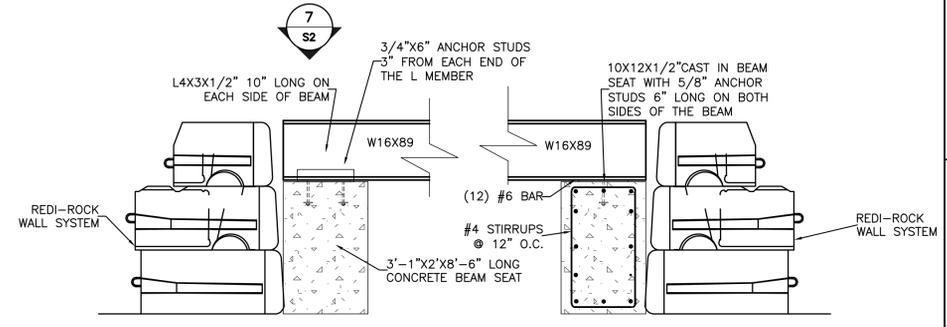
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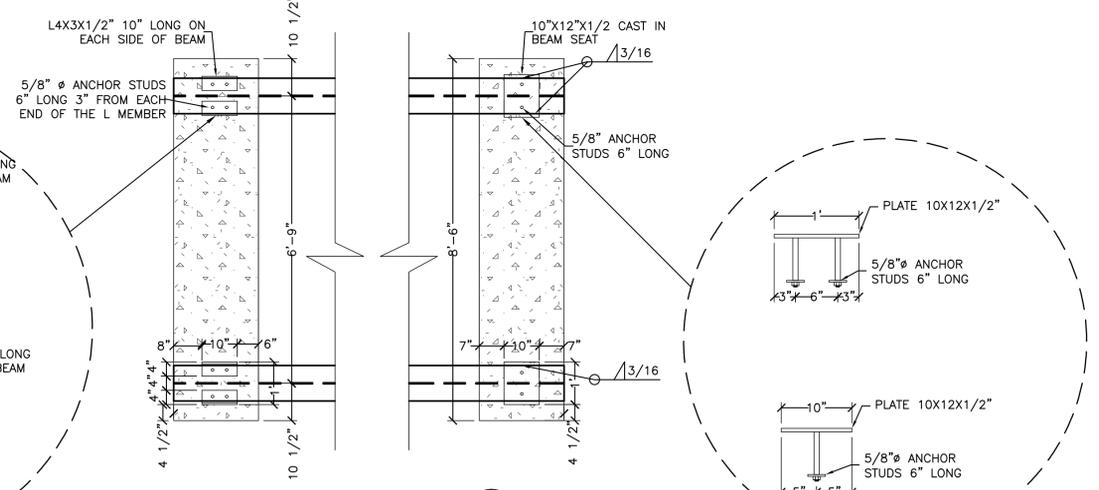
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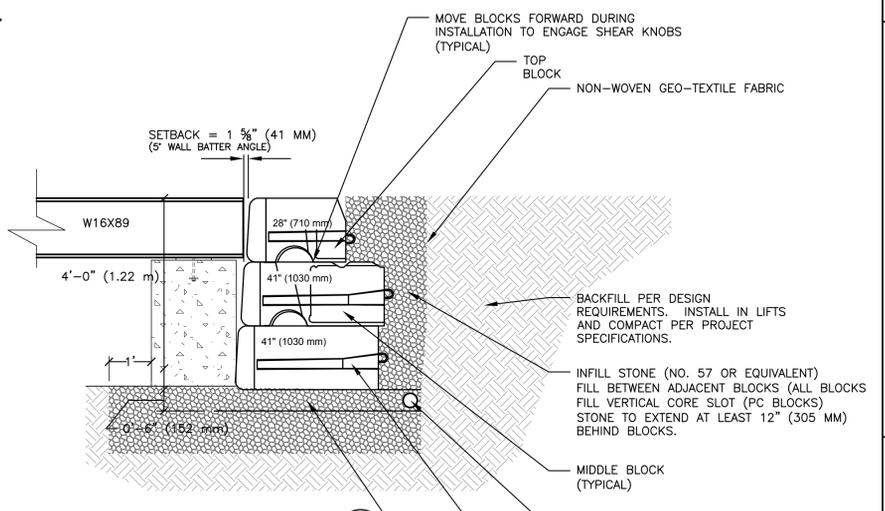
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BEAM SEAT DETAIL
SCALE: NTS



BEAM CONNECTION DETAIL
SCALE: NTS



PRELIMINARY REDI-ROCK ABUTMENT SECTION
SCALE: NTS

NOTE: DETAIL TAKEN FROM REDI-ROCK IF REDI-ROCK ABUTMENTS ARE TO BE USED CONTACT PLYMOUTH ENGINEERING FOR DETAILS

NO.	DATE	DESCRIPTION

PROJECT NAME: **Downeast/Sunrise Trail Extension**
 Calais Branch-Maine Central Railroad
 PROJECT NO: 14204
 DRAWING NO: 14204-Base.dwg
 CHECKED: [Signature]
 SCALE: N/A
 PLAN DATE: March 1, 2016
 DATE ISSUED: March 1, 2016
 CLIENT: Parks and Public Lands, Agriculture, Conservation and Forestry, 18 Elkins Lane, Augusta, Maine 04333

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SHEET 39 OF 39

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